



By Hand and Email (tpbpd@pland.gov.hk)
16 February 2026

The Secretary
Town Planning Board
15/F, North Point Government Offices
333 Java Road
North Point
Hong Kong

Your ref TPB/Y/SK-HC/8
Our ref 295143/00/WSTY/MYNL/TYAL/NC/CHAC/05734

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Dear Sir/Madam,

Application for Amendment of Plan Under Section 12A of the Town Planning Ordinance (Cap. 131) to Rezone the Application Site from “Green Belt” and Area shown as ‘Road’ to “Residential (Group C)5” for Proposed Residential Development at Various Lots in D.D. 210 and Adjoining Government Land, Pak Wai, Sai Kung (Planning Application No. Y/SK-HC/8)

Submission of Further Information – Responses to Departmental Comments

We refer to the comments received from various Government departments from 23 January to 3 February 2026 on the captioned Planning Application.

We are pleased to submit herewith a Responses-to-Comments Table (**Appendix A**) together with the Revised Geotechnical Planning Review Report (**Appendix B**), the Revised Traffic Impact Assessment (**Appendix C**) and the Replacement Page of the Revised Indicative Architectural Drawings (**Appendix D**) in support of the Application for your consideration.

In response to the verbal comments from the District Planning Office of the Planning Department, we hereby also submit the Supplementary Information (**Appendix E**) with two supplementary drawings for your consideration.

Please note that in response to the departmental comments, the Revised Geotechnical Planning Review Report has undergone a major methodological change, which includes a significant expansion of the boundaries of catchment study area for the assessment.

We sincerely seek favourable consideration from the Town Planning Board to agree to the captioned s.12A Application.

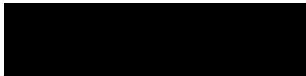
Should you have any queries, please contact the undersigned or our Miss Natalie CHAN at [REDACTED] or Mr. Alex CHENG at [REDACTED].

Yours faithfully,





Natalie Leung
Associate Town Planner

d
e



Enc - 4 Copies of **Appendices A to E**

- cc - Sai Kung & Island District Planning Office – Ms. KONG Sze Nga, Tammy ()
- Sai Kung & Island District Planning Office – Mr. YIP Ho Yeung, Jackin ()
- Client

Appendix A

Responses-to-Comments Table

Comments from Related Departments


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1. Environmental Protection Department, Water Quality Management Division, Water Quality Management Group, Specific Waste Management and Land Contamination, Assessment, dated 03 February 2026	2
2. Geotechnical Engineering Office, dated 02 February 2026.....	2
3. Planning Department, District Planning Branch, Special Duties Division, Urban Design & Landscape Section, Urban Design Unit, dated 23 January 2026	4
4. Transport Department, NT Regional Office, Traffic Engineering (NTE) Division, Housing & Planning Section, dated 23 January 2026	4

COMMENTS FROM RELATED DEPARTMENTS

No.	Comments	Responses
1.	<p>Environmental Protection Department, Water Quality Management Division, Water Quality Management Group, Specific Waste Management and Land Contamination, Assessment, dated 03 February 2026</p> <p>Please find the comments from EPD on FI(4) below for your reference.</p> <p><u>Other comments/observation on the supporting SIA to Y/SK-HC/8</u></p> <p>SIA</p> <p>The project proponent is reminded that future connection to public sewer, if available, is subject to the agreement of EPD and DSD. The project proponent would need to review the SIA and assess the potential sewerage impact arising from the development, and where appropriate, to demonstrate the effectiveness of mitigation measures. The project proponent is required to implement the sewer connection and mitigation works, if any, at its own cost to meet the satisfaction of DSD.</p>	<p>Noted.</p>
2.	<p>Geotechnical Engineering Office, dated 02 February 2026</p> <p>Please also find the comments from Geotechnical Engineering Office (GEO) on the response-to-comments table and Geotechnical Planning Review Report in FI(4) below for your follow-up action.</p> <p>1. Section 6.3: The revised screening in respect of natural terrain hazard still contains many deficiencies and our previous comments related to the screening in particular those related to catchment boundary have not be fully addressed. The Consultants shall refer to Section 2.3 of GEO Report No. 138 when conducting the screening exercise and the specific comments are provided below.</p>	<p>Noted. The catchment boundary has been revised. Please refer to Figure 4 of the Revised Geotechnical Planning Review Report (GPRR) (Appendix B of this FI refers) for the revised catchment.</p>

Responses to Comments – Departmental Comments

No.	Comments	Responses
	<p>a. Further amendments are required for catchment boundaries clouded in the attached plan. For example, the hillside above the crest of catchment CD(1), OH(A2) and OH(B) shall be included as part of the catchments. Minor portions of hillside to the southeast of catchment TD(1) and OH(A1) shall be included as part of the catchments. The southwestern portion of catchment OH(A1) does not overlook the site. Please review and amend the catchment boundaries based on the topography.</p> <p>b. The toe of the catchments shall extend to adjoin the site boundary.</p> <p>c. Figures 5 - 10: The AOR shall be measured along the steepest credible flow path. For example, in Section B-B (Figure 6), the crest to measure the AOR from the site shall be the point of elevation at +50mPD on the ground profile instead of +54mPD, which will result a steeper AOR at about 28°. Please review the other sections accordingly.</p> <p>2. It is noted that the AOR measured in the screening provided in the GPRR is based on measurement from the site boundary. Given the layout of the proposed development is available, the applicant may conduct the screening based on the facilities involved (i.e. residential buildings, club house, internal roads, swimming pool, footpath, etc.). If this approach is adopted, please classify each individual facility according to Table 2.2 of GEO Report No. 138, and reassess the AOR accordingly. This approach may provide a more refined delineation of the hillside requiring an NTHS. However, please note that the screening and extent of NTHS will need to be reviewed and updated if there are changes to the development layout.</p> <p style="text-align: center;"> Attached Plan.pdf</p>	<p>Noted. The catchment boundaries for OH(A1) have been revised and separated to OH(A1), OH(A2) and OH(A3), OH(A2) is renamed to OH(A4) and boundaries are extended.</p> <p>The catchment boundary of OH(B) has been renamed to OH(B1) and the boundary has been extended.</p> <p>The catchment boundary of TD(1) has been renamed to OH(B2) and the boundary has been extended.</p> <p>An addition catchment CD(2) has been added to cover the channelized catchment at the north-west of the lot.</p> <p>Please refer to Figure 4 of the Revised GPRR for the revised catchment.</p> <p>Noted. The catchments have been extended to adjoin the site boundary in the Revised GPRR.</p> <p>Noted. The measurement of the AOR has been revised. All sections have been revised accordingly under the Revised GPRR.</p> <p>Noted. The AOR is revised to measure from the nearest facilities. All sections are revised accordingly under the Revised GPRR.</p> <p>The screening and extent of NTHS will be reviewed and updated when the development layout is changed.</p>

Responses to Comments – Departmental Comments

No.	Comments	Responses
3.	<p>Planning Department, District Planning Branch, Special Duties Division, Urban Design & Landscape Section, Urban Design Unit, dated 23 January 2026</p> <p><i>Comments on the VIA:</i></p> <ol style="list-style-type: none"> 1. Photomontages of VP1 and VP3 - the proposed development shown in the photomontages is unclear, please review if the building bulks of the proposed development are accurately reflected in the photomontages. 	<p>The building bulk of the proposed development is accurately reflected in the photomontages. As the building height of Block 4 would be lower than the proposed landscape feature and most of the building profile for Blocks 1 and 2 will be blocked by Block 3. Therefore, only the proposed Block 3 and limited building profile of Block 2 could be seen in VP1 and VP3, which has been reflected in the latest submission of the Visual Impact Assessment (VIA).</p>
4.	<p>Transport Department, NT Regional Office, Traffic Engineering (NTE) Division, Housing & Planning Section, dated 23 January 2026</p> <p><i>Comments from the Traffic Engineering (NTE) Division:</i></p> <ol style="list-style-type: none"> 1. It is noted the layout of the internal road and parking space in the basement have been provided in the first submission. Please include the layout in Drawing No. FL-02 in the Traffic Impact Assessment Report. <p><i>Comments from the Transport Operations (NT) Division:</i></p> <ol style="list-style-type: none"> 2. For Table 3.4, please supplement (1) the date and period of the on-site survey, (2) location(s) of the survey conducted, (3) no. of left behind and (4) survey result conducted during PM peak hour; and 3. The assessment on the projected public transport demand (for both AM and PM peak) should also be included in Section 6.1 Summary. 	<p>Noted. Please refer to Appendix C of the Revised Traffic Impact Assessment (TIA) (Appendix C of this FI refers) for details.</p> <p>Please also refer to the Replacement Page of the Revised Indicative Architectural Drawings (Appendix D of this FI refers) for the Revised Basement Plan.</p> <p>Please note that no passengers are left behind during the survey periods. Other information has been supplemented in the TIA report accordingly. Please refer to Section 3.5 of the Revised TIA for details.</p> <p>Noted. The paragraph is included in Section 6.1 of the Revised TIA accordingly.</p>

(Last updated 12 February 2026)

Appendix B

Revised Geotechnical Planning Review Report

GEOTECHNICAL PLANNING REVIEW REPORT (GPRR)

FOR

APPLICATION FOR AMENDMENT OF PLAN

UNDER SECTION 12A OF THE TOWN PLANNING ORDINANCE (Cap.131)

TO REZONE THE APPLICATION SITE

FROM “GREEN BELT” AND AREA SHOWN AS “ROAD”

TO “RESIDENTIAL (GROUP C) 5”

FOR

PROPOSED RESIDENTIAL DEVELOPMENT AT

VARIOUS LOTS IN D.D. 210 AND ADJOINING GOVERNMENT LAND

PAK WAI, SAI KUNG

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APPENDICES

Appendix A	Information of Existing Slopes/Retaining Walls
Appendix B	Existing Boreholes Information
Appendix C	ENTLI and HLC from GInfo
Appendix D	Geology Map from GInfo
Appendix E	Summary of API

1. INTRODUCTION

This report presents the geotechnical planning review for the proposed development based on the available ground information in supplementary to the master layout plans.

The development involved the construction of four blocks of 1 to 4 storey residential building with carparking podium.

The geotechnical review is prepared under the supervision of Registered Geotechnical Engineer, Wong Wai Yi 0151939, in accordance with the requirement set out in PNAP-APP25 (PNAP 78). This report contains the following as listed.

- Impact and proposed investigation to the adjoining premises
- Conclusion and recommendation

2. SITE LOCATION AND DESCRIPTION

2.1 Site Location

The captioned site locates in a valley opposing Pak Wai in Sai Kung, The total area of the site is about 12,692m². The elevation of the site is about +4.5 – 7.5mPD. A site location plan is attached in Figure 1.

2.2 Existing Features

According to the Slope Information System, features related to the Site are listed as follow:

1) Features within the Site:

Feature No.	Type	Location	Responsibility	Height	Angle	Length
*7SE-D/C 154	Slope	West	DD210 Lot31 Lands D	4.0 m	40	35 m
*7SE-D/F 46	Slope	West	Lands D	5.0 m	40	35 m
7SE-D/FR 70	(Obsolete feature)					

* Refer to Section 6.2.

2) Features within the scope of 10m from site boundary

Feature No.	Type	Location	Responsibility	Height	Angle	Length
7SE-D/C 153	Slope	West	Lands D	3.5 m	60	75 m
7SE-D/R 54	Wall	East	Lands D	4.4 m	85	30 m
7SE-D/C 171	Slope	East	Lands D	7.1 m	33	52 m

3) Feature attached in the Catchment OH(A2):

Feature No.	Type	Location	Responsibility	Height	Angle	Length
7SE-D/C 163	Slope	West	Lands D	9.0 m	45	45 m

4) Feature attached in the Catchment OH(A3):

Feature No.	Type	Location	Responsibility	Height	Angle	Length
7SE-D/C 163	Slope	West	Lands D	9.0 m	45	45 m

5) Feature attached in the Catchment OH(A4):

Feature No.	Type	Location	Responsibility	Height	Angle	Length
7SE-D/C 370	Slope	West	DD210 Lot 11 Lands D	3.8 m	40	16 m

6) Feature attached in the Catchment OH(B1):

Feature No.	Type	Location	Responsibility	Height	Angle	Length
7SE-D/C 157	Slope	East	Lands D	6.0 m	70	125 m
7SE-D/C 159	Slope	East	Lands D	6.0 m	60	28 m

7) Feature attached in the Catchment OH(B2):

Feature No.	Type	Location	Responsibility	Height	Angle	Length
7SE-D/C 152	Slope	East	Lands D	15.0 m	65	50 m
7SE-D/C 164	Slope	East	Lands D	6.0 m	55	31 m
7SE-D/C 155	Slope	East	Lands D	6.8 m	70	35 m

8) Feature attached in the Catchment CD(2):

Feature No.	Type	Location	Responsibility	Height	Angle	Length
7SE-D/C 152	Slope	East	Lands D	15.0 m	65	50 m
7SE-D/C 164	Slope	East	Lands D	6.0 m	55	31 m
7SE-D/C 155	Slope	East	Lands D	6.8 m	70	35 m

Location plan & basic information of the features are attached in **Appendix**

A. The available information indicates that the slope shall be in good condition.

A natural terrain is located at the east, west and north of the site.

3. THE PROPOSED DEVELOPMENT

The development involved the construction of residential development of four blocks of 1 to 4 storey residential building with one-level basement carpark. The schematic master layout plan and diagrammatic section of the proposed new building are given in Figure 2 and Figure 3.

4. EXISTING INFORMATION

4.1 Ground Investigation Information

Two boreholes carried out by Enpack (HK) Ltd. In 2001 BH8 and BH9 were found within the site, information is given in Appendix B.

4.2 Layout of Existing Utilities

Enquiries shall be made to various utility companies for the layout of existing utilities adjacent to the site. A waterpipe laid along the existing footpath to be diverted before the site formation work.

4.3 Layout of Existing Building Structure

There is one temporary structure of concrete building in the lot and this will be demolished during the development abandoned.

5. GROUND CONDITIONS

5.1 Topography

The elevation of site is about +4.5 – 7.5mPD. The site can be accessed via Access road from Hiram's Highway. The total area of the site is about

12,692m².

5.2 Geology

From the two boreholes information, the site would be covered by a layer of fill/ alluvium/ residual soil of 2m to 6m thick. Underneath the fill/ alluvium/ residual soil there would be a layer of grade V to IV TUFF. Grade III or better grade of Tuff be found at a depth of 3m to 6m below existing ground level.

Soil Properties

FILL

Fill generally consists of light brown, sandy silt.

ALLUVIUM

Alluvium generally consists of firm, greyish brown, silt or clayey sand, locally sandy or silty clay.

TUFF

Tuff was classified as weak to strong, light yellowish brown, highly to slightly decomposed, coarse ash crystal.

5.3 Groundwater Condition

Based on the available ground investigation information, highest water table may be at about 2m below existing ground level.

6. PROPOSED WORKS

6.1 Proposed Ground Investigation Works

The proposed Ground Investigation works for the site area at D.D.210, Sai Kung for the proposed development will include drilling of 7 vertical drillholes with installation of 3 standpipe / piezometer inside the site. Sampling of soil/rock material and testing will be proposed to determine the soil/rock properties.

6.2 Proposed Works on Existing Features

There are 2 existing features within the site and 4 outside site boundary but close to the site.

The stability of existing features within or close to the site (including any unregistered features) to be affected or being affected by the development during site formation works shall be assessed. Monitoring works shall be carried out during the whole construction period. Remedial or upgrading works shall be proposed and carried out if found necessary.

6.3 Proposed Works on Natural Terrain

6.3.1 Description of the Catchment Area

Based on available topographic maps No. 7SE-25A, 7SE-25B, 7SE-25C and 7SE-25D of Hong Kong from the Survey & Mapping Office (SMO) of the Lands Department, the hillside catchment of proposed Redevelopment under study are described below, and presented as Figure 4, with relevant cross-sections of the catchment presented as Figure 5 to 13.

The Study Area can be sub-divided into 5 major catchments i.e.

- (a) Catchment OH(A1), OH(A2), OH(A3) & OH(A4) – They are piece of abandoned cultivated land at toe and undisturbed natural openhill terrain at crest. The majority portion of these areas is characterized by moderately gentle terrains i.e. 20° to 33° .
- (b) Catchment OH(B1) – Undisturbed natural openhill terrain. On the NE flank of the hillside, the contour run from +65.5 mPD highest at the burial urns. The majority portion of this catchment is characterized by gentle terrains i.e. $17^{\circ} < 20^{\circ}$.
- (c) Catchment OH(B2) – Undisturbed natural openhill terrain. On the NE flank of the hillside, the contour run from +40 mPD highest to +16mPD inclined gently at $17^{\circ} < 20^{\circ}$ followed by an incise

inclined river embankment. This catchment is separated from the subject site by a river 6m wide.

(d) Catchment CD(1) – the contour within this catchment contain channelized catchment pronounced with a definite flow path running downhill north easterly. The identified runoff angle in this depression zone is gentle i.e. 16° to 20° .

(e) Catchment CD(2) –the contour within this catchment contain channelized catchment pronounced with a definite flow path running downhill southerly. The identified runoff angle in this depression zone is gentle i.e. $10^{\circ} < 20^{\circ}$

6.3.2 Published Geology

Available geological information for the Catchment Areas have been reviewed. Based on GInfo Geological Map & Solid Geology, the catchment are underlain by coarse ash crystal TUFF, with overlying colluvium. Part plan of geological mapsheets are attached in Appendix D.

6.3.3 Landslide Inventories

According to the ENTL1 dataset (Appendix C refers), there are three historic relict landside first identified in 1963. These are 07SED0074E, 07SED0075E & 07SED0076E. These locations are marked on Aerial photos 1963 attached in Appendix E, also marked on Figure 4. In which two of them lies totally outside the Catchment Zone. Only 07SED0075E is of relevant to this study, and is further discussed in 6.3.5.

6.3.4 Summary of Catchment Study Areas

Referring to 2.3.4 Alert Criteria Fig 2.5 in GEO Report 138, the summary of Catchment Study Areas as follow:

Catchment Study Area	Angular Elevation of natural terrain from the site	Angular Elevation $\geq 20^\circ$	Within 50m of ground slope at $> 15^\circ$	Alert Criteria met?	Remark
OH(A1) (Section H-H)	19.7°	No	Yes	No	-
OH(A1) (Section B-B)	24°	Yes	Yes	Yes	-
OH(A3) (Section A-A)	26°	Yes	Yes	Yes	-
OH(A4) (Section D-D)	23°	Yes	N/A	Yes	The nearest facility is the swimming pool, which is Group 4 in the faculty grouping system. The clubhouse serves solely as a changing room for the pool. No one will remain in the clubhouse during critical weather or when the pool is closed.
CD(1) (Section C-C)	19°	No	N/A	No	
CD(2) (Section G-G)	10°	No	N/A	No	
OH(B1) (Section F-F)	17°	No	Yes	No	The catchment has a locally steep (23°) with only 10m slope at the toe, but with an overall angular elevation of 17°, it should not meet the Alert Criteria
OH(B2) (Section E-E)	31°	Yes	Yes	Yes	-

6.3.5 Initial Findings to Catchment OH(A2), OH(A3) and OH(B3) to be studied

As revealed from aerial photographs from 1963 to 2024. The catchment under study has no past landslide failure. These pre-historic events were identified. Two of these totally lied outside study catchment and direction of landslide is non-critical. Lies within

Catchment OH(A2) and OH(A3). No debris flow path can be traced in 1963 aerial photo. Primarily Catchment CD(1) remained undisturbed.

In conclusion, Catchment OH(A2) and OH(A3) contain cultivated land which likely have been progressively abandoned from 1963 onwards. Therefore, this vast piece of farmland and its surrounding natural habitats have been gradually covered up fully by mature dense vegetation. A summary of API is attached in Appendix E.

6.3.6 Screening of Catchment in accordance with GEO Report 138

Refer to 6.3.4. It is proposed the hillside catchment OH(A2) Section B-B, OH(A3) Section A-A & OH(B2) Section E-E shall carry out further study. The extent of natural terrains affecting the Site to be studied is demarcated in Figure 13.

6.4 Proposed Foundation Works

The proposed development comprises four blocks of 1 to 4 storey residential building with one level of basement carpark. The available ground investigation information indicates that the rockhead level, defined as weathering grade III or better rock with total core recovery greater than 85%, or a firm stratum such as grade IV rock, may be encountered on average at a level about 3m to 6m below the existing ground. As the loading from the building is comparatively general, mini pile or pre-boring socket H-piles are considered to be feasible foundation options for the proposed building. In case the rock head level is shallow or the bearing capacity is checked to be adequate, footing foundation shall be an alternative. The proposed foundation scheme of the development shall have minimal effect or impact to the stability of all slopes (man-made & natural terrains), retaining walls and existing building or structures within or in vicinity to the lot. Detailed foundation design will be submitted separately when the proposed ground investigation works are completed. Stability of all existing slopes (man-made & natural terrains) and retaining wall (including unregistered features) within or in vicinity to the lot affecting or being affected by the development during demolition & construction shall be assessed & remedial works shall be carried out if found necessary.

6.5 Proposed Site Formation, Excavation and Lateral Support Works

Since the proposed building platform is similar to existing ground level of the site, site formation works are considered to be minimal. For the construction of footing / pile cap, open excavation, sheet pile / pipe pile / soldier pile wall with walings and struts are considered to be feasible scheme to retain excavation depth. The choice of scheme of temporary support shall be subject to detail assessment. Stability of all existing slopes (man-made & natural terrains) and retaining walls (including unregistered features) within or in vicinity to the lot affecting or being affected by the development during demolition & construction shall be assessed & remedial works shall be carried out if found necessary. The detailed design of the works will be submitted separately.

6.6 Construction Method & Sequence

Monitoring points shall be installed & initial reading shall be recorded prior commencement of any works. Remedial works on existing slopes & retaining walls being affected shall be carried out prior commencement of site formation, ELS, foundation & superstructure. Pre-boring shall be carried out prior H-pile installation.

Obstruction during pipe pile installation for temporary ELS shall be overcome by pre-boring. Strut of the ELS shall not be dismantled until completion of the permanent screen wall support the level difference. All temporary cut slope and back filling shall be backfilled by proper material with proper compaction. Groundwater shall be controlled during the excavation. Excessive groundwater drawdown at the adjacent ground shall not be allowed.

7. IMPACTS ON ADJACENT PREMISES/GEOTECHNICAL FEATURES

As the existing registered retaining walls & slope is in close proximity to the captioned site, consideration shall be taken in the design of the proposed works. Also, vibration caused by the installation of the pipe piles / socket H-pile and the ground settlement caused by the wall deflection and dewatering in the excavated areas may cause adverse effects to the adjacent utilities and

structures. Therefore, it is necessary to limit the amount of vibration and ground settlement by adoption suitable lateral support works design and construction method. Pre-boring shall be carried out prior installation of piling for foundation and ELS. In all case, stability of existing geotechnical features within or in vicinity to the lot affecting or being affected by the development shall be assessed & remedial works shall be carried out if found necessary.

Groundwater control during the excavation is also critical for the design. Excessive groundwater drawdown at the adjacent ground is not allowed because ground settlement will be induced and damage will be caused to the adjacent structures and utilities. Detail assessment and design will be included in the foundation, site formation and ELS submission.

In addition, in order to ensure the adjacent premises will not be damaged by the proposed works, settlement monitoring stations, titling check points and building settlement pins will be proposed to be installed around the site. The movement of the adjacent premises will be monitored at these stations continuously throughout the work period. The noise from proposed works shall be kept within acceptable limit to minimize the disturbance to the environment. The detailed assessment and discussion on these aspects will be presented in the separate submission for ELS works.

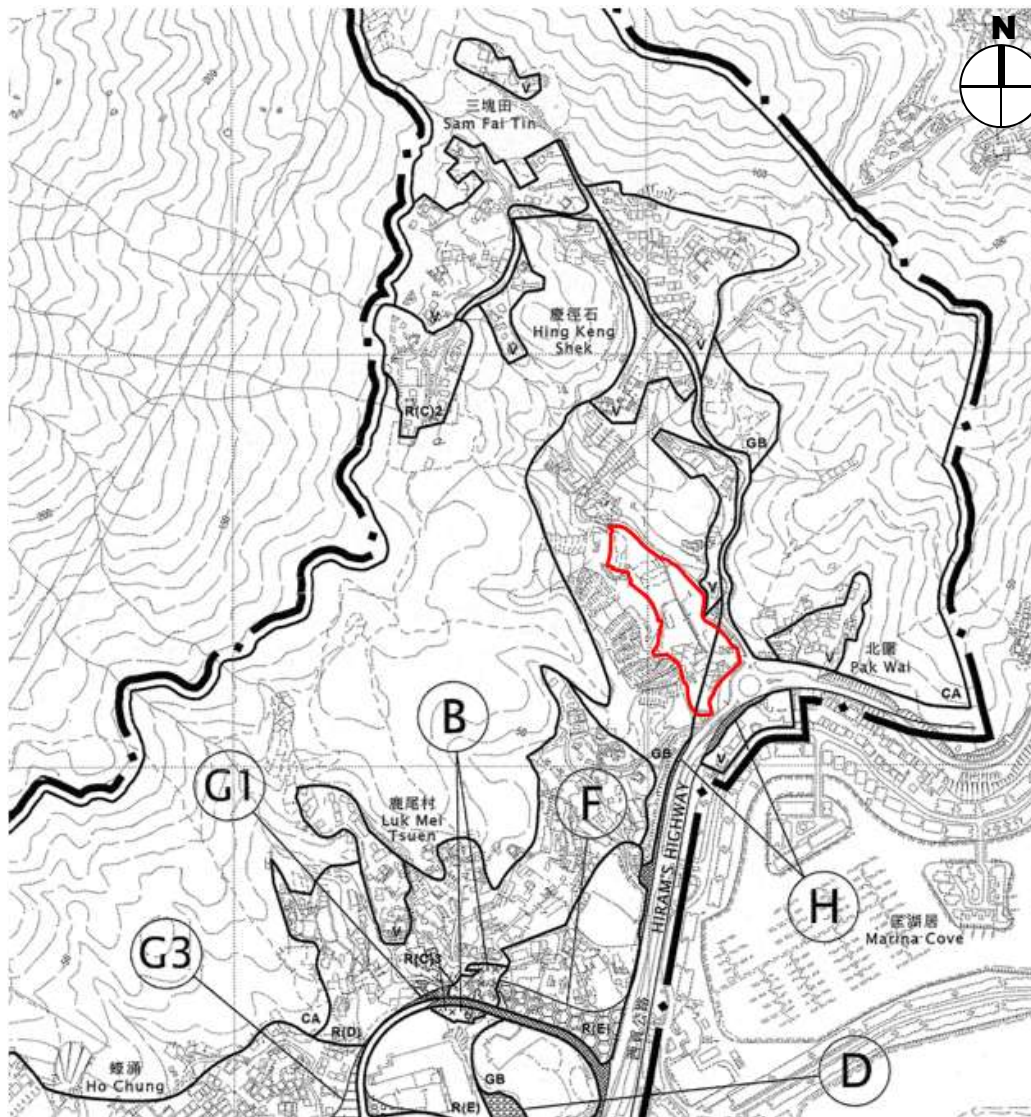
8. **CONCLUSION**

On the basis of the available geotechnical information, the following conclusions and recommendation are drawn:

- The proposed development as presented in the Master Layout Plan is considered as a geotechnical feasible scheme.
- Ground investigation works will be carried out within the proposed development site.
- Mini pile or socket H pile with pre-boring is considered to be feasible foundation options for the proposed development. Footing may be an alternative if the soil stratum at shallow level provides adequate bearing capacity.
- The site formation works for the proposed development shall be minimal.

- Suitable groundwater control scheme shall be considered in design to avoid excessive groundwater drawdown at the adjacent area.
- Proper excavation and lateral support works design and construction method have to be adopted to minimize the adverse effect on the existing utilities and structures. The detailed discussion will be presented the separate submission for excavation and lateral support works.
- Stability of all slopes (man-made & natural terrains) and retaining walls (including unregistered features) within or in vicinity to the lot affecting or being affected by the development during demolition & construction works shall be assessed & remedial works shall be carried out if found necessary.
- Stability of the natural terrain affecting or being affected by the development permanently or temporarily during demolition & construction works shall be assessed & monitored. Mitigation works shall be proposed and carried out as necessary.

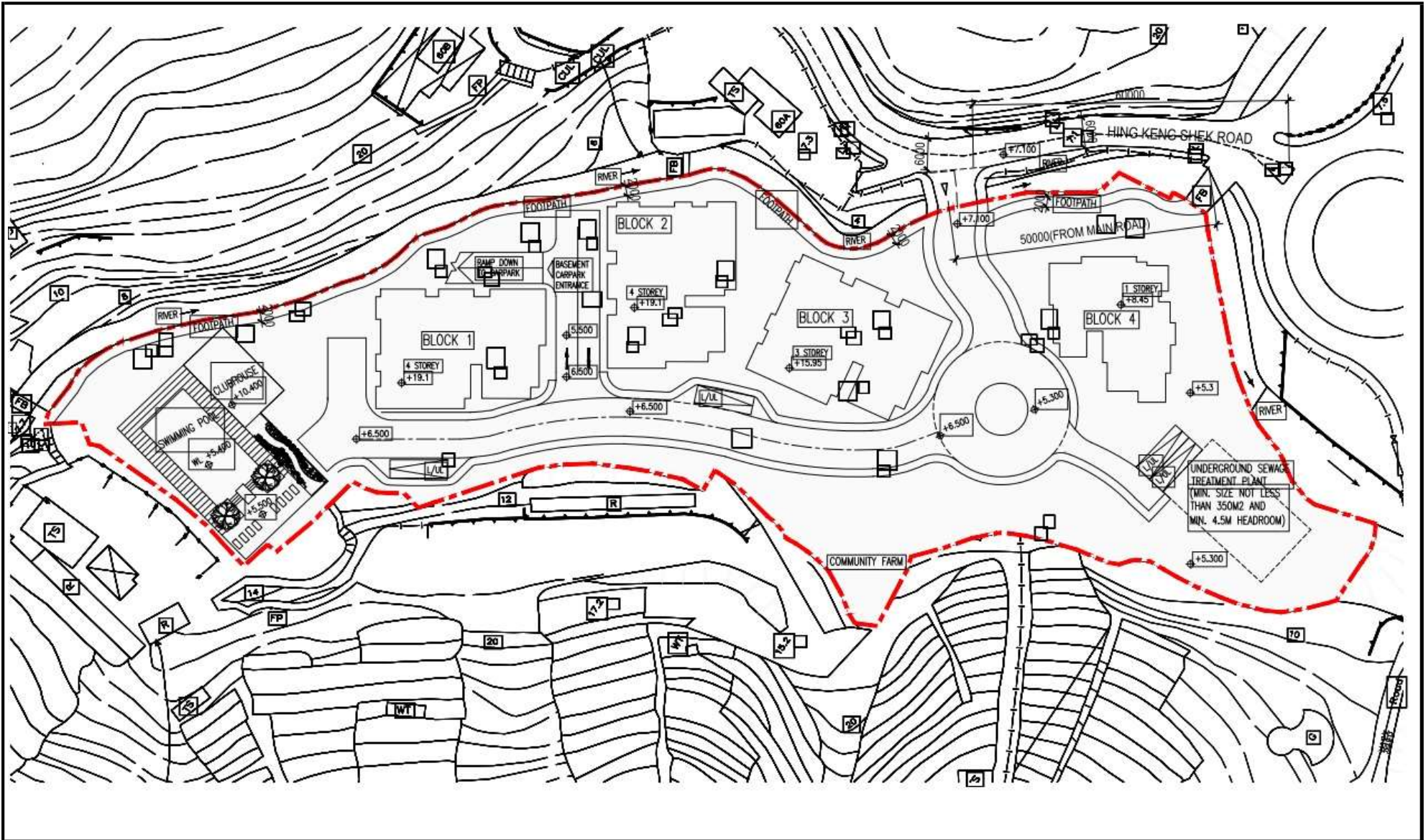
It is noteworthy to note that the works / proposal mentioned in this report are preliminary only at the planning stage and will be subjected to detailed study in the detail design stage under separate cover. Based on the current assessment, the proposed development is considered geotechnical feasible.



Site Location Plan

(Extracted from Draft Ho Chung Outline Zoning Plan No. S/SK-HC/12)

Figure 1

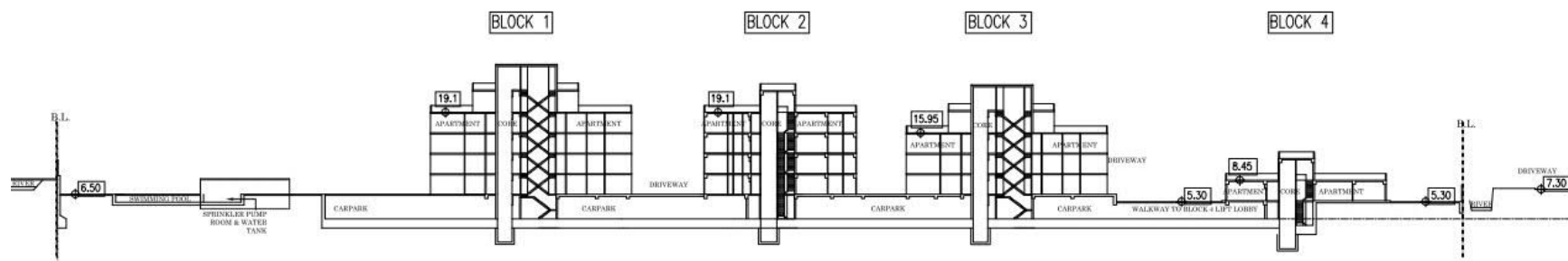


Master Layout Plan

Figure 2



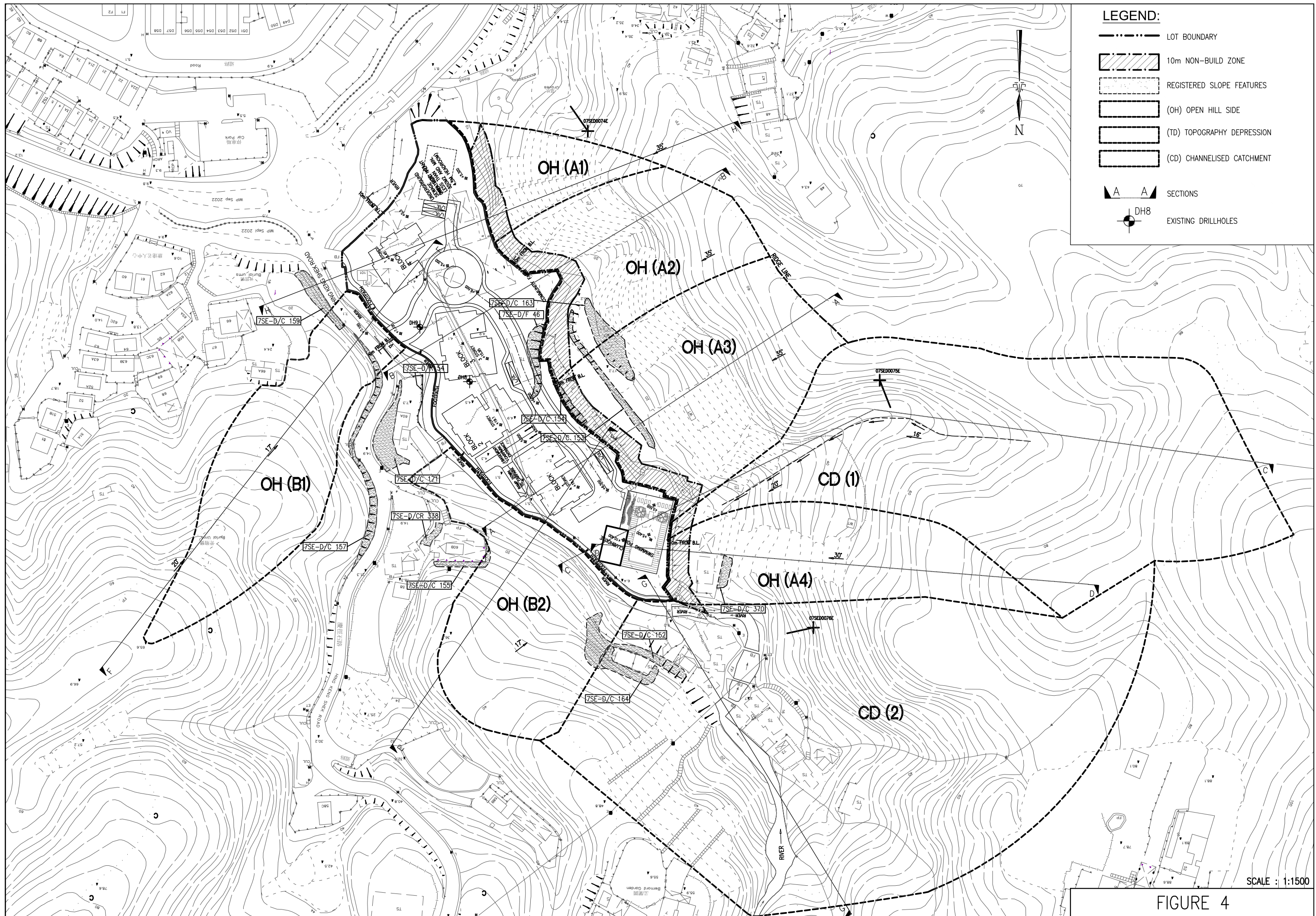
KEY PLAN 1 : 3000



DIAGRAMATIC SECTION

Diagrammatic Section

Figure 3



SCALE : 1:1500

FIGURE 4

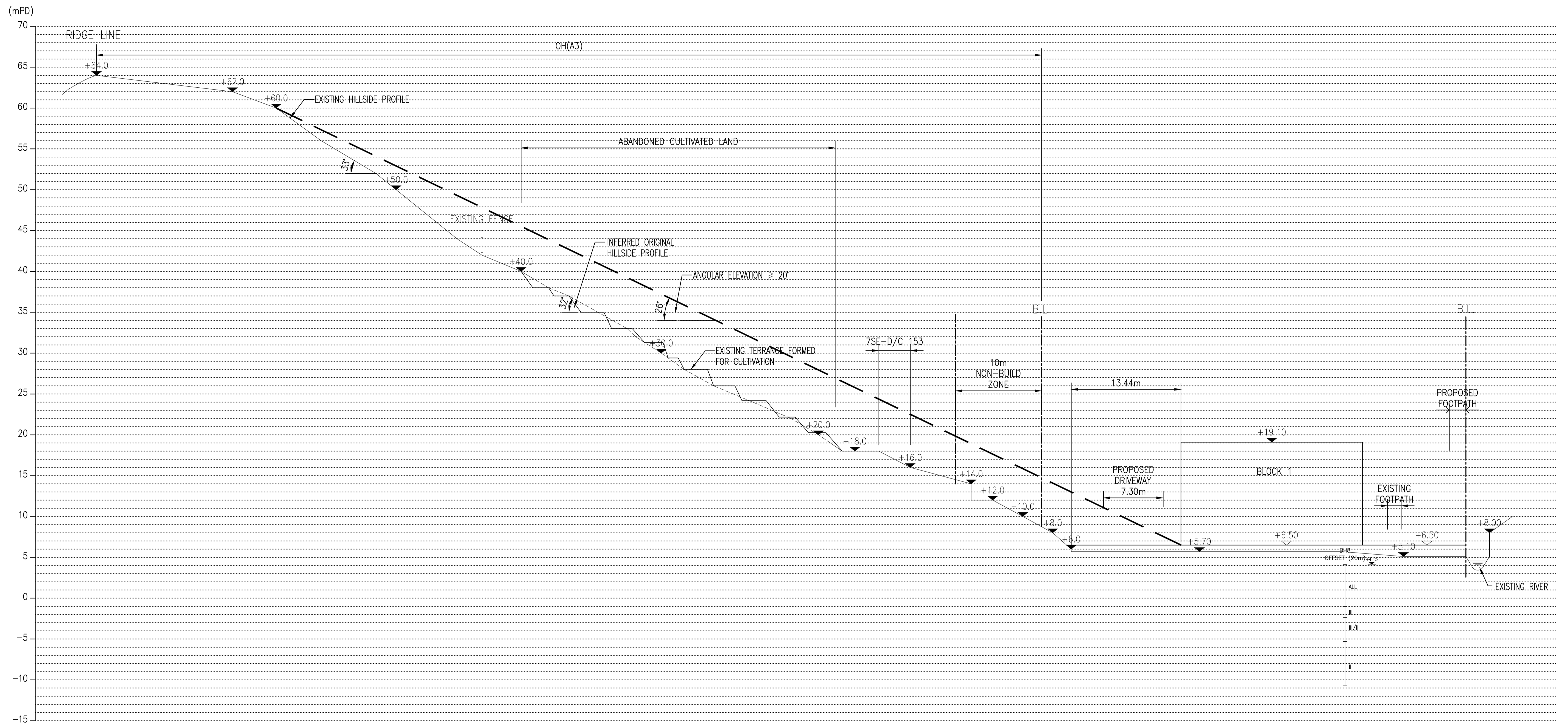
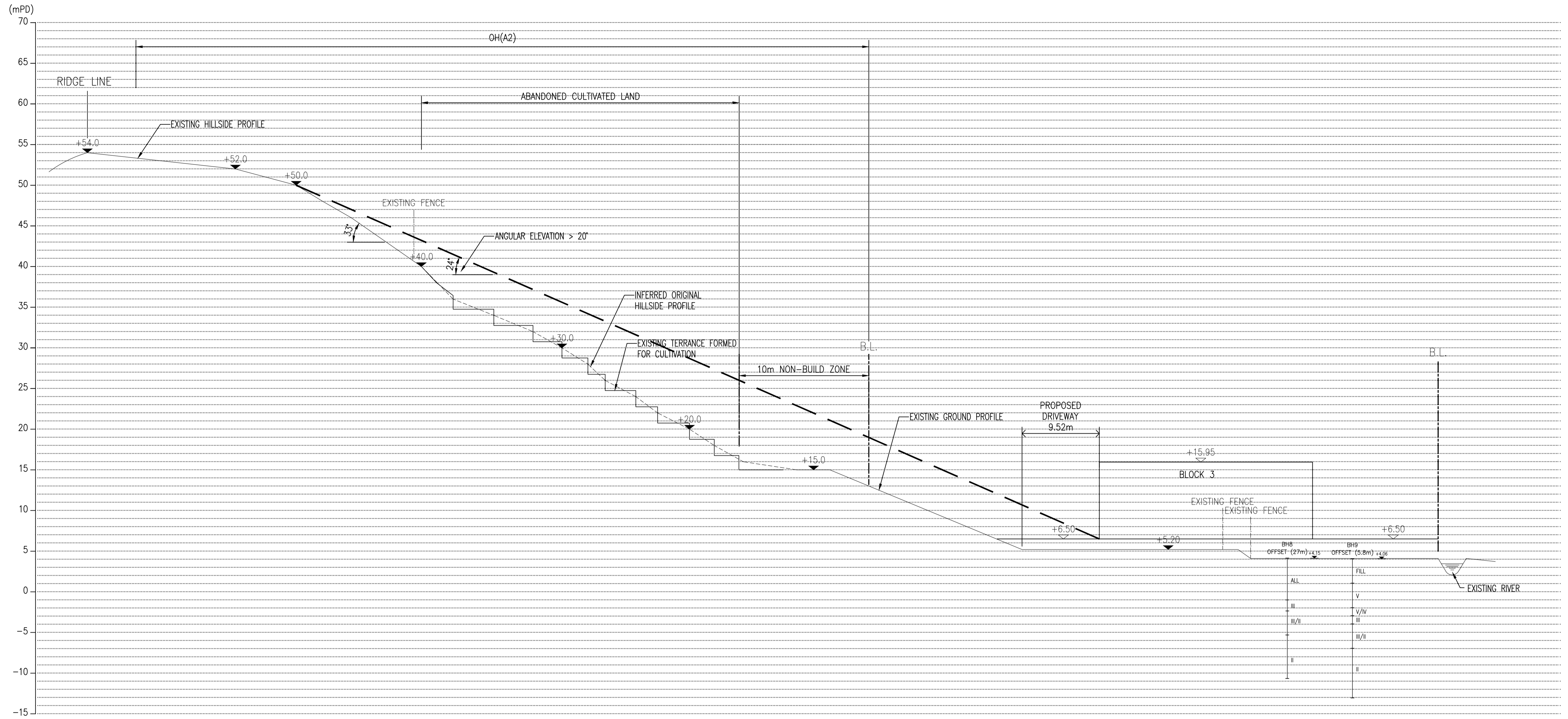
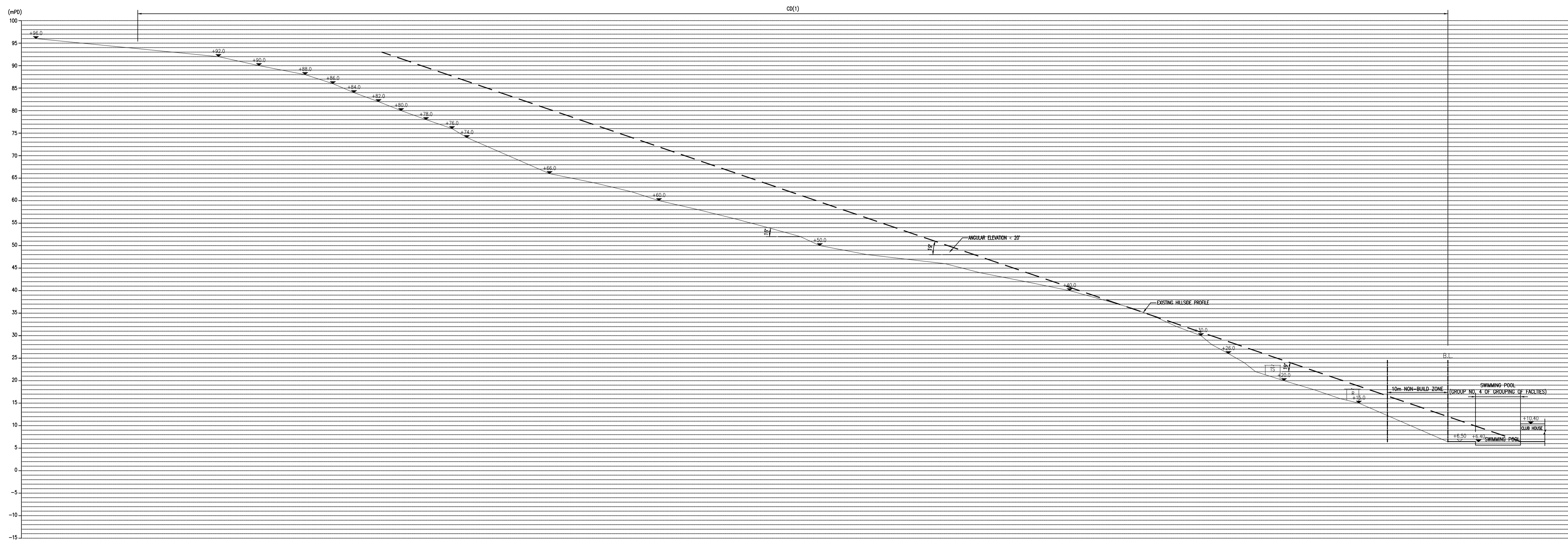


FIGURE 5



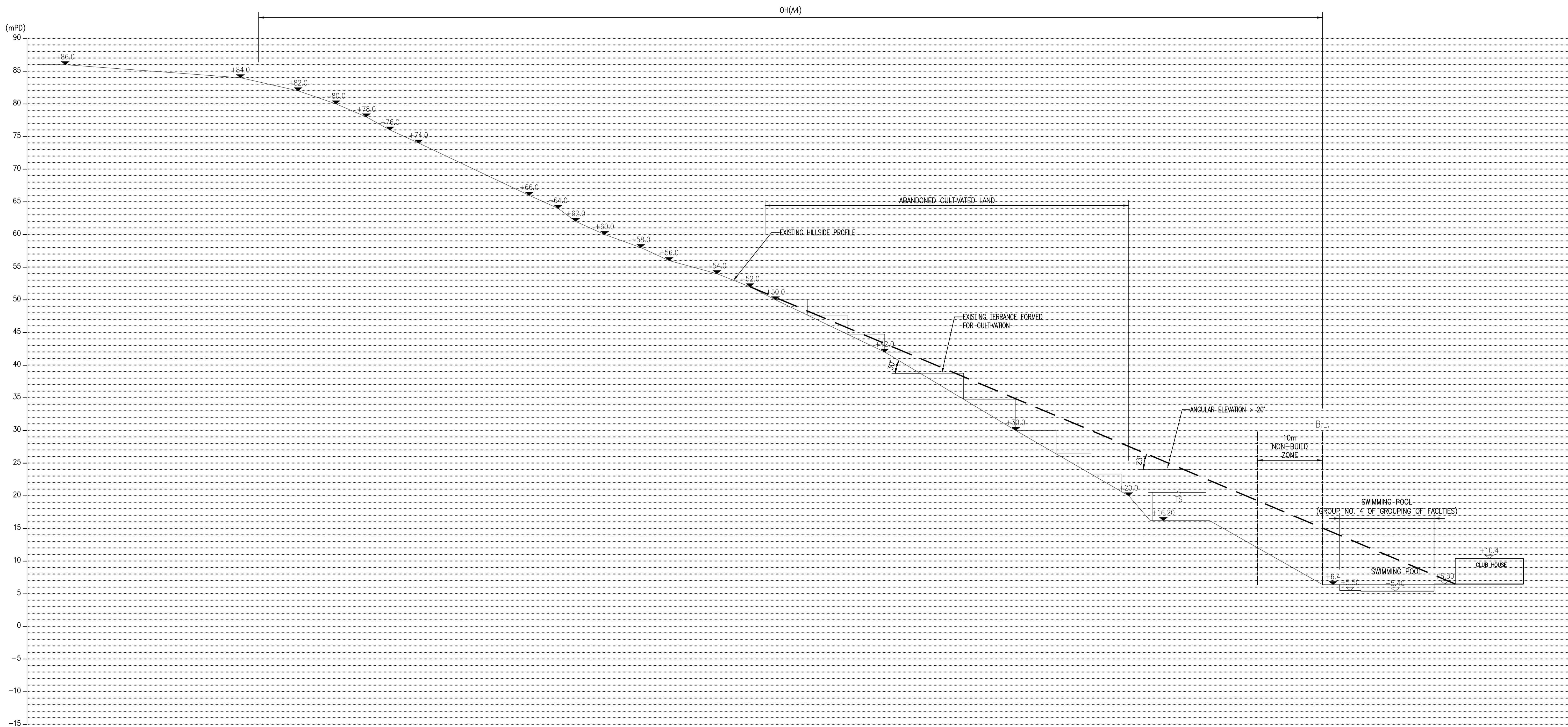
SECTION B-B
1 : 500

FIGURE 6



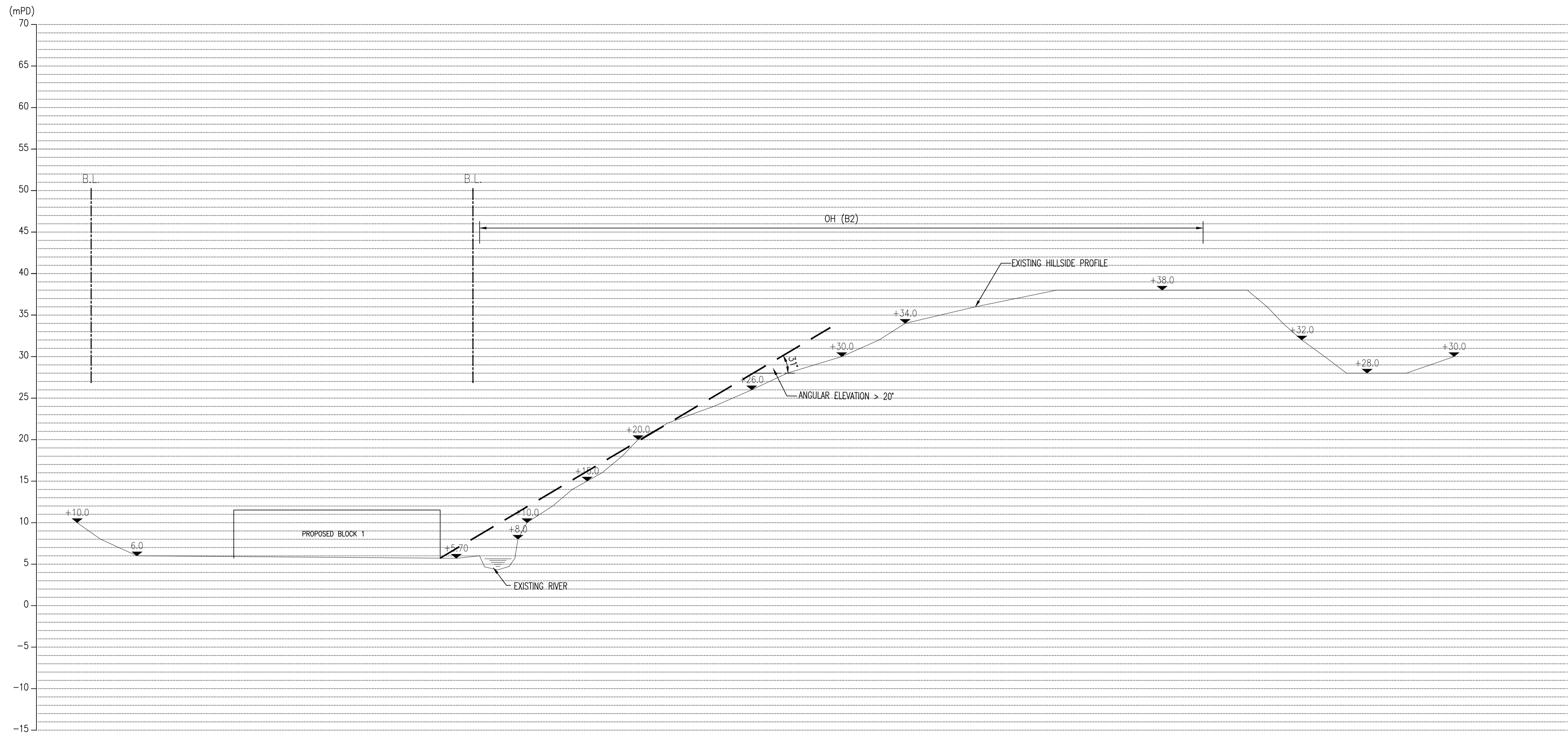
SECTION C-C
1 : 600

FIGURE 7



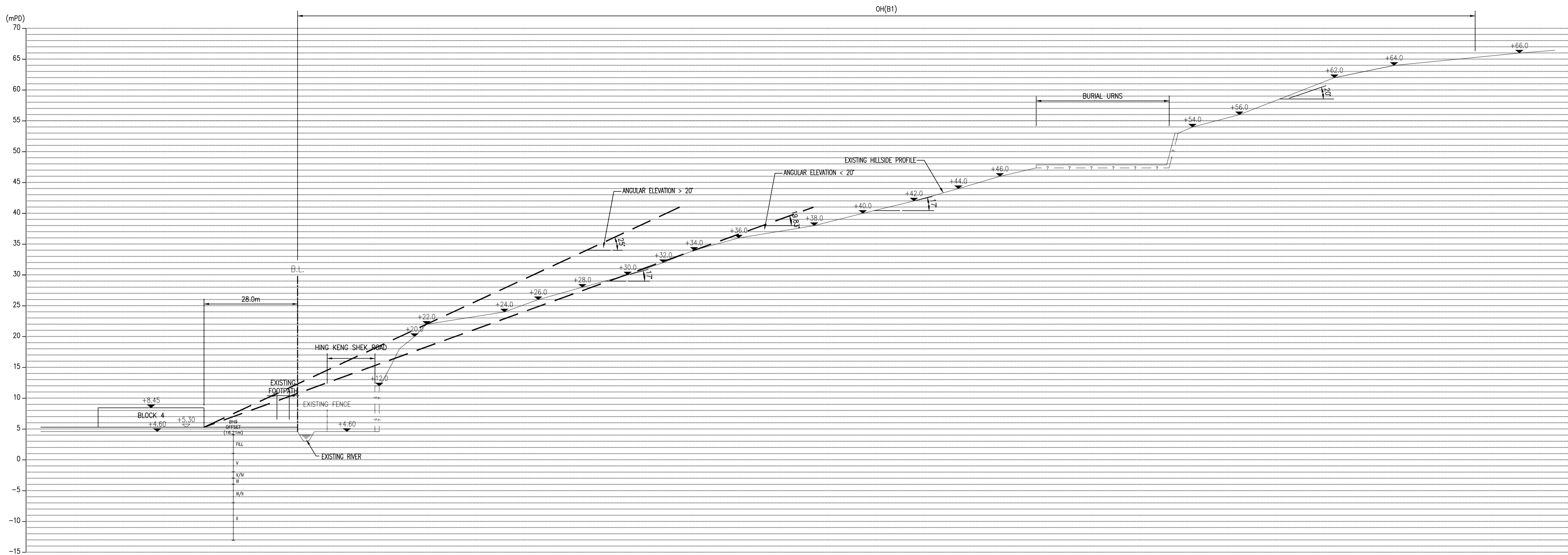
SECTION D-D
1 : 500

FIGURE 8



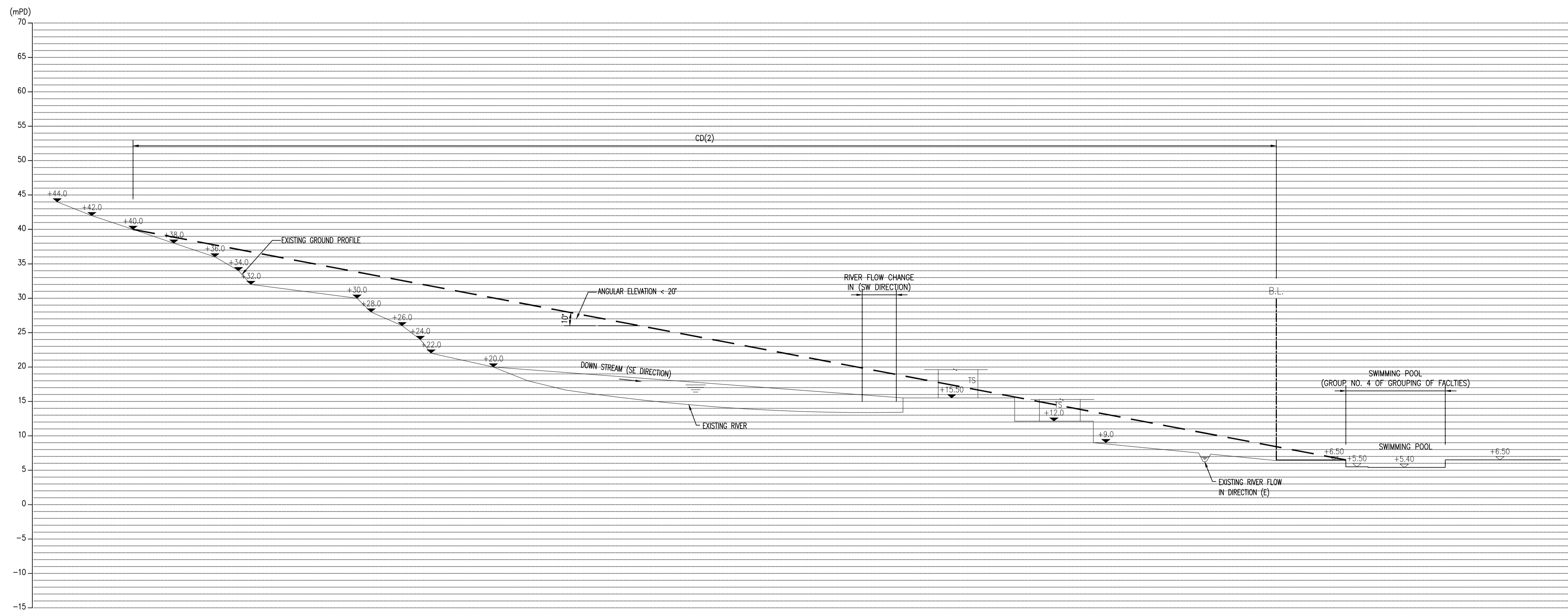
SECTION E-E
1 : 500

FIGURE 9



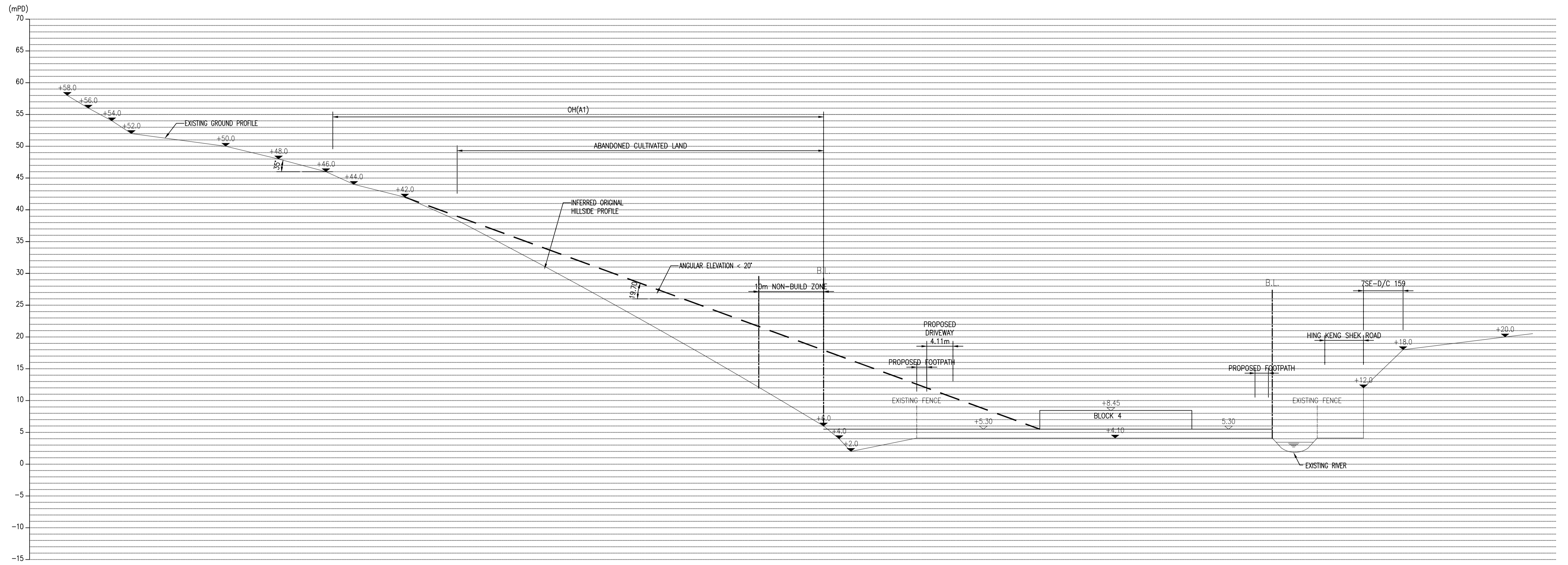
SECTION F-F
1 : 600

FIGURE 10



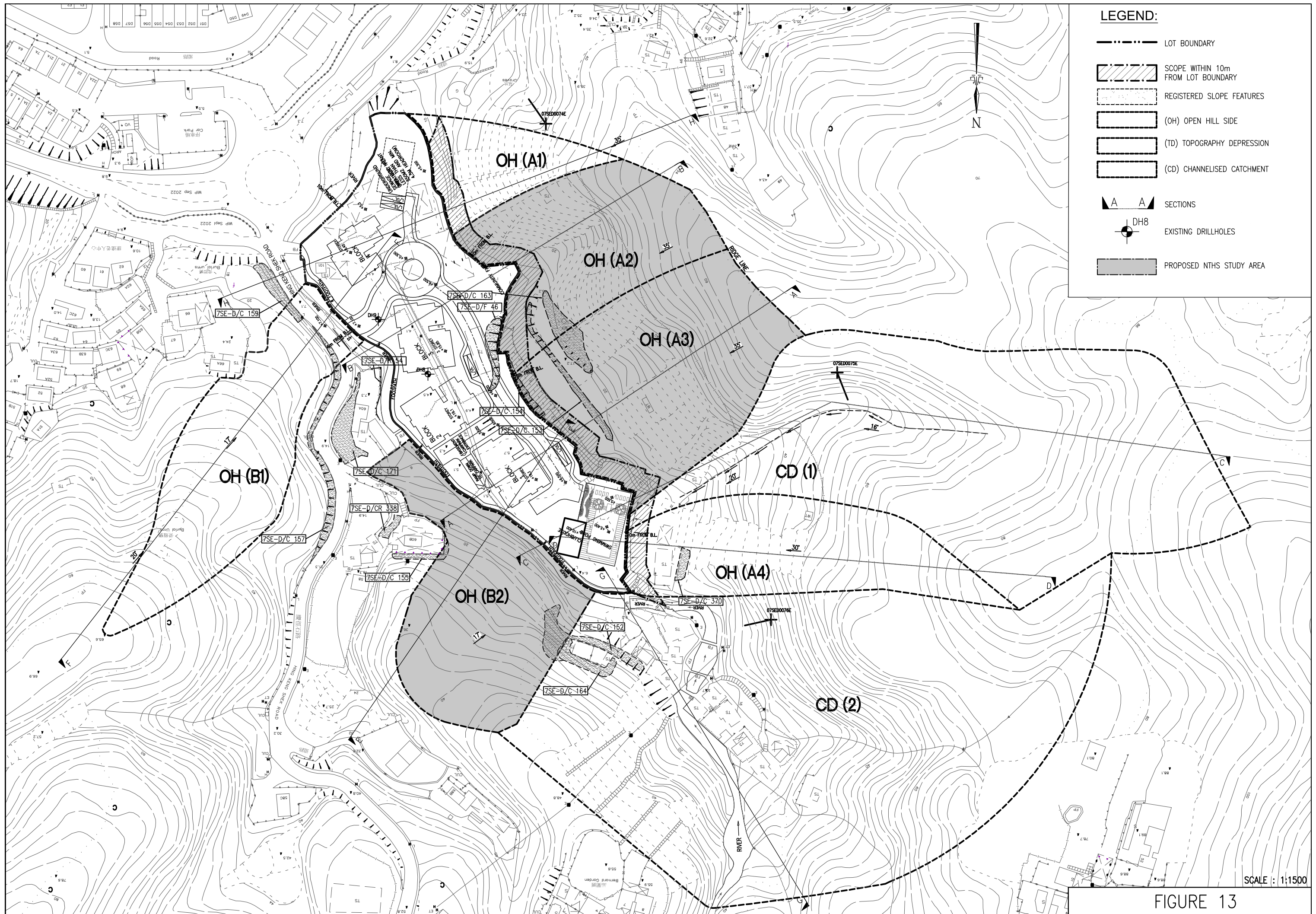
SECTION G-G
1 : 600

FIGURE 11



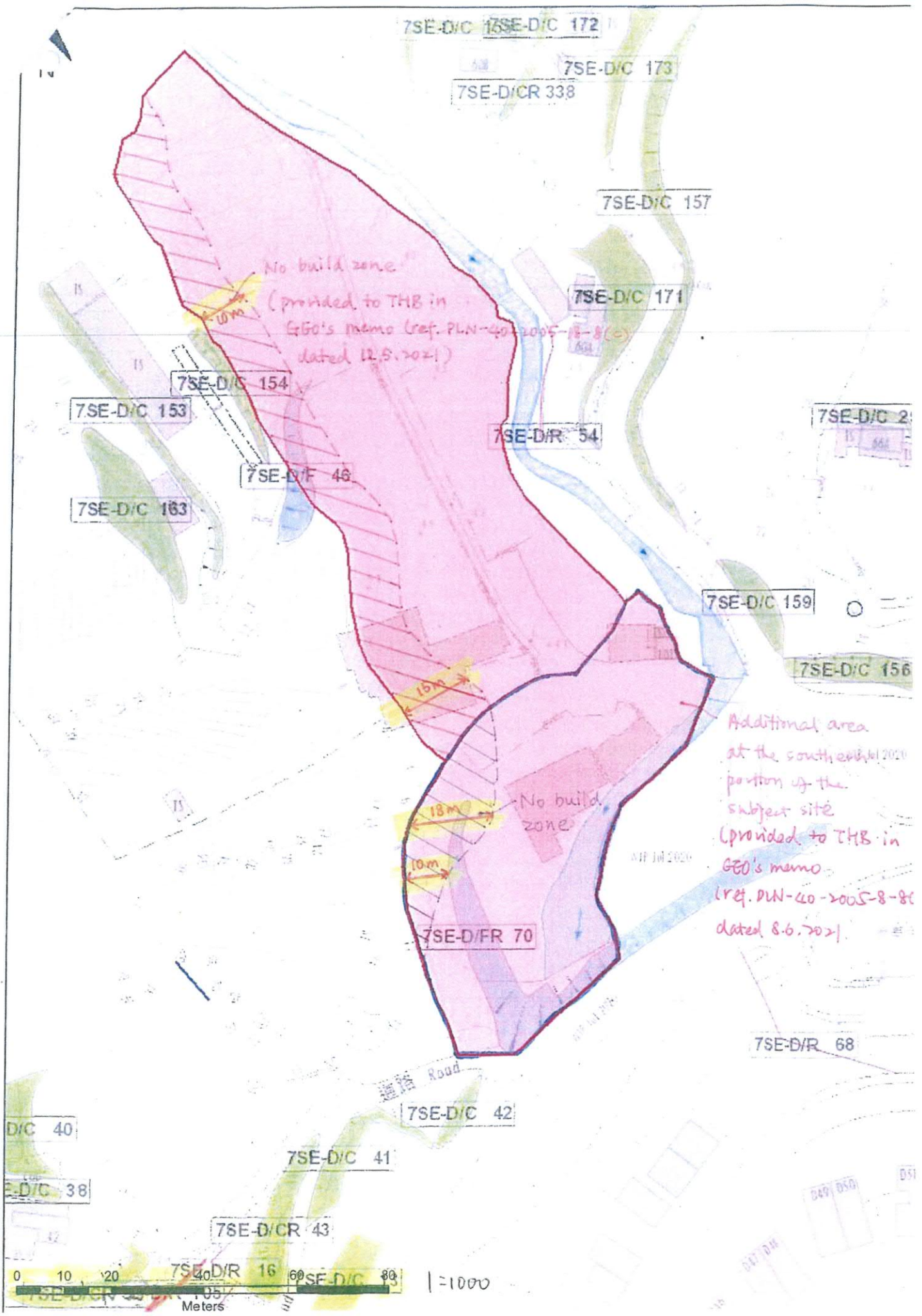
SECTION H-H
1 : 600

FIGURE 12



Appendix A

Information of Existing Slopes/ Retaining Walls



No build zone
 (provided to THB in
 GEO's memo (ref. PLN-40-2005-8-8(c)
 dated 12.5.2021)

No build
 zone

Additional area
 at the south end of
 portion of the
 subject site
 (provided to THB in
 GEO's memo
 (ref. PLN-40-2005-8-8(c)
 dated 8.6.2021)

D/C 40

D/C 38

0 10 20

7SE-D/R 16

60 SE-D/C 88

1:1000

Meters

7SE-D/C 42

7SE-D/C 41

7SE-D/CR 43

7SE-D/FR 70

7SE-D/R 68

7SE-D/C 156

7SE-D/C 159

7SE-D/R 54

7SE-D/C 163

7SE-D/F 46

7SE-D/C 153

7SE-D/C 154

7SE-D/C 171

7SE-D/C 157

7SE-D/CR 338

7SE-D/C 173

7SE-D/C 172



BASIC INFORMATION

Location: Southern part of Hing Keng Shek Village, Sai Kung
Registration Date: 19-12-1997
Ranking Score (NPRS): 0 (EI)
Date of Formation: pre-1977
Date of Construction/ Modification:
Data Source: Project Office
Approximate Coordinates: Easting : 844005 Northing : 824697

CONSEQUENCE-TO-LIFE CATEGORY

Facility at Crest: Remote area or abandoned facilities
Distance of Facility from Crest (m): 0
Facility at Toe: Remote area or abandoned facilities
Distance of Facility from Toe (m): 0
Consequence-to-life Category: 3
Remarks: N/A

SLOPE PART

(1) Max. Height (m): 4 Length (m): 35 Average Angle (deg): 40

WALL PART

N/A

MAINTENANCE RESPONSIBILITY

(1) Sub Div.: 1 Mixed Feature Party: Lands D Agent: Lands D Land Cat.: 1,5b(vi),7 Reason Code: 62,90 MR Endorsement Date: 05-05-1998
(2) Sub Div.: 2 Mixed Feature Party: DD210 Lot31 Agent: N/A Land Cat.: 1,5b(vi),7 Reason Code: 1 MR Endorsement Date: 05-05-1998

DETAILS OF SLOPE / RETAINING WALL

Date of Inspection: 04-12-2018
Data Source: Project Office
Slope Part Drainage: N/A
Wall Part Drainage: N/A

SLOPE PART



Slope Part (1)

Surface Protection (%): Bare: 0 Vegetated: 100 Chunam: 0 Shotcrete: 0 Other Cover: 0
Material Description: Material type: Soil Geology: N/A
Berm: No. of Berms: N/A Min. Berm Width (m): N/A
Weepholes: Size (mm): N/A Spacing (m): N/A

WALL PART

N/A

SERVICES

N/A

CHECKING STATUS INFORMATION

N/A

BACKGROUND INFORMATION

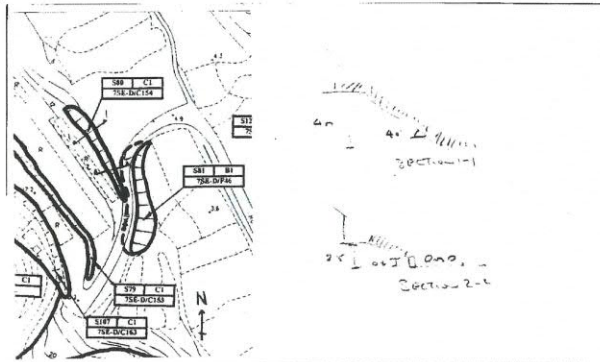
GIU Cell Ref.: 7SE25A9
Map Sheet Reference (1:1000): 7SE-25A
Aerial Photos: 9301 (1974), 9302 (1974)
Nearest Rainguage Station (Station Number): Pak Kong Tsui Hang Special Area Management Centre(N50)
Data Collected On: 04-12-2018
Date of Construction, Subsequent Modification and Demolition: Modification: Constructed Before: 1974 After: 1968
Related Reports/Files or Documents: N/A
Remarks: N/A
Follow Up Actions: N/A
DH-Order (To Be Confirmed with Buildings Department): None
Advisory Letter (To Be Confirmed with Buildings Department): None
LPMIS: None

ENHANCED MAINTENANCE INFORMATION

From Maintenance Department: (Last Updated Date: 25/09/2025)

STAGE 1 STUDY REPORT

Inspected On: 13-03-1997
 Weather: Mainly Fine
 District: ME



Section No: 1-1
 Height(m): H1 : 4 , H2 : 0
 Type of Toe Facility: Remote area or abandoned facilities
 Distance from Toe(m): 0
 Type of Crest Facility: Remote area or abandoned facilities
 Distance from Crest(m): 0
 Consequence Category: 3
 Engineering Judgement: U
 Section No: 2-2
 Type of Toe Facility: N/A
 Distance from Toe(m): 0
 Type of Crest Facility: N/A
 Distance from Crest(m): 0
 Consequence Category: 3
 Engineering Judgement: U
 Sign of Seepage: Slope : No signs of seepage
 Wall : N/A
 Criterion A satisfied: N
 Sign of Distress: Slope : Minor (mid-portion)
 Wall : N/A
 Criterion D satisfied: N
 Non-routine maintenance required: N
 Note: N/A
 Masonry wall/Masonry facing: N
 Note: N/A
 Consequence category (for critical section): 3
 Observations: N/A



Emergency Action Required: N
Action By: N/A

ACTION TO INITIATE PREVENTIVE WORKS

Criterion A/Criterion D: N/A
Action By: N/A
Further Study: N
Action By: N/A

OTHER EXTERNAL ACTION

Check / repair Services: N
Action By: N/A
Non-routine Maintenance: N
Action By: N/A

PHOTO





BASIC INFORMATION

Location: SOUTHERN PART OF HING KENG SHEK VILLAGE
 Registration Date: 19-12-1997
 Ranking Score (NPRS): 0 (Notional)
 Date of Formation: pre-1977
 Date of Construction/ Modification:
 Data Source: SIRST
 Approximate Coordinates: Easting : 844019 Northing : 824678

CONSEQUENCE-TO-LIFE CATEGORY

Facility at Crest: Road/footpath with very low traffic density
 Distance of Facility from Crest (m): 0
 Facility at Toe: Remote area or abandoned facilities
 Distance of Facility from Toe (m): 0
 Consequence-to-life Category: 3
 Remarks: N/A

SLOPE PART

(1) Max. Height (m): 5 Length (m): 35 Average Angle (deg): 40

WALL PART

N/A

MAINTENANCE RESPONSIBILITY

(1) Sub Div.: 1 Mixed Feature Party: DD210 LOT31 Agent: N/A Land Cat.: 1 Reason Code: 1 MR Endorsement Date: 15-03-2001
 (2) Sub Div.: 2 Mixed Feature Party: Lands D Agent: Lands D Land Cat.: 5b(vi) Reason Code: 62 MR Endorsement Date: 15-03-2001

DETAILS OF SLOPE / RETAINING WALL

Date of Inspection: 13-03-1997
 Data Source: SIRST
 Slope Part Drainage: N/A
 Wall Part Drainage: N/A

SLOPE PART

Slope Part (1)
 Surface Protection (%): Bare: 0 Vegetated: 100 Chunam: 0 Shotcrete: 0 Other Cover: 0
 Material Description: Material type: Soil Geology: N/A
 Berm: No. of Berms: N/A Min. Berm Width (m): N/A
 Weepholes: Size (mm): N/A Spacing (m): N/A



WALL PART

N/A

SERVICES

N/A

CHECKING STATUS INFORMATION

N/A

BACKGROUND INFORMATION

GIU Cell Ref.:	7SE25A9
Map Sheet Reference (1:1000):	7SE-25A
Aerial Photos:	9301 (1974), 9302 (1974)
Nearest Rainguage Station (Station Number):	Pak Kong Tsui Hang Special Area Management Centre(N50)
Data Collected On:	13-03-1997
Date of Construction, Subsequent Modification and Demolition:	Modification: Constructed Before: 1974 After: 1968
Related Reports/Files or Documents:	N/A
Remarks:	N/A
Follow Up Actions:	N/A
DH-Order (To Be Confirmed with Buildings Department):	None
Advisory Letter (To Be Confirmed with Buildings Department):	None
LPMIS:	None

ENHANCED MAINTENANCE INFORMATION

From Maintenance Department: (Last Updated Date: 25/09/2025)



STAGE 1 STUDY REPORT

Inspected On:
 Weather:
 District: ME
 Section No: 1-1
 Height(m):
 Type of Toe Facility: Remote area or abandoned facilities
 Distance from Toe(m): 0
 Type of Crest Facility: Road/footpath with very low traffic density
 Distance from Crest(m): 0
 Consequence Category:
 Engineering Judgement:
 Section No: 2-2
 Type of Toe Facility:
 Distance from Toe(m):
 Type of Crest Facility:
 Distance from Crest(m):
 Consequence Category:
 Engineering Judgement:
 Sign of Seepage:
 Criterion A satisfied:
 Sign of Distress:
 Criterion D satisfied:
 Non-routine maintenance required:
 Note:
 Masonry wall/Masonry facing:
 Note:
 Consequence category (for critical section):
 Observations: N/A
 Emergency Action Required:
 Action By: N/A

ACTION TO INITIATE PREVENTIVE WORKS

Criterion A/Criterion D: N/A
 Action By: N/A
 Further Study:
 Action By: N/A

OTHER EXTERNAL ACTION

Check / repair Services:
 Action By: N/A
 Non-routine Maintenance:



Action By:

N/A



PHOTO



7SE-D/F46

General View



BASIC INFORMATION

(OBSOLETE
FEATURE)

Location: Hiram's Highway

Date of Formation: post-1977

Date of Construction/
Modification:

Approximate Coordinates: Easting : 844090 Northing : 824574

CONSEQUENCE-TO-LIFE CATEGORY

Facility at Crest: Road/footpath with heavy traffic density

Distance of Facility from Crest (m): 0
Facility at Toe: Horticulture garden

Distance of Facility from Toe (m): 0
Consequence-to-life Category: 2
Remarks: N/A

SLOPE PART

(1) Max. Height (m): 4 Length (m): 140 Average Angle (deg): 30

WALL PART

(1) Max. Height (m): 2 Length (m): 58 Face Angle (deg): 90



MAINTENANCE RESPONSIBILITY

Government Feature Party: HyD Agent: HyD

DETAILS OF SLOPE / RETAINING WALL

Date of Inspection: 18-06-2010
Data Source: EI(HyD)
Slope Part Drainage: N/A

Wall Part Drainage: N/A

SLOPE PART

Slope Part (1)
Surface Protection (%): Bare: 0 Vegetated: 100 Chunam: 0 Shotcrete: 0 Other Cover: 0
Material Description: Material type: Soil Geology: N/A
Berm: No. of Berms: N/A Min. Berm Width (m): N/A
Weepholes: Size (mm): N/A Spacing (m): N/A



WALL PART

Wall Part (1)

Type of Wall: Wall Material: Concrete Wall Location: Wall at toe
Berm: No. of Berms: N/A Min. Berm Width (m): N/A
Weepholes: Size (mm): N/A Spacing (m): N/A

SERVICES

- (1) Utilities Type: Cable Size(mm): 0 Location: On crest Remark: Size cannot be determined
- (2) Utilities Type: Electricity Size(mm): 0 Location: On crest Remark: Size cannot be determined
- (3) Utilities Type: Gas Size(mm): 0 Location: On crest Remark: Size cannot be determined
- (4) Utilities Type: Sewer/Drain Size(mm): 0 Location: On crest Remark: Size cannot be determined
- (5) Utilities Type: Water Main Size(mm): 100 Location: On crest Remark: N/A
- (6) Utilities Type: Water Main Size(mm): 150 Location: On crest Remark: N/A



STAGE 1 STUDY REPORT

Inspected On:

Weather:

District: ME

Section No: 1-1

Height(m):

Type of Toe Facility: Horticulture garden

Distance from Toe(m): 0

Type of Crest Facility: Road/footpath with heavy traffic density

Distance from Crest(m): 0

Consequence Category:

Engineering Judgement:

Section No: 2-2

Type of Toe Facility:

Distance from Toe(m):

Type of Crest Facility:

Distance from Crest(m):

Consequence Category:

Engineering Judgement:



Sign of Seepage:

Criterion A satisfied:

Sign of Distress:

Criterion D satisfied:

Non-routine maintenance required:

Note:

Masonry wall/Masonry facing:

Note:

Consequence category (for critical section):

Observations: N/A

Emergency Action Required:

Action By: N/A

ACTION TO INITIATE PREVENTIVE WORKS

Criterion A/Criterion D: N/A

Action By: N/A

Further Study:

Action By: N/A

OTHER EXTERNAL ACTION

Check / repair Services:

Action By: N/A

Non-routine Maintenance:

Action By: N/A



PHOTO





BASIC INFORMATION

Location: Southern part of Hing Keng Shek Village, Pak Sha Wan
 Registration Date: 19-12-1997
 Ranking Score (NPRS): 0 (EI)
 Date of Formation: pre-1977
 Date of Construction/ Modification:
 Data Source: Project Office
 Approximate Coordinates: Easting : 843982 Northing : 824693

CONSEQUENCE-TO-LIFE CATEGORY

Facility at Crest: Remote area or abandoned facilities
 Distance of Facility from Crest (m): 0
 Facility at Toe: Remote area or abandoned facilities
 Distance of Facility from Toe (m): 0
 Consequence-to-life Category: 3
 Remarks: N/A

SLOPE PART

(1) Max. Height (m): 3.5 Length (m): 75 Average Angle (deg): 60

WALL PART

N/A

MAINTENANCE RESPONSIBILITY

(1) Sub Div.: 0 Government Feature Party: Lands D Agent: Lands D Land Cat.: 5b(vi) Reason Code: 62 MR Endorsement Date: 01-09-1998

DETAILS OF SLOPE / RETAINING WALL

Date of Inspection: 04-12-2018
 Data Source: Project Office
 Slope Part Drainage: N/A
 Wall Part Drainage: N/A

SLOPE PART

Slope Part (1)
 Surface Protection (%): Bare: 0 Vegetated: 100 Chunam: 0 Shotcrete: 0 Other Cover: 0
 Material Description: Material type: Soil Geology: N/A
 Berm: No. of Berms: N/A Min. Berm Width (m): N/A
 Weepholes: Size (mm): N/A Spacing (m): N/A



WALL PART

N/A

SERVICES

N/A

CHECKING STATUS INFORMATION

N/A

BACKGROUND INFORMATION

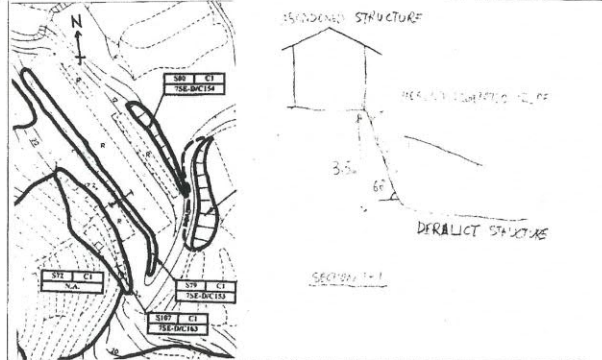
GIU Cell Ref.:	7SE25A8
Map Sheet Reference (1:1000):	7SE-25A
Aerial Photos:	N/A
Nearest Rainguage Station (Station Number):	Pak Kong Tsui Hang Special Area Management Centre(N50)
Data Collected On:	04-12-2018
Date of Construction, Subsequent Modification and Demolition:	N/A
Related Reports/Files or Documents:	N/A
Remarks:	N/A
Follow Up Actions:	N/A
DH-Order (To Be Confirmed with Buildings Department):	None
Advisory Letter (To Be Confirmed with Buildings Department):	None
LPMIS:	None

ENHANCED MAINTENANCE INFORMATION

From Maintenance Department: (Last Updated Date: 25/09/2025)

STAGE 1 STUDY REPORT

Inspected On: 13-03-1997
 Weather: Mainly Fine
 District: ME



Section No: 1-1
 Height(m): H1 : 4 , H2 : 0
 Type of Toe Facility: Remote area or abandoned facilities
 Distance from Toe(m): 0
 Type of Crest Facility: Remote area or abandoned facilities
 Distance from Crest(m): 0
 Consequence Category: 3
 Engineering Judgement: P
 Section No: 2-2
 Type of Toe Facility: N/A
 Distance from Toe(m): 0
 Type of Crest Facility: N/A
 Distance from Crest(m): 0
 Consequence Category: 3
 Engineering Judgement: P
 Sign of Seepage: Slope : No signs of seepage
 Wall : N/A
 Criterion A satisfied: N
 Sign of Distress: Slope : Reasonable (mid-portion)
 Wall : N/A
 Criterion D satisfied: N
 Non-routine maintenance required: N
 Note: N/A
 Masonry wall/Masonry facing: N
 Note: N/A
 Consequence category (for critical section): 3
 Observations: N/A
 Emergency Action Required: N
 Action By: N/A



ACTION TO INITIATE PREVENTIVE WORKS

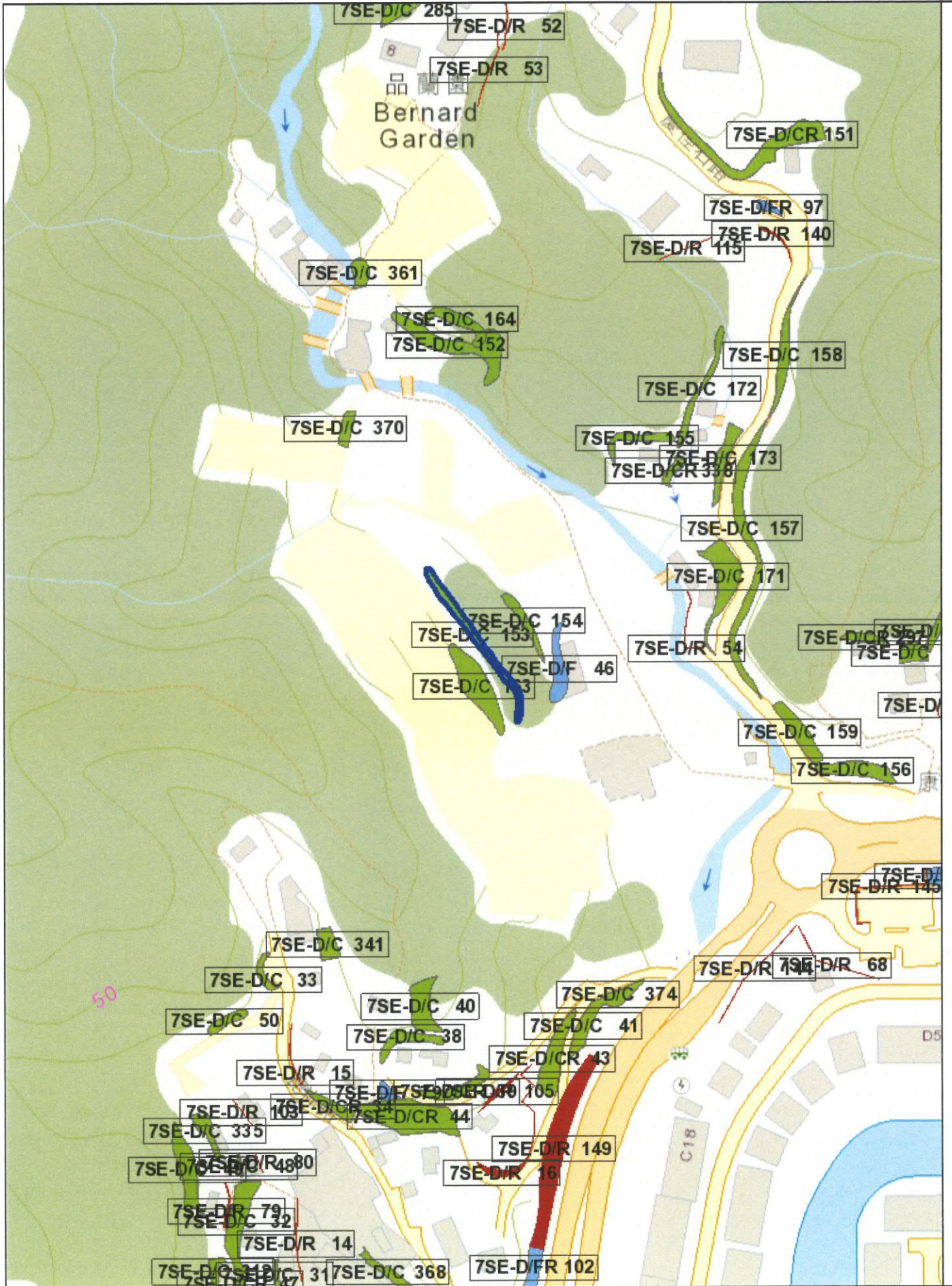
Criterion A/Criterion D:	N/A
Action By:	N/A
Further Study:	Y
Action By:	Mixed

OTHER EXTERNAL ACTION

Check / repair Services:	N
Action By:	N/A
Non-routine Maintenance:	N
Action By:	N/A

PHOTO







BASIC INFORMATION

Location: Hing Keng Shek Road, Sai Kung
Registration Date: 19-12-1997
Ranking Score (NPRS): 0 (EI)
Date of Formation: pre-1977
Date of Construction/ Modification: 17-03-2010
Data Source: Project Office
Approximate Coordinates: Easting : 844072 Northing : 824698

CONSEQUENCE-TO-LIFE CATEGORY

Facility at Crest: Cottage, licensed and squatter area
Distance of Facility from Crest (m): 3
Facility at Toe: Catchwater w/consequence on Group 5 facilities
Distance of Facility from Toe (m): 0
Consequence-to-life Category: 1
Remarks: N/A

SLOPE PART

N/A

WALL PART

(1) Max. Height (m): 4.4 Length (m): 30 Face Angle (deg): 85

MAINTENANCE RESPONSIBILITY

(1) Sub Div.: 0 Government Feature Party: Lands D Agent: Lands D Land Cat.: 5b(vi) Reason Code: 62 MR Endorsement Date: 22-03-2011

DETAILS OF SLOPE / RETAINING WALL

Date of Inspection: 12-02-2019
Data Source: Project Office
Slope Part Drainage: N/A
Wall Part Drainage: N/A

SLOPE PART

N/A

WALL PART



Wall Part (1)
Type of Wall: Wall Material: Concrete Wall Location: Retaining wall with level platform
Berm: No. of Berms: N/A Min. Berm Width (m): N/A
Weepholes: Size (mm): 75 Spacing (m): 1.5

SERVICES

N/A

CHECKING STATUS INFORMATION

Tagmark: SCS_14115 Part: 0 Checking Status: Feature modified/upgraded to current standard Checking Certificate No.: GEO/ME 085/2011

BACKGROUND INFORMATION

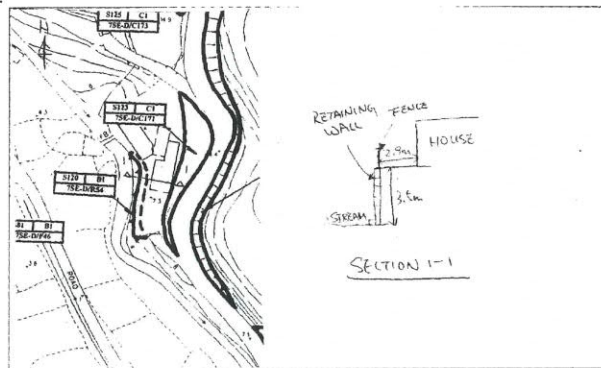
GIU Cell Ref.: 7SE25A9
Map Sheet Reference (1:1000): 7SE-25A
Aerial Photos: 9301 (1974), 9302 (1974)
Nearest Rainuage Station (Station Number): Pak Kong Tsui Hang Special Area Management Centre(N50)
Data Collected On: 12-02-2019
Date of Construction, Subsequent Modification and Demolition: Modification: Constructed Before: 1974 After: 1962
Related Reports/Files or Documents: N/A
Remarks: N/A
Follow Up Actions: N/A
DH-Order (To Be Confirmed with Buildings Department): None
Advisory Letter (To Be Confirmed with Buildings Department): None
LPMIS: None

ENHANCED MAINTENANCE INFORMATION

From Maintenance Department: (Last Updated Date: 25/09/2025)

STAGE 1 STUDY REPORT

Inspected On: 13-03-1997
 Weather: Mainly Fine
 District: ME



Section No: 1-1
 Height(m): H1 : 4 , H2 : 4
 Type of Toe Facility: Catchwater w/consequence on Group 5 facilities
 Distance from Toe(m): 0
 Type of Crest Facility: Cottage, licensed and squatter area
 Distance from Crest(m): 3
 Consequence Category: 1
 Engineering Judgement: P
 Section No: 2-2
 Type of Toe Facility:
 Distance from Toe(m):
 Type of Crest Facility:
 Distance from Crest(m):
 Consequence Category: 1
 Engineering Judgement: P
 Sign of Seepage: Slope : N/A
 Wall : Signs of seepage
 Criterion A satisfied: N
 Sign of Distress: Slope : N/A
 Wall : Moderate(mid-portion, at toe)
 Criterion D satisfied: N
 Non-routine maintenance required: N
 Note: N/A
 Masonry wall/Masonry facing: Y
 Note: N/A
 Consequence category (for critical section): 1
 Observations: N/A
 Emergency Action Required: N
 Action By: N/A



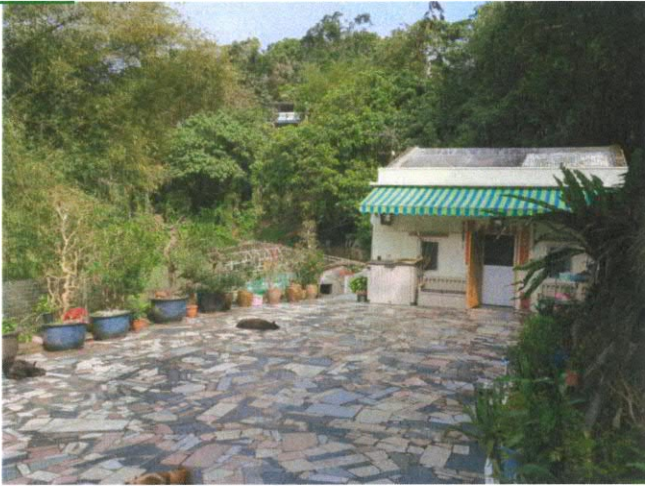
ACTION TO INITIATE PREVENTIVE WORKS

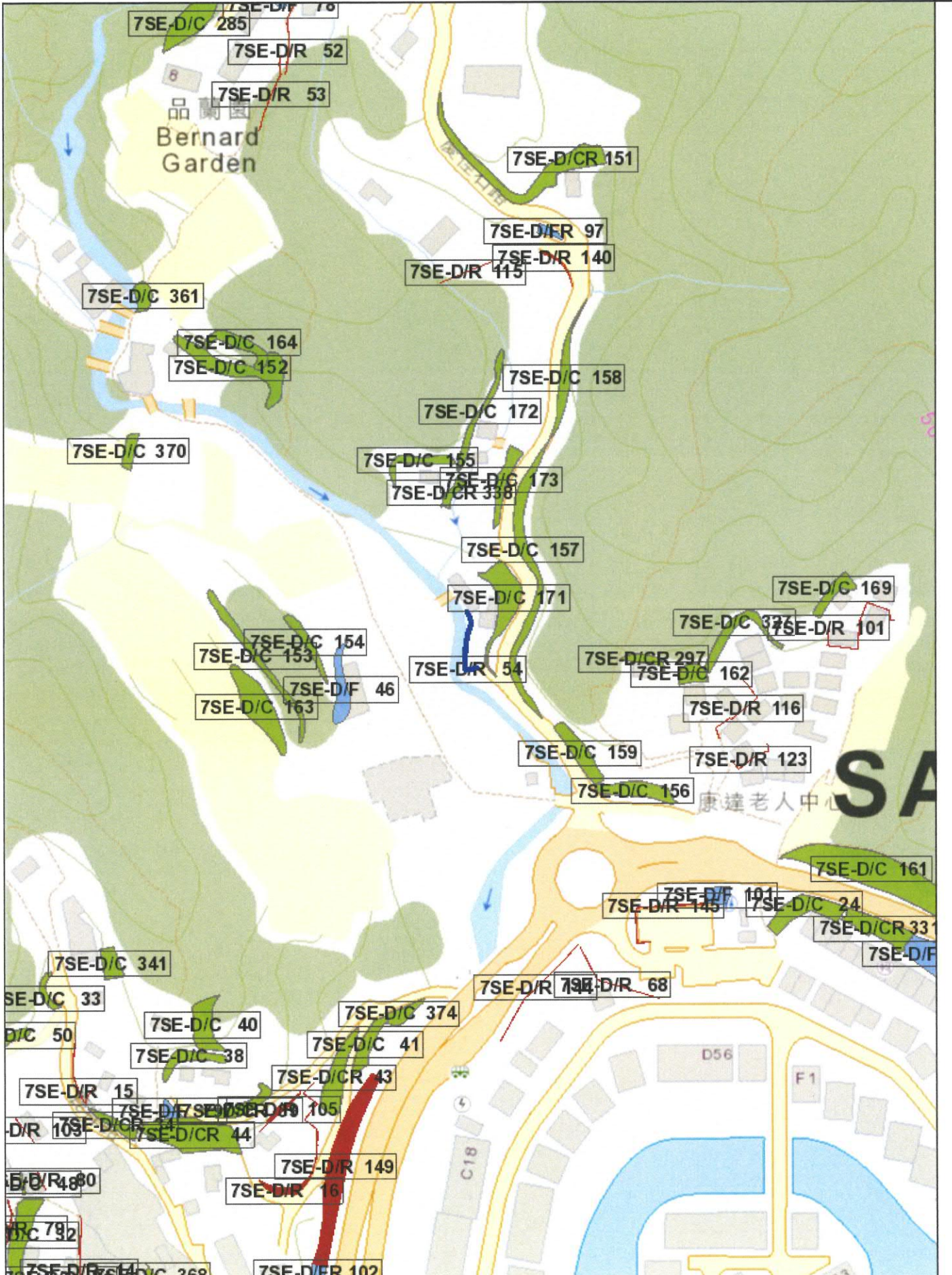
Criterion A/Criterion D:	N/A
Action By:	N/A
Further Study:	Y
Action By:	Mixed

OTHER EXTERNAL ACTION

Check / repair Services:	N
Action By:	N/A
Non-routine Maintenance:	N
Action By:	N/A

PHOTO







BASIC INFORMATION

Location: 60A Pak Wai, Sai Kung
 Date of Formation: pre-1977
 Date of Construction/ Modification: 20-12-2008
 Approximate Coordinates: Easting : 844088 Northing : 824712

CONSEQUENCE-TO-LIFE CATEGORY

Facility at Crest: Road/footpath with low traffic density
 Distance of Facility from Crest (m): 0
 Facility at Toe: Cottage, licensed and squatter area
 Distance of Facility from Toe (m): 0.5
 Consequence-to-life Category: 1
 Remarks: N/A

SLOPE PART

(1) Max. Height (m): 7.1 Length (m): 52 Average Angle (deg): 33

WALL PART

N/A

MAINTENANCE RESPONSIBILITY

Maintenance Type: Government Party: Lands D Agent: Lands D

DETAILS OF SLOPE / RETAINING WALL

Date of Inspection: 12-04-2019
 Data Source: Project Office
 Slope Part Drainage: (1) Position: Toe Size(mm): 300
 Wall Part Drainage: (1) Position: Crest Size(mm): 200

SLOPE PART

Slope Part (1)
 Surface Protection (%): Bare: 0 Vegetated: 60 Chunam: 0 Shotcrete: 40 Other Cover: 0
 Material Description: Material type: Soil Geology: N/A
 Berm: No. of Berms: N/A Min. Berm Width (m): N/A
 Weepholes: Size (mm): 75 Spacing (m): 1.5

WALL PART

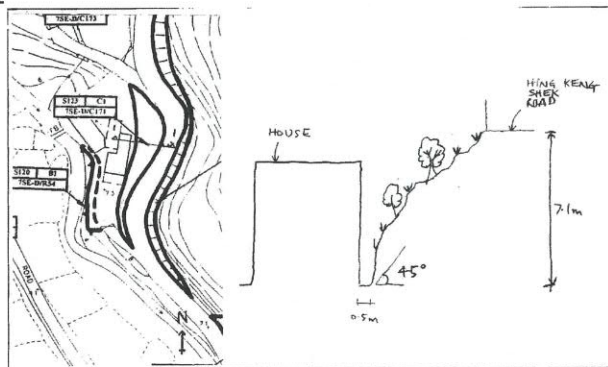
N/A

SERVICES

- (1) Utilities Type: Water Main Size (mm): 20 Location: On crest Remark: N/A
- (2) Utilities Type: Water Main Size (mm): 20 Location: On slope Remark: N/A

STAGE 1 STUDY REPORT

Inspected On: 13-03-1997
 Weather: Mainly Fine
 District: ME



Section No: 1-1
 Height (m): N/A
 Type of Toe Facility: Cottage, licensed and squatter area
 Distance from Toe (m): 0.5
 Type of Crest Facility: Road/footpath with low traffic density
 Distance from Crest (m): 0
 Consequence Category: 1
 Engineering Judgement: P
 Section No: 2-2
 Type of Toe Facility: N/A
 Distance from Toe (m): 0
 Type of Crest Facility: N/A
 Distance from Crest (m): 0
 Consequence Category: 1
 Engineering Judgement: P
 Sign of Seepage: Slope : Signs of seepage
 Wall : N/A
 Criterion A satisfied: N
 Sign of Distress: Slope : Reasonable (near crest, mid-portion)
 Wall : N/A
 Criterion D satisfied: N
 Non-routine maintenance required: N
 Note: N/A



Masonry wall / Masonry facing: N
Note: N/A
Consequence category (for critical section): 1
Observations: N/A
Emergency Action Required: N
Action By: N/A

ACTION TO INITIATE PREVENTIVE WORKS

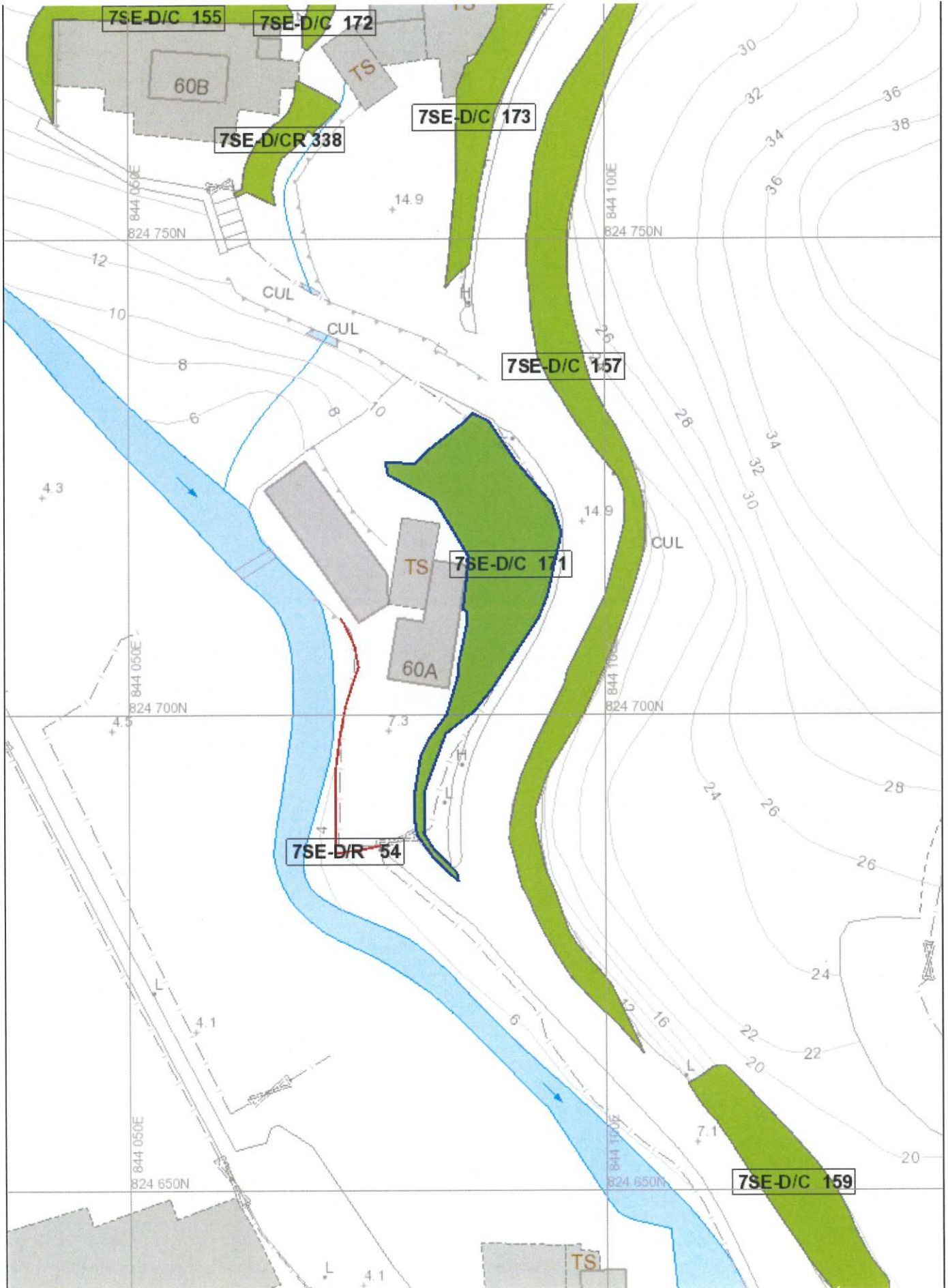
Criterion A / Criterion D: N/A
Action By: N/A
Further Study: Y
Action By: Mixed

OTHER EXTERNAL ACTION

Check / repair Services: N
Action By: N/A
Non-routine Maintenance: N
Action By: N/A

PHOTO







BASIC INFORMATION

Location: Southern part of Hing Keng Shek Village
 Registration Date: 19-12-1997
 Ranking Score (NPRS): 0 (EI)
 Date of Formation: pre-1977
 Date of Construction/ Modification:
 Data Source: Project Office
 Approximate Coordinates: Easting : 843985 Northing : 824668

CONSEQUENCE-TO-LIFE CATEGORY

Facility at Crest: Undeveloped green belt
 Distance of Facility from Crest (m): 0
 Facility at Toe: Lightly-used playground
 Distance of Facility from Toe (m): 1
 Consequence-to-life Category: 3
 Remarks: TGN 15 case - provided by SP Division (31 MAR 2004)

SLOPE PART

(1) Max. Height (m): 9 Length (m): 45 Average Angle (deg): 45

WALL PART

N/A

MAINTENANCE RESPONSIBILITY

(1) Sub Div.: 0 Government Feature Party: Lands D Agent: Lands D Land Cat.: 5b(vi) Reason Code: 62 MR Endorsement Date: 05-05-1998

DETAILS OF SLOPE / RETAINING WALL

Date of Inspection: 13-11-2008
 Data Source: Project Office
 Slope Part Drainage: N/A
 Wall Part Drainage: N/A

SLOPE PART

Slope Part (1)
 Surface Protection (%): Bare: 0 Vegetated: 100 Chunam: 0 Shotcrete: 0 Other Cover: 0
 Material Description: Material type: Soil Geology: Decomposed volcanic
 Berm: No. of Berms: N/A Min. Berm Width (m): N/A
 Weepholes: Size (mm): N/A Spacing (m): N/A



WALL PART

N/A

SERVICES

N/A

CHECKING STATUS INFORMATION

N/A

BACKGROUND INFORMATION

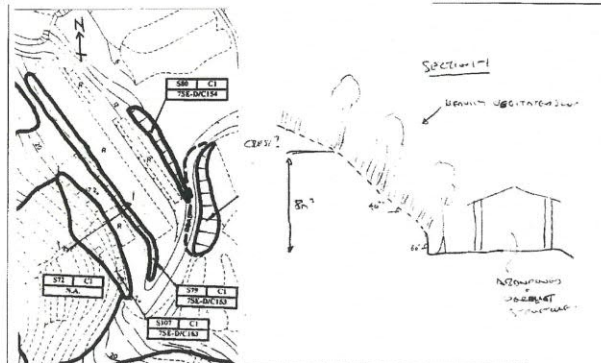
GIU Cell Ref.:	7SE25A8
Map Sheet Reference (1:1000):	7SE-25A
Aerial Photos:	9301 (1974), 9302 (1974)
Nearest Rainguage Station (Station Number):	Pak Kong Tsui Hang Special Area Management Centre(N50)
Data Collected On:	13-11-2008
Date of Construction, Subsequent Modification and Demolition:	Modification: Constructed Before: 1974 After: 1968
Related Reports/Files or Documents:	N/A
Remarks:	N/A
Follow Up Actions:	N/A
DH-Order (To Be Confirmed with Buildings Department):	None
Advisory Letter (To Be Confirmed with Buildings Department):	None
LPMIS:	None

ENHANCED MAINTENANCE INFORMATION

From Maintenance Department: (Last Updated Date: 25/09/2025)

STAGE 1 STUDY REPORT

Inspected On: 13-03-1997
 Weather: Mainly Fine
 District: ME



Section No: 1-1
 Height(m): H1 : 8 , H2 : 0
 Type of Toe Facility: Lightly-used playground
 Distance from Toe(m): 1
 Type of Crest Facility: Undeveloped green belt
 Distance from Crest(m): 0
 Consequence Category: 1
 Engineering Judgement: P
 Section No: 2-2
 Type of Toe Facility: N/A
 Distance from Toe(m): 0
 Type of Crest Facility: N/A
 Distance from Crest(m): 0
 Consequence Category: 1
 Engineering Judgement: P
 Sign of Seepage: Slope : No signs of seepage
 Wall : N/A
 Criterion A satisfied: N
 Sign of Distress: Slope : Minor (mid-portion)
 Wall : N/A
 Criterion D satisfied: N
 Non-routine maintenance required: N
 Note: N/A
 Masonry wall/Masonry facing: N
 Note: N/A
 Consequence category (for critical section): 1
 Observations: TGN 15 case - provided by SP Division (31 MAR 2004)
 Emergency Action Required: N
 Action By: N/A



ACTION TO INITIATE PREVENTIVE WORKS

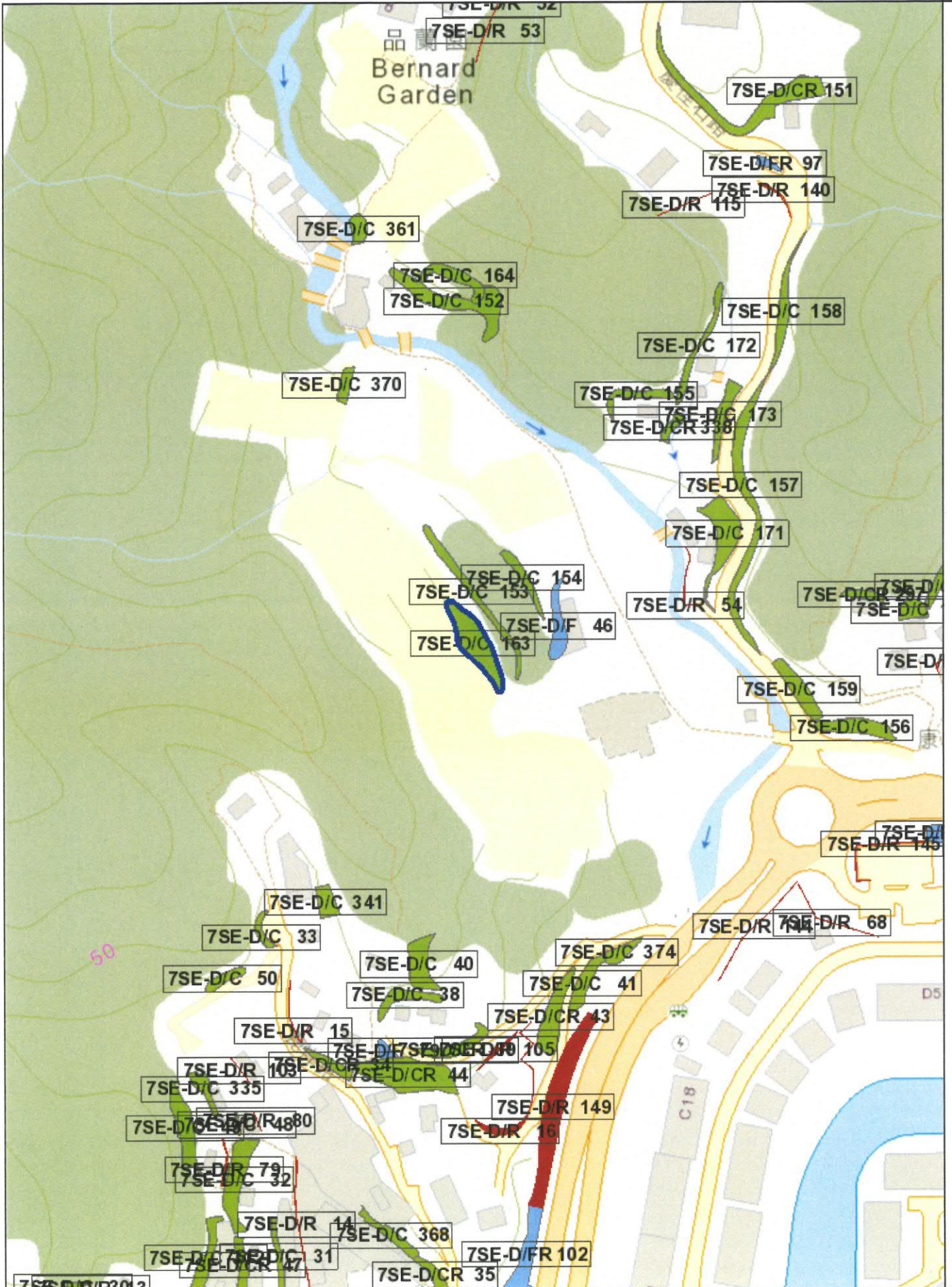
Criterion A/Criterion D:	N/A
Action By:	N/A
Further Study:	Y
Action By:	Mixed

OTHER EXTERNAL ACTION

Check / repair Services:	N
Action By:	N/A
Non-routine Maintenance:	N
Action By:	N/A

PHOTO







BASIC INFORMATION

Location: Northwest of Marina Cove and West of Hing Keng Shek Road, Southwest Sai Kung
 Registration Date: 15-05-2019
 Ranking Score (NPRS): 17 (LPMit)
 Date of Formation: post-1977
 Date of Construction/ Modification:
 Data Source: Project Office
 Approximate Coordinates: Easting : 843930 Northing : 824779

CONSEQUENCE-TO-LIFE CATEGORY

Facility at Crest: Undeveloped green belt
 Distance of Facility from Crest (m): 0
 Facility at Toe: Cottage, licensed and squatter area
 Distance of Facility from Toe (m): 0.5
 Consequence-to-life Category: 1
 Remarks: N/A

SLOPE PART

(1) Max. Height (m): 3.8 Length (m): 16 Average Angle (deg): 40

WALL PART

N/A

MAINTENANCE RESPONSIBILITY

(1) Sub Div.: 1 Mixed Feature Party: DD210 LOT 11 Agent: N/A Land Cat.: 1 Reason Code: 1 MR Endorsement Date: 19-10-2020
 (2) Sub Div.: 2 Mixed Feature Party: Lands D Agent: Lands D Land Cat.: 5b(vi) Reason Code: 62 MR Endorsement Date: 19-10-2020

DETAILS OF SLOPE / RETAINING WALL

Date of Inspection: 31-12-2018
 Data Source: Project Office
 Slope Part Drainage: N/A
 Wall Part Drainage: N/A

SLOPE PART

Slope Part (1)
 Surface Protection (%): Bare: 0 Vegetated: 100 Chunam: 0 Shotcrete: 0 Other Cover: 0
 Material Description: Material type: Soil Geology: N/A
 Berm: No. of Berms: 1 Min. Berm Width (m): 0.8
 Weepholes: Size (mm): N/A Spacing (m): N/A



WALL PART

N/A

SERVICES

N/A

CHECKING STATUS INFORMATION

N/A

BACKGROUND INFORMATION

GIU Cell Ref.:	N/A
Map Sheet Reference (1:1000):	N/A
Aerial Photos:	N/A
Nearest Rainguage Station (Station Number):	()
Data Collected On:	31-12-2018
Date of Construction, Subsequent Modification and Demolition:	N/A
Related Reports/Files or Documents:	N/A
Remarks:	N/A
Follow Up Actions:	N/A
DH-Order (To Be Confirmed with Buildings Department):	None
Advisory Letter (To Be Confirmed with Buildings Department):	None
LPMIS:	None

ENHANCED MAINTENANCE INFORMATION

From Maintenance Department: (Last Updated Date: 25/09/2025)



STAGE 1 STUDY REPORT

Inspected On:

Weather:

District: N/A

Section No: 1-1

Height(m):

Type of Toe Facility: Cottage, licensed and squatter area

Distance from Toe(m): 0.5

Type of Crest Facility: Undeveloped green belt

Distance from Crest(m): 0

Consequence Category:

Engineering Judgement:

Section No: 2-2

Type of Toe Facility:

Distance from Toe(m):

Type of Crest Facility:

Distance from Crest(m):

Consequence Category:

Engineering Judgement:

Sign of Seepage:

Criterion A satisfied:

Sign of Distress:

Criterion D satisfied:

Non-routine maintenance required:

Note:

Masonry wall/Masonry facing:

Note:

Consequence category (for critical section):

Observations: N/A

Emergency Action Required:

Action By: N/A

ACTION TO INITIATE PREVENTIVE WORKS

Criterion A/Criterion D: N/A

Action By: N/A

Further Study:

Action By: N/A

OTHER EXTERNAL ACTION

Check / repair Services:

Action By: N/A

Non-routine Maintenance:

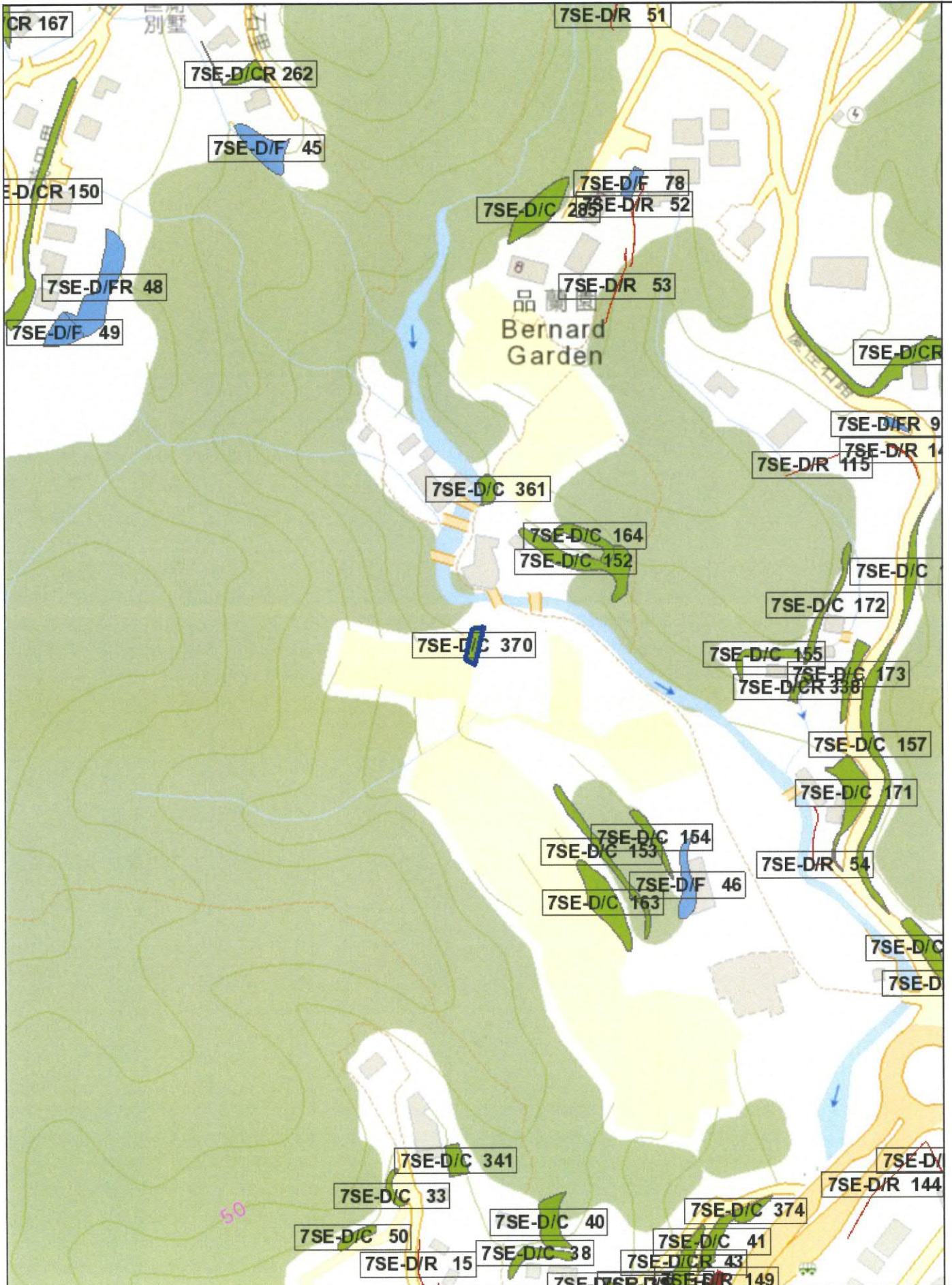


Action By:

N/A

PHOTO







BASIC INFORMATION

Location: HING KENG SHEK ROAD, SAI KUNG
 Date of Formation: pre-1977
 Date of Construction/ Modification: N/A
 Approximate Coordinates: Easting : 844098 Northing : 824731

CONSEQUENCE-TO-LIFE CATEGORY

Facility at Crest: Undeveloped green belt
 Distance of Facility from Crest (m): 0
 Facility at Toe: Road/footpath with low traffic density
 Distance of Facility from Toe (m): 0
 Consequence-to-life Category: 3
 Remarks: N/A

SLOPE PART

(1) Max. Height (m): 6 Length (m): 125 Average Angle (deg): 70

WALL PART

N/A

MAINTENANCE RESPONSIBILITY

Maintenance Type: Government Party: Lands D Agent: Lands D

DETAILS OF SLOPE / RETAINING WALL

Date of Inspection: 12-03-1997
 Data Source: SIRST
 Slope Part Drainage: (1) Position: Stepped Size(mm): 251
 Wall Part Drainage: N/A

SLOPE PART

Slope Part (1)
 Surface Protection (%): Bare: 20 Vegetated: 75 Chunam: 5 Shotcrete: 0 Other Cover: 0
 Material Description: Material type: Soil & Rock Geology: Decomposed volcanic
 Berm: No. of Berms: N/A Min. Berm Width (m): N/A
 Weepholes: Size (mm): N/A Spacing (m): N/A

WALL PART

N/A

SERVICES

N/A

STAGE 1 STUDY REPORT

Inspected On: 12-03-1997
 Weather: Mainly Fine
 District: ME



Section No: 1-1
 Height (m): N/A
 Type of Toe Facility: Road/footpath with low traffic density
 Distance from Toe (m): 0
 Type of Crest Facility: Undeveloped green belt
 Distance from Crest (m): 0
 Consequence Category: 3
 Engineering Judgement: P
 Section No: 2-2
 Type of Toe Facility: N/A
 Distance from Toe (m): 0
 Type of Crest Facility: N/A
 Distance from Crest (m): 0
 Consequence Category: 3
 Engineering Judgement: P
 Sign of Seepage: Slope : No signs of seepage
 Wall : N/A
 Criterion A satisfied: N
 Sign of Distress: Slope : Minor (mid-portion, at toe)
 Wall : N/A
 Criterion D satisfied: N
 Non-routine maintenance required: N
 Note: N/A
 Masonry wall / Masonry facing: N



Note: N/A
Consequence category (for critical section): 3
Observations: N/A
Emergency Action Required: N
Action By: N/A

ACTION TO INITIATE PREVENTIVE WORKS

Criterion A / Criterion D: N/A
Action By: N/A
Further Study: Y
Action By: Mixed

OTHER EXTERNAL ACTION

Check / repair Services: N
Action By: N/A
Non-routine Maintenance: N
Action By: N/A

PHOTO



7SE-D/C157

GENERAL VIEW





BASIC INFORMATION

Location: ADJACENT HING KENG SHEK ROAD, PAK WAI VILLAGE, SAI KUNG.
Date of Formation: pre-1977
Date of Construction/ Modification: N/A
Approximate Coordinates: Easting : 844120 Northing : 824650

CONSEQUENCE-TO-LIFE CATEGORY

Facility at Crest: Undeveloped green belt
Distance of Facility from Crest (m): 0
Facility at Toe: Road/footpath with very low traffic density
Distance of Facility from Toe (m): 0
Consequence-to-life Category: 3
Remarks: N/A

SLOPE PART

(1) Max. Height (m): 6 Length (m): 28 Average Angle (deg): 60

WALL PART

N/A

MAINTENANCE RESPONSIBILITY

Maintenance Type: Government Party: Lands D Agent: Lands D

DETAILS OF SLOPE / RETAINING WALL

Date of Inspection: 14-04-2025
Data Source: Project Office
Slope Part Drainage: N/A
Wall Part Drainage: N/A

SLOPE PART

Slope Part (1)
Surface Protection (%): Bare: 0 Vegetated: 0 Chunam: 0 Shotcrete: 100 Other Cover: 0
Material Description: Material type: Soil & Rock Geology: N/A
Berm: No. of Berms: N/A Min. Berm Width (m): N/A
Weepholes: Size (mm): N/A Spacing (m): N/A

WALL PART

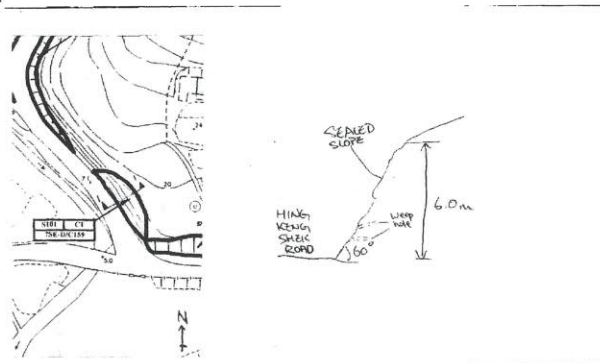
N/A

SERVICES

N/A

STAGE 1 STUDY REPORT

Inspected On: 13-03-1997
 Weather: Mainly Fine
 District: ME



Section No: 1-1
 Height (m): N/A
 Type of Toe Facility: Road/footpath with very low traffic density
 Distance from Toe (m): 0
 Type of Crest Facility: Undeveloped green belt
 Distance from Crest (m): 0
 Consequence Category: 3
 Engineering Judgement: P
 Section No: 2-2
 Type of Toe Facility: N/A
 Distance from Toe (m): 0
 Type of Crest Facility: N/A
 Distance from Crest (m): 0
 Consequence Category: 3
 Engineering Judgement: P
 Sign of Seepage: Slope : No signs of seepage
 Wall : N/A
 Criterion A satisfied: N
 Sign of Distress: Slope : N/A
 Wall : N/A
 Criterion D satisfied: N
 Non-routine maintenance required: N
 Note: N/A
 Masonry wall / Masonry facing: N

Note: N/A
Consequence category (for critical section): 3
Observations: N/A
Emergency Action Required: N
Action By: N/A

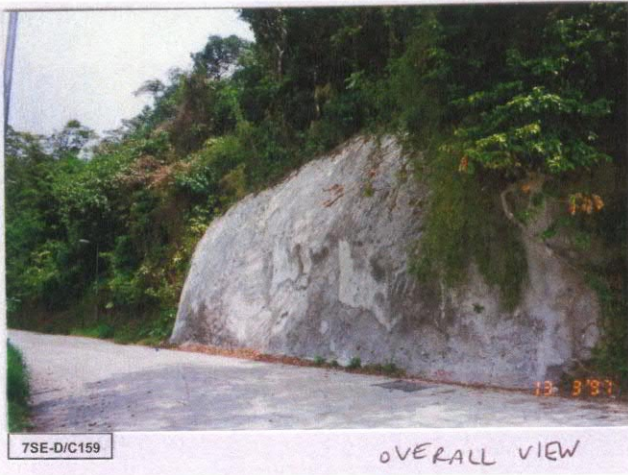
ACTION TO INITIATE PREVENTIVE WORKS

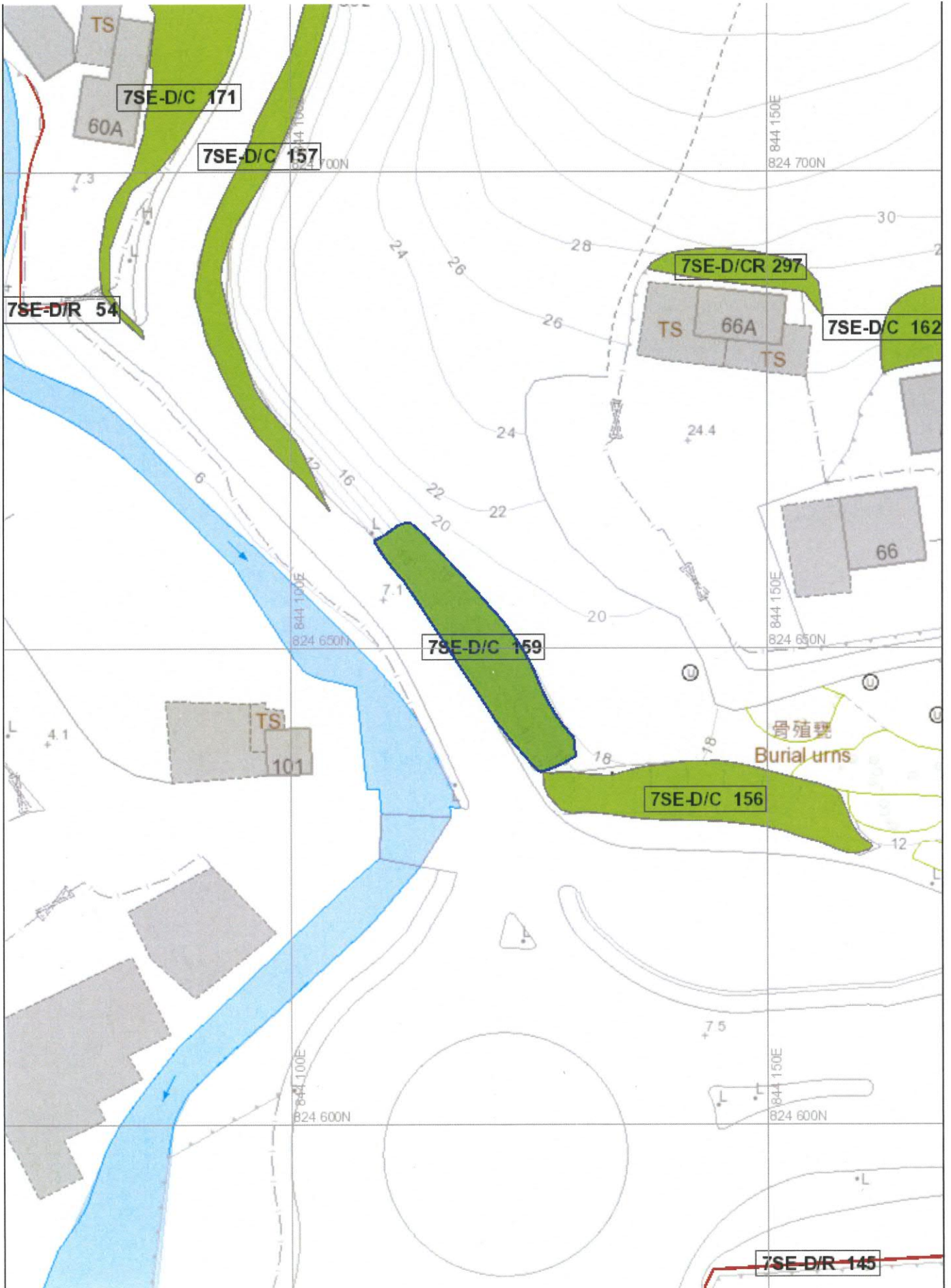
Criterion A / Criterion D: N/A
Action By: N/A
Further Study: Y
Action By: Mixed

OTHER EXTERNAL ACTION

Check / repair Services: N
Action By: N/A
Non-routine Maintenance: N
Action By: N/A

PHOTO





BASIC INFORMATION

Location: West of Hing Keng Shek Road
Date of Formation: pre-1977
Date of Construction/ Modification: 06-10-2006
Approximate Coordinates: Easting : 843974 Northing : 824812

CONSEQUENCE-TO-LIFE CATEGORY

Facility at Crest: Undeveloped green belt
Distance of Facility from Crest (m): 0
Facility at Toe: Cottage, licensed and squatter area
Distance of Facility from Toe (m): 0.5
Consequence-to-life Category: 1
Remarks: N/A

SLOPE PART

(1) Max. Height (m): 15 Length (m): 50 Average Angle (deg): 65

WALL PART

N/A

MAINTENANCE RESPONSIBILITY

Maintenance Type: Government Party: Lands D Agent: Lands D

DETAILS OF SLOPE / RETAINING WALL

Date of Inspection: 07-08-2017
Data Source: EI(Lands D)
Slope Part Drainage: (1) Position: Crest Size(mm): 225
Wall Part Drainage: N/A

SLOPE PART

Slope Part (1)
Surface Protection (%): Bare: 0 Vegetated: 80 Chunam: 0 Shotcrete: 20 Other Cover: 0
Material Description: Material type: Soil Geology: N/A
Berm: No. of Berms: N/A Min. Berm Width (m): N/A
Weepholes: Size (mm): 75 Spacing (m): 1.5

WALL PART

N/A



SERVICES

N/A

PHOTO







BASIC INFORMATION

Location: Adjacent Hing Keng Shek Road, Sai Kung
Date of Formation: pre-1977
Date of Construction/ Modification: N/A
Approximate Coordinates: Easting : 843975 Northing : 824825

CONSEQUENCE-TO-LIFE CATEGORY

Facility at Crest: Undeveloped green belt
Distance of Facility from Crest (m): 0
Facility at Toe: Remote area or abandoned facilities
Distance of Facility from Toe (m): 1
Consequence-to-life Category: 3
Remarks: N/A

SLOPE PART

(1) Max. Height (m): 6 Length (m): 31 Average Angle (deg): 55

WALL PART

N/A

MAINTENANCE RESPONSIBILITY

Maintenance Type: Government Party: Lands D Agent: Lands D

DETAILS OF SLOPE / RETAINING WALL

Date of Inspection: 13-08-2009
Data Source: EI(Lands D)
Slope Part Drainage: N/A
Wall Part Drainage: N/A

SLOPE PART

Slope Part (1)
Surface Protection (%): Bare: 0 Vegetated: 100 Chunam: 0 Shotcrete: 0 Other Cover: 0
Material Description: Material type: Soil & Rock Geology: N/A
Berm: No. of Berms: N/A Min. Berm Width (m): N/A
Weepholes: Size (mm): N/A Spacing (m): N/A

WALL PART

N/A



SERVICES

N/A

PHOTO





BASIC INFORMATION

Location: West of Hing Keng Shek Road, Sai Kung
 Date of Formation: post-1977
 Date of Construction/ Modification: 06-11-2006
 Approximate Coordinates: Easting : 844050 Northing : 824774

CONSEQUENCE-TO-LIFE CATEGORY

Facility at Crest: Undeveloped green belt
 Distance of Facility from Crest (m): 0
 Facility at Toe: Cottage, licensed and squatter area
 Distance of Facility from Toe (m): 0
 Consequence-to-life Category: 1
 Remarks: N/A

SLOPE PART

(1) Max. Height (m): 6.8 Length (m): 35 Average Angle (deg): 70

WALL PART

N/A

MAINTENANCE RESPONSIBILITY

Maintenance Type: Government Party: Lands D Agent: Lands D

DETAILS OF SLOPE / RETAINING WALL

Date of Inspection: 19-10-2017
 Data Source: El(Lands D)
 Slope Part Drainage: (1) Position: Toe Size(mm): 300
 Wall Part Drainage: N/A

SLOPE PART

Slope Part (1)
 Surface Protection (%): Bare: 0 Vegetated: 0 Chunam: 0 Shotcrete: 0 Other Cover: 100
 Material Description: Material type: Soil Geology: N/A
 Berm: No. of Berms: 1 Min. Berm Width (m): 0.4
 Weepholes: Size (mm): 75 Spacing (m): 1.5

WALL PART

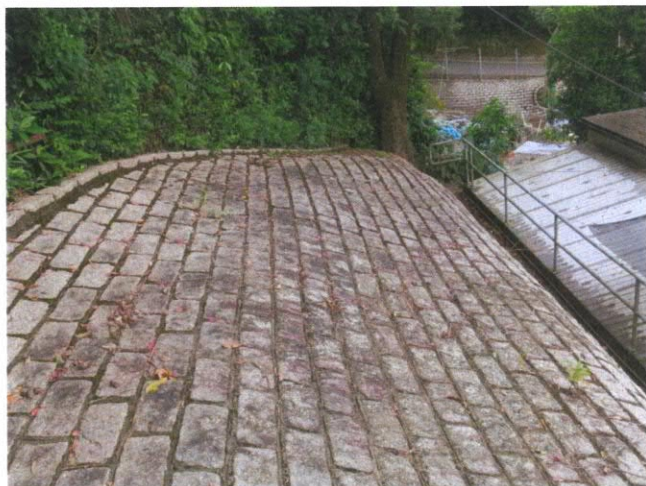
N/A

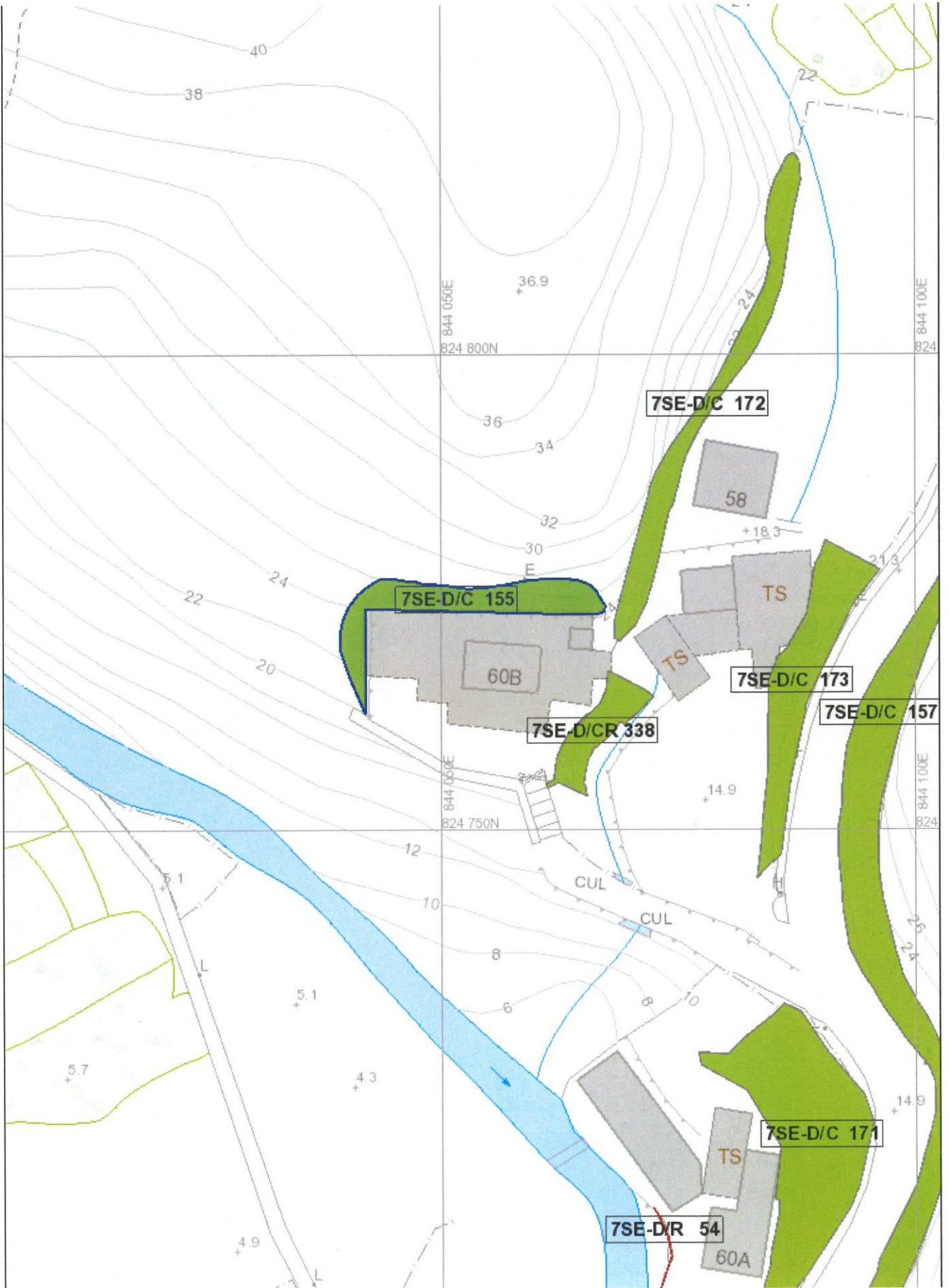


SERVICES

(1) Utilities Type: Water Main Size (mm): 20 Location: On slope Remark: N/A

PHOTO





Appendix B

Existing Boreholes Information

DRILLHOLE RECORD

HOLE NO. **BH 8**

CONTRACT NO. **GE/99/06**

SHEET **1** of **2**

PROJECT **PWP Item 4273DS-Port Shelter Sewerage Stage 3 Phase 3, Ho Chung and Pk Shui Sun Tsuen Sewerage, Ground Investigation**

METHOD **W+RC**

CO-ORDINATES

W.O. No **GE/99/06.59**

MACHINE & No. **DR 77**

E 844,049.46
N 824,686.94

DATE: **17/11/2001** to **19/11/2001**

FLUSHING MEDIUM **WATER**

ORIENTATION **VERTICAL**

GROUND LEVEL **+4.15** mPD

Drilling Progress	Casing size	Water level (m) Shift start/end	T.C.R.(%)	S.C.R.(%)	R.Q.D.(%)	F.I.	Tests	Samples	Reduced Level	Depth (m)	Legend	Grade	Description
17.11.01	PX							1 I.P.					Loose, light yellowish brown (10YR 8/4), silty fine to medium SAND with some angular to subangular fine to medium gravel sized rock fragments. (ALLUVIUM)
2			32				38blows	4 T2-101	+2.10	2.05			Light grey (10R 7/1), angular to subangular fine to coarse GRAVEL sized moderately strong tuff fragments. (ALLUVIUM)
3			51					5 T2-101	+0.65	3.50			Dark grey (5R 4/1), subangular fine to medium GRAVEL sized tuff fragments with a matrix of silty sand. (ALLUVIUM)
4	PX HX		100					6 T2-101	+0.05	4.10			Light grey, angular to subangular medium to coarse GRAVEL with much cobble and occasional boulder sized moderately strong to strong tuff fragments. (ALLUVIUM)
5	HX		41			NA		7 T2-101		4.95			
6		1.20m at 18:00	98	21	13	>20		8 T2-101	-1.00	5.15		III	Moderately strong, light grey to light yellowish brown, moderately decomposed coarse ash crystal TUFF. Joints are extremely closely occasionally very closely to closely spaced, rough, undulating and planar, extremely to very narrow, iron and manganese stained and clay infilled (<2-3mm), dipping at 60°-70° and subvertical.
17.11.01								9 T2-101		6.05			
18.11.01		1.10m at 08:00	98	32	10	10.0		10 T2-101	-2.35	6.50		III/II	Moderately strong to strong, grey, moderately to slightly decomposed coarse ash crystal TUFF. Joints are closely, locally extremely to very closely spaced, rough, undulating and planar, extremely narrow, iron and manganese stained, kaolin infilled (<2mm), dipping at 60°-70° and subvertical.
7						>20		11 T2-101		7.35			
8			100	80	47	5.0		12 T2-101		8.10			
9			100	67	44	NI		13 T2-101		9.45			From 8.90-9.45m : Non intact.
10						7.7		14 T2-101	-5.30	9.45		II	Strong, grey to dark grey, slightly decomposed coarse ash crystal TUFF.

- Small Disturbed Sample
- Piston Sample
- U76 Undisturbed Sample
- U100 Undisturbed Sample
- Mazier Sample
- SPT Liner Sample
- Water Sample
- Standard Penetration Test
- In-situ Vane Shear Test
- Permeability Test
- Impression Packer Test
- Packer Test
- Piezometer Tip
- Standpipe Tip

LOGGED **P. Barry**
 DATE **20/11/2001**
 CHECKED **J. Morrison**
 DATE **22/11/2001**

REMARKS
 1. Prior to drilling an inspection pit was excavated by hand to 1.50m depth.

ENPACK (H.K.) LIMITED
 Civil Engineers & Contractors
 Astoria Building, 6th Floor, 34 Ashley Road
 Hongkong, Hong Kong
 Tel : 23742121 Fax : 23742022
 ISO 9002 : 1994
 Certificate No. PQ00021

DRILLHOLE RECORD

HOLE NO. **BH 8**

CONTRACT NO. **GE/99/06**

SHEET **2** of **2**

PROJECT **PWP Item 4273DS-Port Shelter Sewerage Stage 3 Phase 3, Ho Chung and Pik Shui Sun Tsuen Sewerage, Ground Investigation**

METHOD **W+RC**

CO-ORDINATES

W.O. No **GE/99/06.59**

MACHINE & No. **DR 77**

E 844,049.46
N 824,688.94

DATE: **17/11/2001** to **19/11/2001**

FLUSHING MEDIUM **WATER**

ORIENTATION **VERTICAL**

GROUND LEVEL **+4.15** mPD

Drilling Progress	Casing size	Water level (m) Shift start/end	T.C.R.(%)	S.C.R.(%)	R.Q.D.(%)	F.I.	Tests	Samples	Reduced Level	Depth (m)	Legend	Grade	Description
10			100	100	77								Joints are closely becoming medium spaced, planar, extremely narrow, kaolin coated and occasionally kaolin infilled (<2mm), dipping at 45°-55°, 60°-70° and subvertical. From 10.15-10.50m : Non intact.
11						NI 2.1	T2-101	10.95					
12			100	90	73		T2-101	12.45					
13			100	100	76		T2-101	13.90					
14		1.15m at 18:00	100	100	89		T2-101	14.80	-10.65	14.80			
15													End of Investigation hole at 14.80m.
16													
17													
18													
19													
20													

<ul style="list-style-type: none"> Small Disturbed Sample Piston Sample U76 Undisturbed Sample U100 Undisturbed Sample Mazier Sample SPT Liner Sample Water Sample 	<ul style="list-style-type: none"> Standard Penetration Test In-situ Vane Shear Test Permeability Test Impression Packer Test Packer Test Piezometer Tip Standpipe Tip
--	--

LOGGED P. Barry
 DATE 20/11/2001
 CHECKED J. Morrison
 DATE 22/11/2001

REMARKS



ENPACK (H.K.) LIMITED

Civil Engineers & Contractors
Asteris Building, 9th Floor, 24 Ashley Road
Kowloon, Hong Kong

Tel : 23792121 Fax : 23794832



ISO 9002:1994
Certificate No. PQ00021

DRILLHOLE RECORD

HOLE NO. **BH 9**

CONTRACT NO. **GE/99/06**

SHEET **1** of **2**

PROJECT **PWP Item 4273DS-Port Shelter Sewerage Stage 3 Phase 3, Ho Chung and Pik Shui Sun Tsuen Sewerage, Ground Investigation**

METHOD **W+RC**

CO-ORDINATES

W.O. No **GE/99/06.59**

MACHINE & No. **DR 77**

E 844,072.77

DATE: **21/11/2001** to **22/11/2001**

N 824,663.34

FLUSHING MEDIUM **WATER**

ORIENTATION **VERTICAL**

GROUND LEVEL **+4.06** mPD

Drilling Progress	Casing size	Water level (m) Shift start/end	T.C.R.(%)	S.C.R.(%)	R.Q.D.(%)	F.I.	Tests	Samples	Reduced Level	Depth (m)	Legend	Grade	Description
21.11.01	PX												Soft to firm, yellowish brown (10YR 5/6) and light grey (10R 7/1), sandy SILT with some angular fine to coarse gravel sized weak to moderately strong tuff fragments. (FILL)
1								1 I.P. 0.50 2 I.P. 1.00 3 I.P. 1.50					
2			0				20blows	4 1.90 5 2.00					
3			100				21blows	6 2.45 7 2.50	+1.06	3.00		V	Extremely weak, red (10R 5/8) mottled yellowish brown, completely decomposed coarse ash crystal TUFF. (Firm to stiff, slightly sandy clayey SILT with occasional angular fine to medium gravel sized rock fragments)
4							(1, 2, 3, 4, 4, 4) N=15	8 3.40					
5								10 4.00					
6			100				(2, 2, 3, 5, 5, 6) N=19	12 5.65 13 5.95 14 6.00	-1.94	6.00		V/IV	Extremely weak to weak, light yellowish brown (2.5Y 6/4), completely to highly decomposed coarse ash crystal TUFF. (Sandy angular fine to coarse GRAVEL sized weak rock fragments)
7	PX							15 6.90	-2.94	7.00		III	Moderately strong, light yellowish brown, moderately decomposed highly micro fractured coarse ash crystal TUFF.
8			95	30	0	>20		T2-101 7.50					Joints are extremely very closely spaced, rough, planar, extremely narrow, iron and manganese stained, dipping at 35°-45°, 60°-70° and subvertical.
9			100	100	50	8.8		T2-101 7.80	-3.94	8.00		III/II	Moderately strong to strong, grey, moderately to slightly decomposed coarse ash crystal TUFF.
10			100	85	38	8.8		T2-101 8.00					Joints are very closely to closely spaced, rough, undulating and planar, extremely to very narrow, iron and manganese stained, kaolin infilled (<2-3mm), dipping at 35°-45°, 60°-70° and subvertical. From 8.80-9.10m : Extremely closely spaced joints.
9								T2-101 8.80					
10			95	73	37	>20	8.7						

- Small Disturbed Sample
- Piston Sample
- U75 Undisturbed Sample
- U100 Undisturbed Sample
- Mazier Sample
- SPT Liner Sample
- Water Sample
- Standard Penetration Test
- In-situ Vane Shear Test
- Permeability Test
- Impression Packer Test
- Packer Test
- Piezometer Tip
- Standpipe Tip

LOGGED P. Barry
 DATE 23/11/2001
 CHECKED J. Morrison
 DATE 01/12/2001

REMARKS
 1. Prior to drilling an inspection pit was excavated by hand to 1.50m depth.

ENPACK (H.K.) LIMITED
 Civil Engineers & Contractors
 Avenida Building, 65/F, 34 Ashley Road
 Kowloon, Hong Kong
 Tel: 23762124 Fax: 23762282



ISO 9001:1994
 Certificate No. PQ00021

DRILLHOLE RECORD

HOLE NO. **BH 9**

CONTRACT NO. **GE/99/06**

SHEET **2** of **2**

PROJECT **PWP Item 4273DS-Port Shelter Sewerage Stage 3 Phase 3, Ho Chung and Pik Shui Sun Tsuen Sewerage, Ground Investigation**

METHOD **W+RC**

CO-ORDINATES

W.O. No **GE/99/06.59**

MACHINE & No. **DR 77**

E 844,072.77
N 824,663.34

DATE: **21/11/2001** to **22/11/2001**

FLUSHING MEDIUM **WATER**

ORIENTATION **VERTICAL**

GROUND LEVEL **+4.06** mPD

Drilling Progress	Casing size	Water level (m) Shift start/end	T.C.R.(%)	S.C.R.(%)	R.Q.D.(%)	F.I.	Tests	Samples	Reduced Level	Depth (m)	Legend	Grade	Description	
10								T2-101	10.30				As sheet 1 of 2.	
11		0.30m at 18:00	100	91	53	1.7		T2-101	-8.94	11.00		II	From 10.30-10.40m : Non intact.	
11.11.01													Strong, grey to dark grey, slightly decomposed coarse ash crystal TUFF. Joints are closely to medium spaced, rough, planar, extremely narrow, chlorite coated, dipping at 35°-45°, 60°-70° and subvertical.	
12		1.25m at 08:00	100	100	100		T2-101							
13								T2-101						
14			100	100	70	5.5		T2-101						
15			100	100	59			T2-101						
16								T2-101						
16			100	92	54			T2-101						
17		1.23m at 18:00						T2-101	17.10	-13.04	17.10		End of Investigation hole at 17.10m.	
17.11.01														
18														
19														
20														

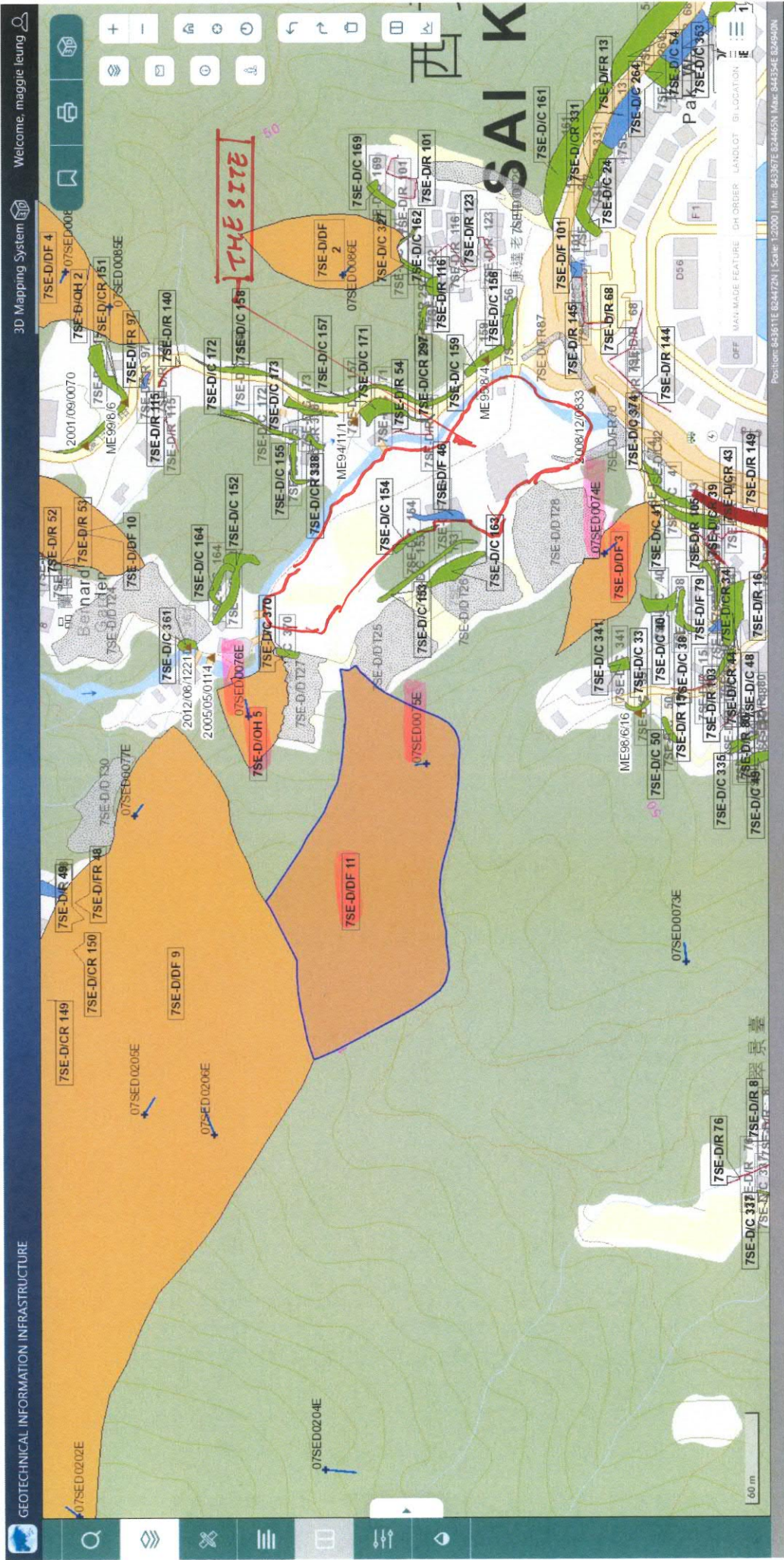
<ul style="list-style-type: none"> Small Disturbed Sample Piston Sample U76 Undisturbed Sample U100 Undisturbed Sample Mazier Sample GPT/Liner Sample Water Sample 	<ul style="list-style-type: none"> Standard Penetration Test In-situ Vane Shear Test Permeability Test Impression Packer Test Packer Test Piezometer Tip Standpipe Tip
---	---

LOGGED P. Barry
 DATE 23/11/2001
 CHECKED J. Morrison
 DATE 01/12/2001

REMARKS

Appendix C

ENTLI and HLC from GInfo



ENTLI & MLC

Historical Landslide Catchment

Name	Value
Catchment No.	7SE-D/DF 3
Plan area of the catchment (Sq. m)	2768.70312927
Maximum elevation difference (m)	40
The plan area of the catchment with gradient less than 15 degree (Sq. m)	194.443969406
Site visit for the catchment	Y
Total number of relict ENTLLI records within the catchment	1
Total number of recent ENTLLI records within the catchment	0
The length of the longest relict ENTLLI record within the catchment (m)	11.18
The length of the longest recent ENTLLI record within the catchment (m)	0
The length of the longest ENTLLI record within the catchment (m)	11.18
The total length of the all ENTLLI record within the catchment (m)	11

ENTLI Crown / Trail

Name	Value
ENTLI No.	07SED0074E
Slide ID	07SBDX0013
Type of slides	Relict
Action	A
Width of main scarp	9.5
Length of landslide source	11
Slope	45
Vegetation cover	Completely in grass
Year landslide first observed	1963
Elevation of landslide crown	32
Elevation of landslide toe	26
Elevation difference of landslide trail	6
Gully	N
Relict class	Depression related to drainage line (10% certain)
Easting	843994.155676
Northing	824571.578318

Historical Landslide Catchment

Name	Value
Catchment No.	7SE-D/DF 11
Plan area of the catchment (Sq. m)	19668
Maximum elevation difference (m)	75
The plan area of the catchment with gradient less than 15 degree (Sq. m)	0
Site visit for the catchment	Y
Total number of relict ENTLI records within the catchment	1
Total number of recent ENTLI records within the catchment	0
The length of the longest relict ENTLI record within the catchment (m)	8
The length of the longest recent ENTLI record within the catchment (m)	0
The length of the longest ENTLI record within the catchment (m)	8
The total length of the all ENTLI record within the catchment (m)	8

ENTLI Crown / Trail

Name	Value
ENTLI No.	07SED0075E
Slide ID	07SBDX0014
Type of slides	Relict
Action	A
Width of main scarp	14
Length of landslide source	7.5
Slope	39
Vegetation cover	Shrubs and trees
Year landslide first observed	1963
Elevation of landslide crown	57
Elevation of landslide toe	49
Elevation difference of landslide trail	8
Gully	N
Relict class	Broad depression (10% certain)
Easting	843857.473781
Northing	824688.2476

Historical Landslide Catchment

Name	Value
Catchment No.	7SE-D/OH 5
Plan area of the catchment (Sq. m)	2190.91106044
Maximum elevation difference (m)	51
The plan area of the catchment with gradient less than 15 degree (Sq. m)	0
Site visit for the catchment	Y
Total number of relict ENTLI records within the catchment	1
Total number of recent ENTLI records within the catchment	0
The length of the longest relict ENTLI record within the catchment (m)	11.69
The length of the longest recent ENTLI record within the catchment (m)	0
The length of the longest ENTLI record within the catchment (m)	11.69
The total length of the all ENTLI record within the catchment (m)	12



SLOPE INFORMATION SYSTEM
GEOTECHNICAL ENGINEERING OFFICE
CIVIL ENGINEERING AND DEVELOPMENT DEPARTMENT

ENTLI Crown / Trail

Name	Value
ENTLI No.	07SED0076E
Slide ID	07SEDX0015
Type of slides	Relict
Action	A
Width of main scarp	16
Length of landslide source	11.5
Slope	47
Vegetation cover	Completely in grass
Year landslide first observed	1963
Elevation of landslide crown	36
Elevation of landslide toe	25
Elevation difference of landslide trail	11
Gully	N
Relict class	Broad depression (10% certain)
Easting	843888.557203
Northing	824804.065281

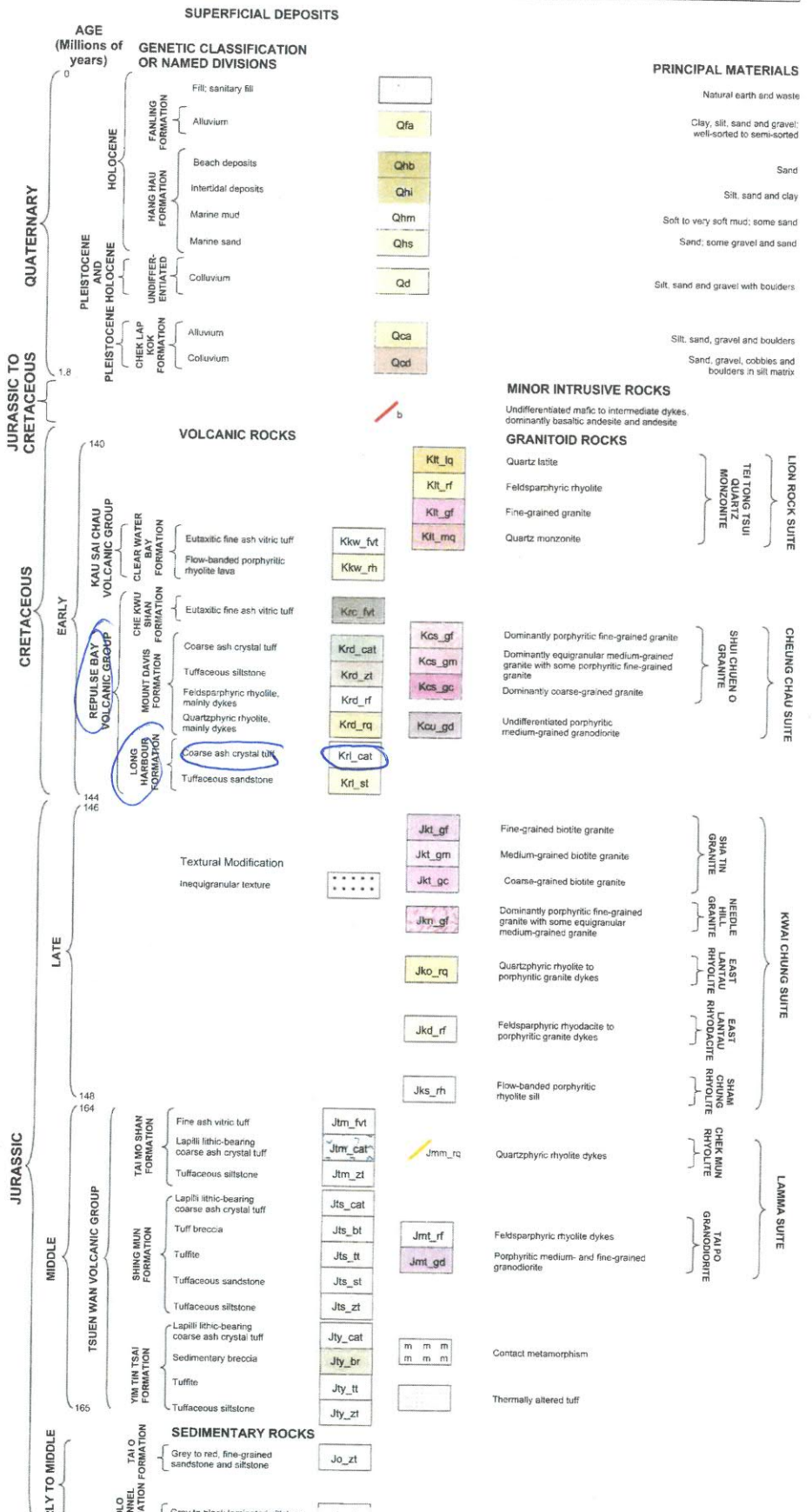
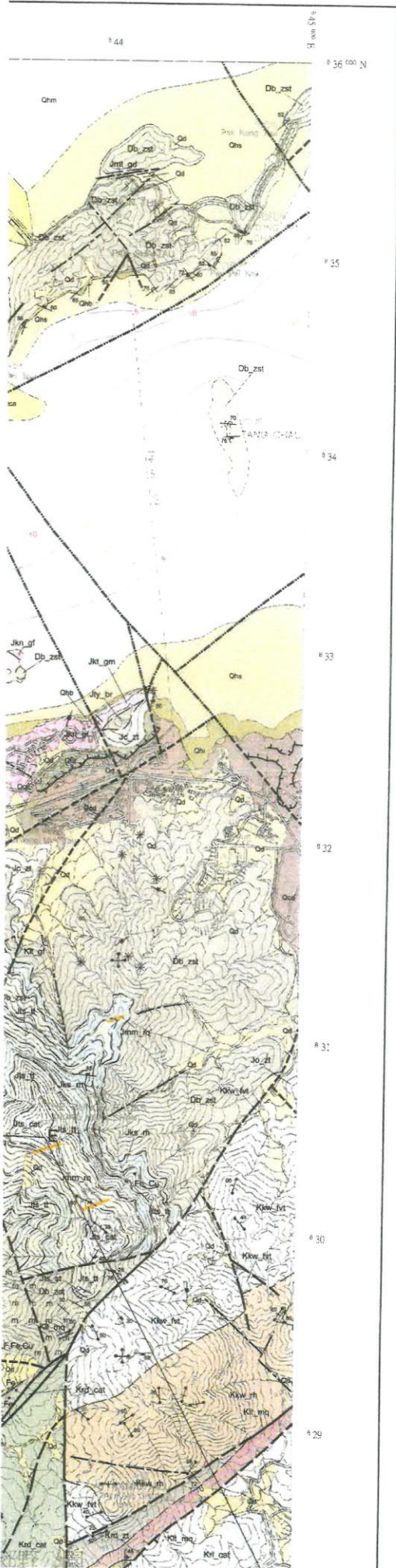
Appendix D

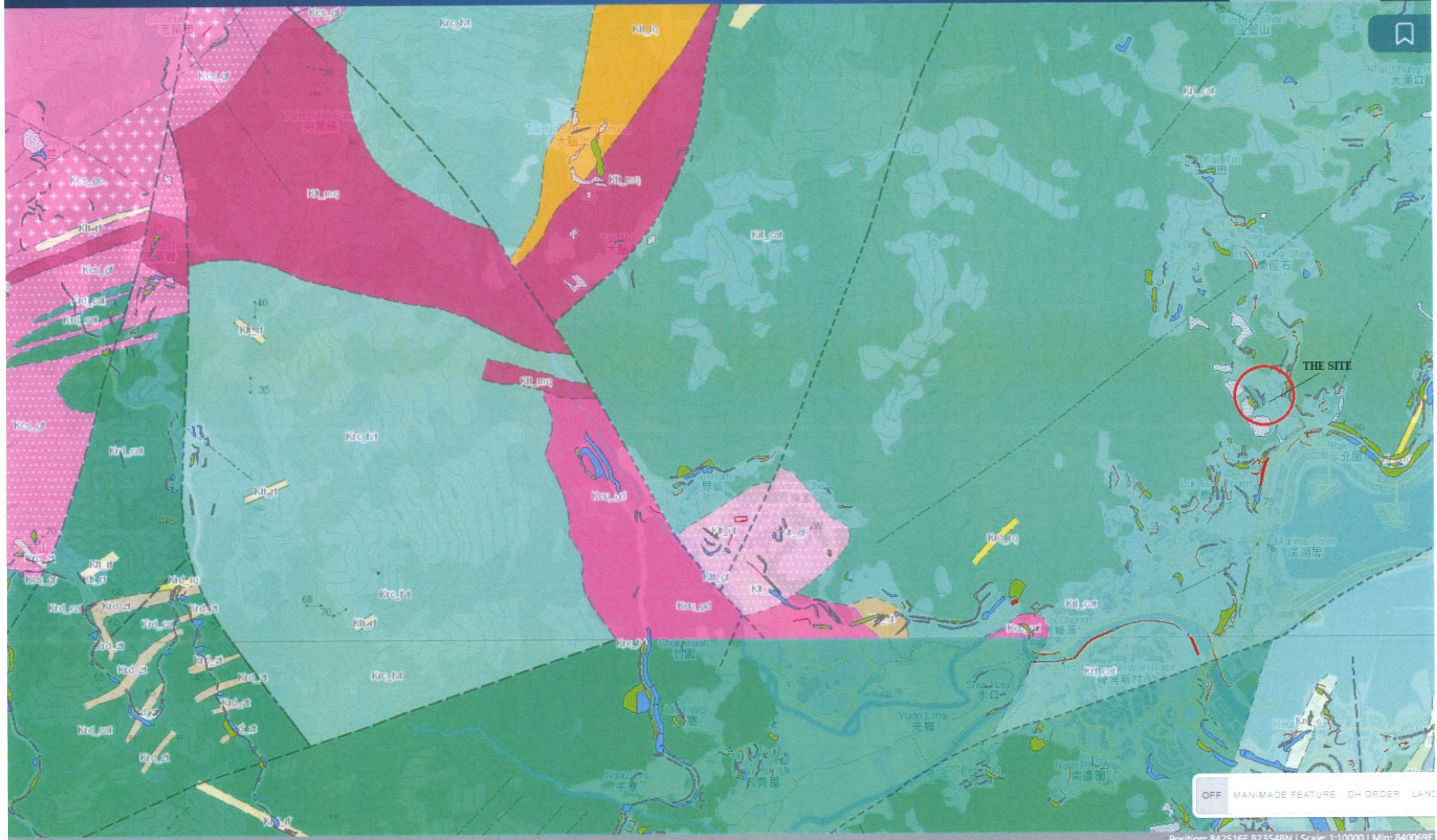
Geology Map from GInfo

HONG KONG GEOLOGICAL SURVEY

Sheet 7 SOLID AND SUPERFICIAL GEOLOGY

Series HGM20
Scale 1:20 000





OFF MAN-MADE FEATURE DH ORDER LAND

Position: 842516E 823548N | Scale: 1:10000 | Min: 840069E

SOLID GEOLOGY

Appendix E

Summary of API

Summary of API

Year	Photographic Reference no.	Activities	Observations
1963	1963-9701 1963-9702	3900	The usage of land in the Site is farmland. Catchment A1 & A2 are also farmland. 3 nos. of Enhanced Natural Terrain Landslide Inventory (ENTLI) with Historical Landslide Catchment (HLC) are found from Ginjo.
1964	1964-4532	1800	The Site, Catchment A1 & A2 are farmland. Catchment B & C were fully covered by vegetation.
1968	1968-0898 1968-0899	2000	The Site, Catchment A2 are farmland. Catchment A1 was started to cover by vegetation/ trees and had been abandoned. Catchment B & C were fully covered by vegetation.
1974	09302	4000	The Site, Catchment A2 are farmland. Catchment A1, B & C were fully covered by vegetation. Catchment A2 was started to cover by vegetation/ trees and had been abandoned
1977	20026	4000	The Site is farmland. Catchments under study were fully covered by vegetation.
1978	23149	4000	The Site is farmland Catchments under study were fully covered by vegetation.
1979	25705	2500	The Site is farmland Catchments under study were fully covered by vegetation.
1980	29666	4000	The Site is farmland Catchments under study were fully covered by vegetation.
1982	43262	4000	The Site is farmland Catchments under study were fully covered by vegetation.

Year	Photographic Reference no.	Activities	Observations
1983	50643	4000	Middle part was farmland and other parts of the Site was started to cover by vegetation/ trees Catchments under study were fully covered by vegetation.
1984	55367	4000	Middle part of the Site was farmland. Other parts of the Site and Catchments under study were fully covered by vegetation.
1986	A05256	4000	Middle part of the Site was farmland. Other parts of the Site and Catchments under study were fully covered by vegetation.
1987	A08855	4000	Middle part of the Site was farmland. Other parts of the Site and Catchments under study were fully covered by vegetation.
1988	A15444	4000	Middle part of the Site was farmland. Other parts of the Site and Catchments under study were fully covered by vegetation.
1989	A17757	4000	Middle part of the Site was farmland. Other parts of the Site and Catchments under study were fully covered by vegetation.
1990	A21734	2000	Middle part of the Site was started to cover by vegetation/ trees. Other parts of the Site and Catchments under study were fully covered by vegetation.
1991	A29309	3000	The Site and Catchments under study were fully covered by vegetation.
1993	CN03209	4000	Some squatters found in the Site. Catchments under study were fully covered by vegetation.
1994	CN06606	3300	Some squatters found in the Site. Catchments under study were fully covered by vegetation.
1995	CN10978	2500	Some squatters found in the Site. Catchments under study were fully covered by vegetation.

Year	Photographic Reference no.	Activities	Observations
1996	CN15116	3500	Some squatters found in the Site. Catchments under study were fully covered by vegetation.
1997	CN16771	4000	Some squatters found in the Site. Catchments under study were fully covered by vegetation.
1998	CN21228	4000	Some squatters found in the Site. Catchments under study were fully covered by vegetation.
1999	CN23114	2500	Some squatters found in the Site. Catchments under study were fully covered by vegetation.
2000	CN26681	4000	Public works (Port Shelter Sewerage Stage 3, Ph. 3 – GE/99/06) was commenced in the middle of the Site. Some squatters found in the Site. Catchments under study were fully covered by vegetation.
2001	CW32429	4000	GI for Public works GE/99/06 was commenced in the middle of the Site Some squatters found in the Site. Catchments under study were fully covered by vegetation.
2002	CW38786	3000	Some squatters found in the Site. Catchments under study were fully covered by vegetation.
2003	CW52903	4000	New Road (Hing Keng Shek Road) beside the Site is formed Some squatters found in the Site. Catchments under study were fully covered by vegetation.
2004	CW52903	4000	Some squatters found in the Site. Catchments under study were fully covered by vegetation.

Year	Photographic Reference no.	Activities	Observations
2005	CW63913 CW64565	2500	Some squatters found in the Site. Catchments under study were fully covered by vegetation.
2006	CW52903	4000	Some squatters found in the Site. Catchments under study were fully covered by vegetation.
2007	CS09320	4000	Some squatters found in the Site. Catchments under study were fully covered by vegetation.
2008	CS22404	6000	Some squatters found in the Site. Catchments under study were fully covered by vegetation.
2009	CS24367	6000	Some squatters found in the Site. Catchments under study were fully covered by vegetation.
2010	CS29288	6000	Some squatters found in the Site. Catchments under study were fully covered by vegetation.
2011	CS35169	6000	Some squatters found in the Site. Catchments under study were fully covered by vegetation.
2012	CS39288	6000	Some squatters found in the Site. Catchments under study were fully covered by vegetation.
2013	CS45139	6000	Some squatters found in the Site. Catchments under study were fully covered by vegetation.
2014	CS48747 CS48748	6000	Some squatters found in the Site. Catchments under study were fully covered by vegetation.
2015	CS57777	6000	Some squatters found in the Site. Catchments under study were fully covered by vegetation.

Year	Photographic Reference no.	Activities	Observations
2016	CS64065	6000	Some squatters found in the Site. Catchments under study were fully covered by vegetation.
2019	E068813C	6900	Some squatters found in the Site. Catchments under study were fully covered by vegetation.
2020	E097551C	6900	Some squatters found in the Site. Catchments under study were fully covered by vegetation.
2021	E129664C	6900	Some squatters found in the Site. Catchments under study were fully covered by vegetation.
2022	E181548C	3000	Some squatters found in the Site. Catchments under study were fully covered by vegetation.
2023	E194258C	6900	Some squatters found in the Site. Catchments under study were fully covered by vegetation.
2024	E233512C E233513C	6900	Latest Aerial Photos Some squatters found in the Site. Catchments under study were fully covered by vegetation. No New ENTLI, HLC & Landslide Incident was found from GInfo in Catchments under study

Year: 1963
Photo No.: 1963-9702

The Site (as farmland)

ENTLI: 07SED0074E

Catchment A2

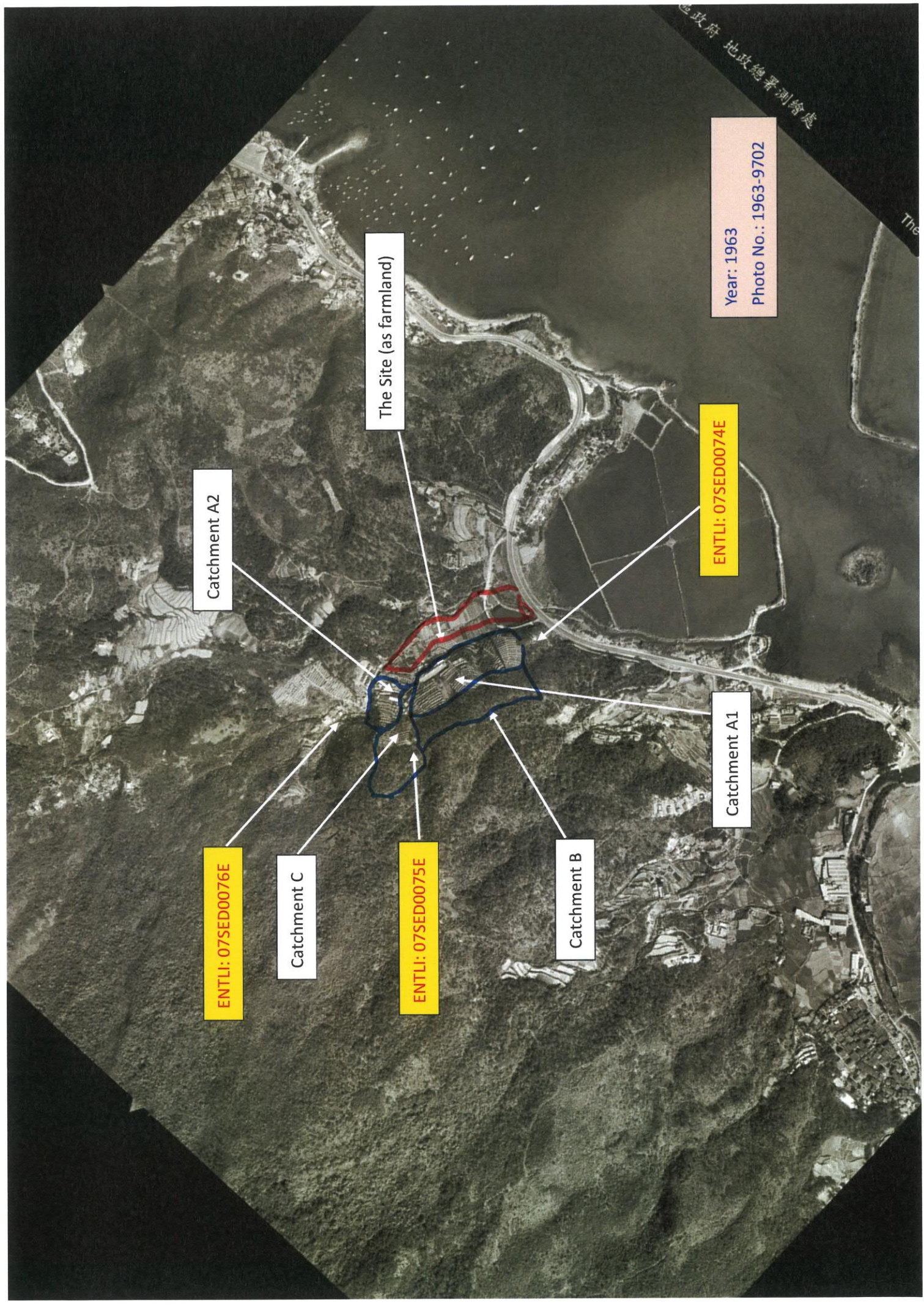
Catchment A1

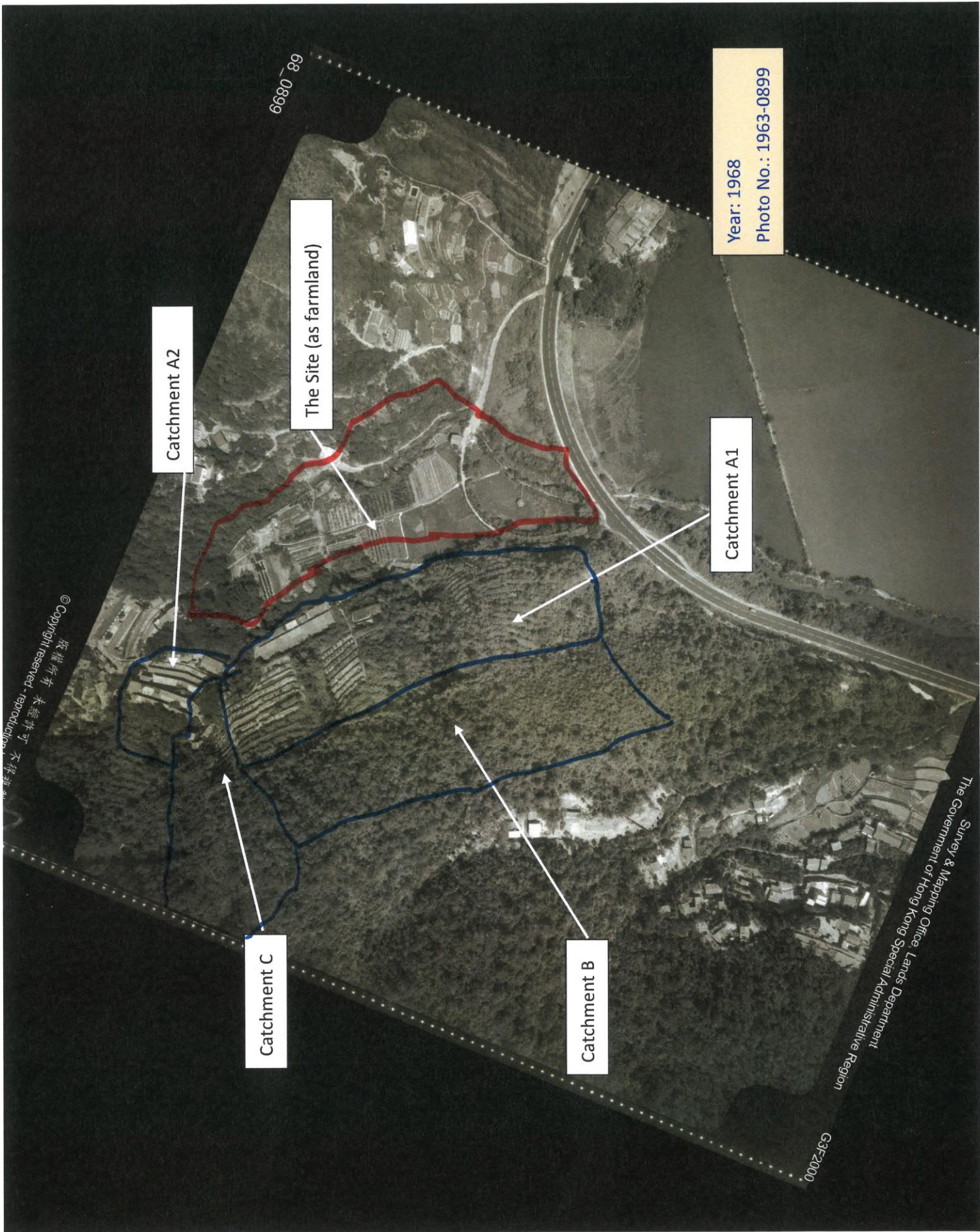
ENTLI: 07SED0076E

Catchment C

ENTLI: 07SED0075E

Catchment B





6680 89

Year: 1968
Photo No.: 1963-0899

Catchment A2

The Site (as farmland)

Catchment A1

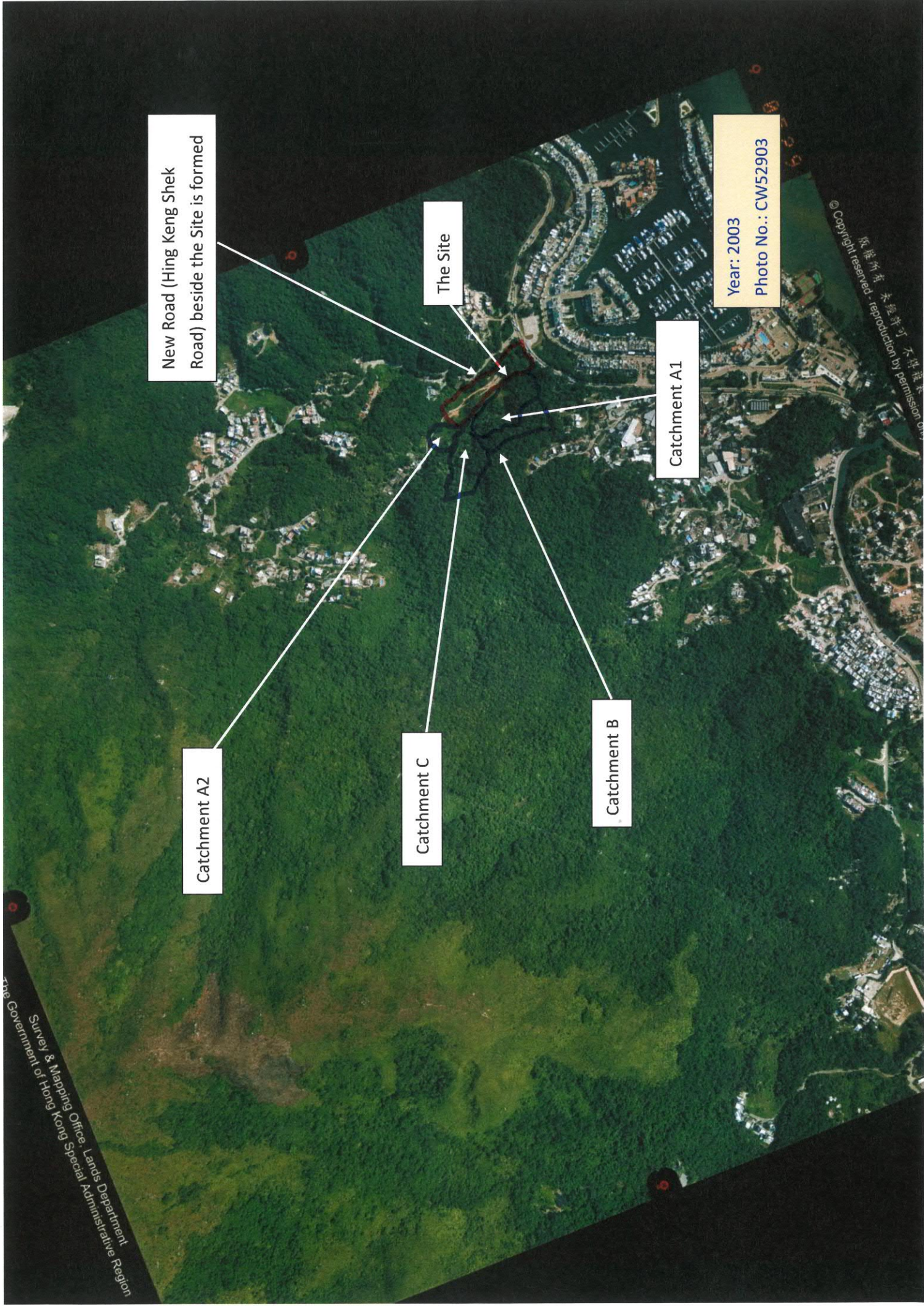
Catchment C

Catchment B

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The Government of Hong Kong Special Administrative Region
Survey & Mapping Office, Lands Department

G3F-2000



New Road (Hing Keng Shek Road) beside the Site is formed

The Site

Year: 2003
Photo No.: CW52903

Catchment A1

Catchment A2

Catchment C

Catchment B

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8747 23 Jan 2014 6000' PAK WAI

Year: 2014
Photo No.: CS48747

The Site

Hing Keng Shek Road
beside the Site

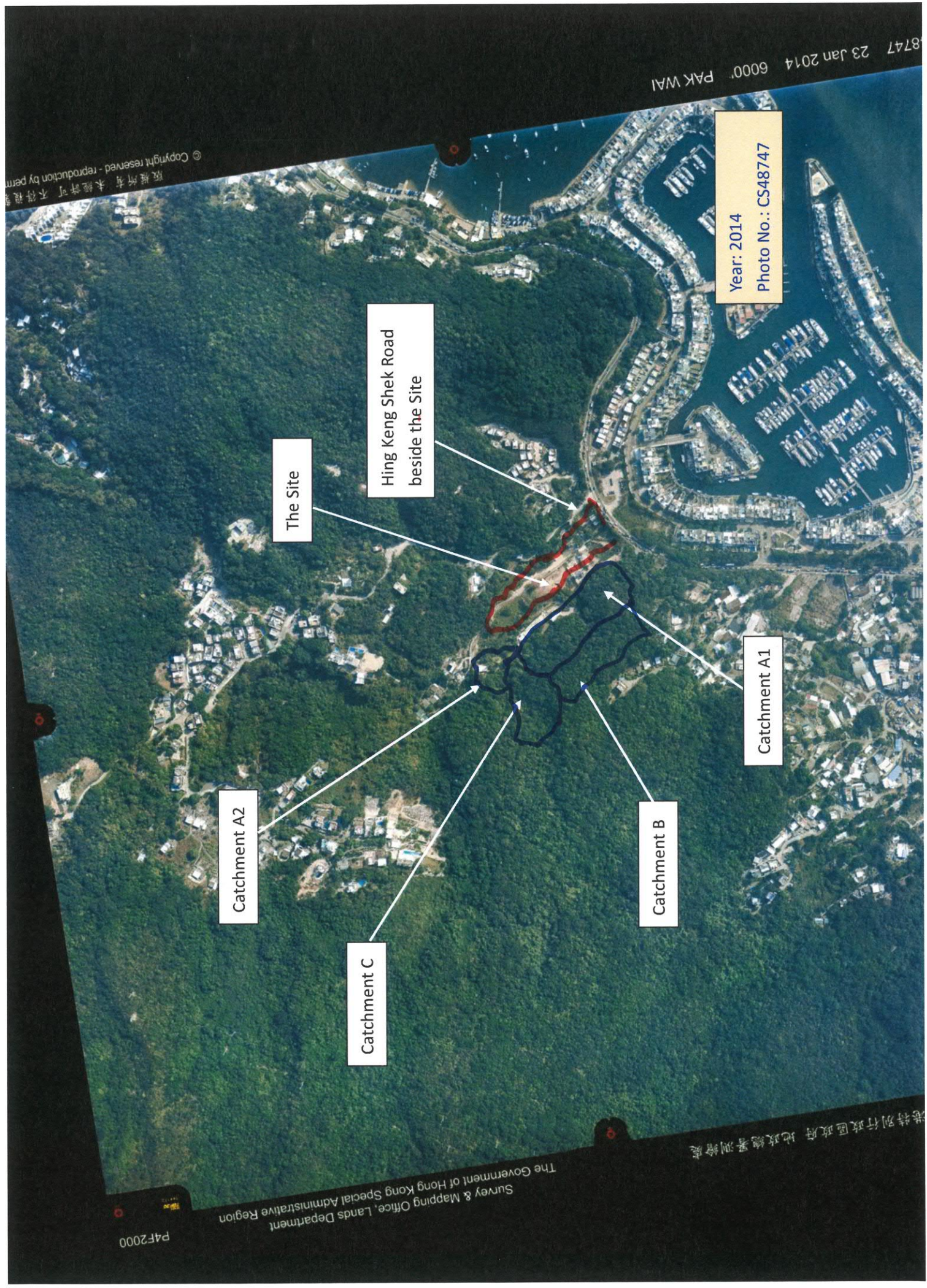
Catchment A1

Catchment A2

Catchment B

Catchment C

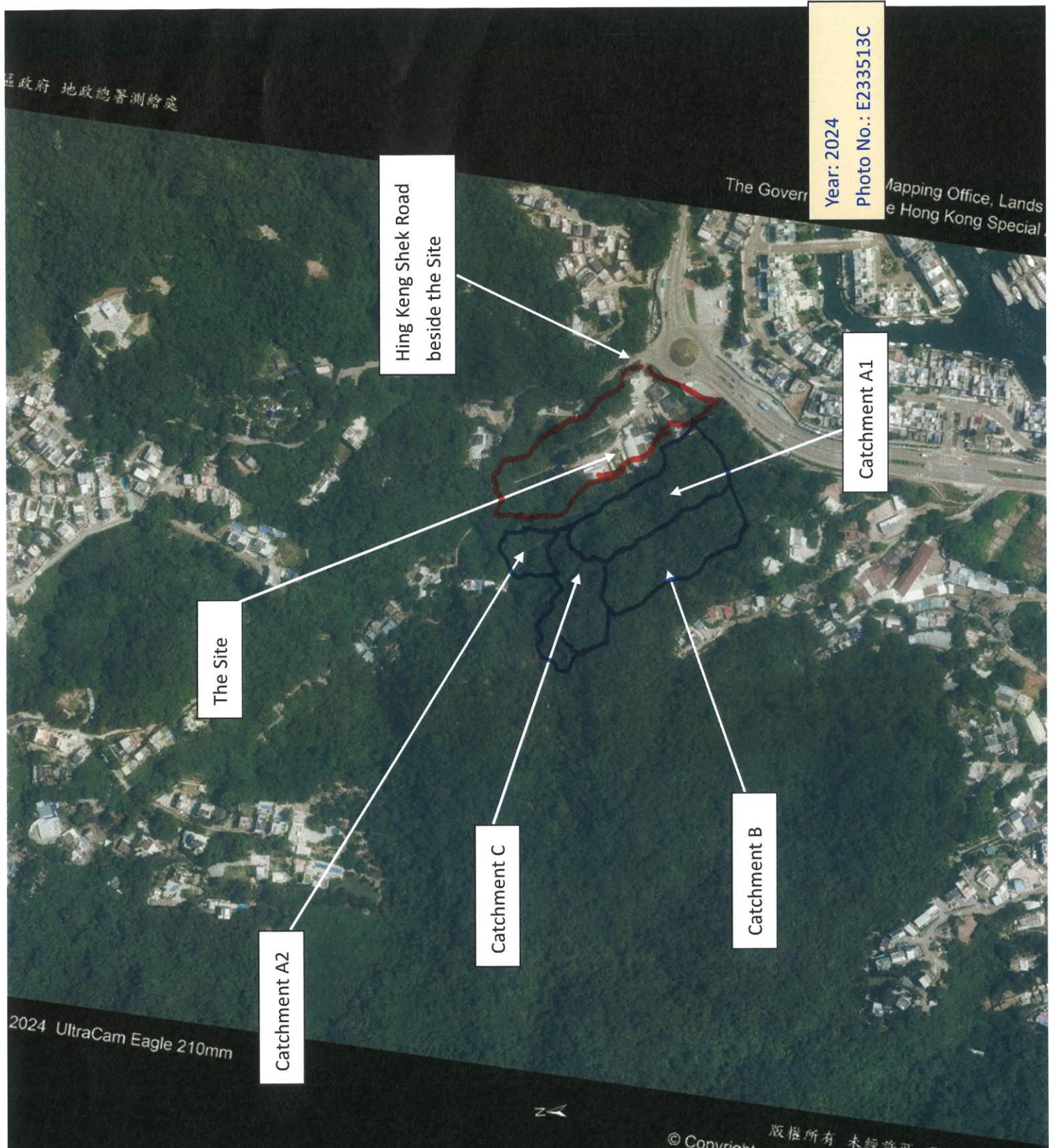
PAF2000
Survey & Mapping Office, Lands Department
The Government of Hong Kong Special Administrative Region
港特別行政區政府 地政總署測繪處



區政府 地政總署測繪處

Year: 2024
Photo No.: E233513C

The Government Mapping Office, Lands
The Hong Kong Special



Hing Keng Shek Road
beside the Site

Catchment A1

The Site

Catchment C

Catchment B

Catchment A2

2024 UltraCam Eagle 210mm



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Appendix C

Revised Traffic Impact Assessment

DOCUMENT STATUS CONTROL RECORD

Application for Amendment of Plan under Section 12A of the Town Planning Ordinance (Cap. 131) to Rezone the Application Site from "Green Belt" and Area Shown as "Road" to "Residential (Group C)5" for Proposed Residential Development at Various Lots in D.D. 210 and Adjoining Government Land, Pak Wai, Sai Kung

Traffic Impact Assessment Report

Originating Organisation : LLA Consultancy Limited Unit 610, 6/F, Island Place Tower, 510 King's Road, North Point, Hong Kong	Prepared by: SKL		Date: 12 February 2026
	Approved by: SLN		Date: 12 February 2026
	Revision No.: -		Date of Issue: 12 February 2026

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1 INTRODUCTION

1.1 Background

- 1.1.1 The Applicant intends to develop the Site into a residential development at various lots in D.D.210, Pak Wai, Sai Kung ("the Site").
- 1.1.2 The Site is currently zoned as "Green Belt" ("GB") and "Road" under the Draft Ho Chung Outline Zoning Plan (OZP) No. S/SK-HC/12. The Applicant proposes amendments to the Draft Ho Chung Outline Zoning Plan (OZP) No. S/SK-HC/12 by rezoning the Application Site from "Green Belt" ("GB") and area shown as "Road" to "Residential (Group C)5" ("R(C)5"), with a maximum plot ratio of 0.6 and maximum building heights (BH) of 4 storeys (excluding basements) to facilitate the proposed residential development.
- 1.1.3 LLA Consultancy Limited was commissioned to carry out a traffic impact assessment study for the proposal to assess the potential traffic impact on its adjacent road network, in support of the planning application. This report presents the finding of the study.

1.2 Objectives

- 1.2.1 The objectives of the traffic impact assessment study are as follows:
- to review the existing traffic conditions in the surrounding road network;
 - to estimate the potential traffic generation due to the proposed development;
 - to assess the future traffic situation in the surrounding road network;
 - to appraise the potential traffic impact of the proposed development; and
 - to recommend the transport facilities provisions for the proposed development.

2 THE PROPOSED DEVELOPMENT

2.1 The Site

- 2.1.1 As shown in **Figure 2.1**, the Site is located near the J/O Hiram's Highway/Hing Keng Shek Road. The Site area is about 12,692 m².

2.2 Development Schedule

- 2.2.1 The Site will comprise of 4 residential towers with 96 residential units. The development parameters are summarized in **Table 2.1**.

Table 2.1 Proposed Development Schedule

Item	Parameter
Site Area	About 12,692 m ²
Plot Ratio	About 0.6
Total GFA	About 7,615.2 m ²
Domestic GFA	About 7,615.2 m ²
Number of Residential Blocks	4 blocks
Number of Residential Units	96 units
Estimated Residential Population	288

3 EXISTING TRAFFIC SITUATION

3.1 Existing Traffic Conditions

3.1.1 Hing Keng Shek Road serves as a local road connecting to Hiram's Highway. It is a single carriageway with few accesses for the low-density developments and villages nearby.

3.1.2 Hiram's Highway is a major road in the eastern part of New Territories connecting Sai Kung with Clear Water Bay Road. The section of Hiram's Highway between Clear Water Bay Road and Po Tung Road carried an AADT of 22,860 vehicles in 2023.

3.2 Existing Junction Capacity Assessment

3.2.1 In order to assess the existing traffic conditions, a traffic count survey was carried out at the following locations in the vicinity of the Site on 13 October 2025 (Monday) during 07:30 – 09:30 and 17:30 – 19:30 and 11 October 2025 (Saturday) from 12:00 to 19:00. The locations of the surveyed junctions are presented in **Figure 3.1**.

- Hiram's Highway/Hing Keng Shek Road Roundabout
- Hiram's Highway/Ho Chung Road
- Hiram's Highway/ New Hiram's Highway/ Nam Pin Wai Road

3.2.2 The identified weekday AM, weekday PM and weekend peak hours were 08:00 – 09:00, 17:45 – 18:45 and 16:45 – 17:45, respectively and the surveyed traffic flows are presented in **Figure 3.2**.

3.3 Existing Junction Capacity Assessment

3.3.1 Based on the existing traffic flows, the performances of the key junctions during the peak hour were assessed. The results are summarized and presented in **Table 3.1** and the detailed junction capacity calculation sheets are attached in **Appendix A**.

Table 3.1 Existing Junction Performance

No.	Junction Location	Type/ Capacity Index ⁽¹⁾	Junction Performance		
			Weekday AM Peak	Weekday PM Peak	Weekend Peak
J1	Hiram's Highway/Hing Keng Shek Road Roundabout	Roundabout /DFC	0.51	0.43	0.50
J2	Hiram's Highway/Ho Chung Road	Signalized/RC	106%	78%	97%
J3	Hiram's Highway/ New Hiram's Highway/ Nam Pin Wai Road	Roundabout/ DFC	0.58	0.57	0.50

Note: (1) RC = Reserve Capacity; DFC = Design Flow to Capacity ratio

3.4 Existing Link Capacity Assessment

3.4.1 The Volume to Capacity (V/C) Ratios of Hiram's Highway were assessed and the results are presented in **Table 3.2**.

Table 3.2 Link Capacity Assessment

Direction	Bound	Capacity (pcu/hr) ⁽¹⁾	Traffic Flow (pcu/hr)			V/C Ratio		
			AM	PM	WN	AM	PM	WN
Hiram's Highway ⁽²⁾	EB	1,020	761	1,005	974	0.75	0.99	0.95
	WB	1,020	1,031	785	1,008	1.01	0.77	0.99
Hiram's Highway ⁽³⁾	NB	3,120	750	1,019	1,005	0.24	0.33	0.32
	SB	3,120	1,083	799	1,039	0.35	0.26	0.33
Hiram's Highway ⁽⁴⁾	NB	3,120	786	1,121	1,066	0.25	0.36	0.34
	SB	3,120	1,177	850	1,065	0.38	0.27	0.34
Hiram's Highway ⁽⁵⁾	NB	3,120	878	1,256	1,149	0.28	0.40	0.37
	SB	3,120	1,282	932	1,133	0.41	0.30	0.36
Hing Keng Shek Road	2-way	120	51	38	69	0.43	0.32	0.58

- Notes: AM – Weekday AM Peak Hour; PM – Weekday PM Peak Hour; WN – Weekend Peak Hour
(1) Capacity refers to TPDM Vol.2 Ch. 2.4. A factor of 1.2 (based on the traffic count survey result) is adopted to convert the capacity from veh/hr to pcu/hr.
(2) The section between Hing Keng Shek Road and Pak Sha Wan Street.
(3) The section between access of Luk Cheung Road and Hing Keng Shek Road.
(4) The section between Ho Chung Road and Luk Mei Tsuen Road.
(5) The section between Nam Pin Wai Road and Ho Chung Road.

3.4.2 As shown in **Table 3.2**, the concerned road sections are operating with spare capacity during weekday AM, weekday PM and weekend peak hours, except the section of Hiram's Highway between Hing Keng Shek Road and Pak Sha Wan Street which is operating at its capacity.

3.5 Public Transport Services

3.5.1 At present, there are few franchised bus and green minibus routes travelling along Hiram's Highway and the details of these routes. The nearby bus stops of the Site are listed out in **Table 3.3** and shown in **Figure 3.3**, respectively.

Table 3.3 Existing Public Transport Services

Mode	Route No.	Terminating Points	Frequency (min)
Bus	92	Sai Kung – Diamond Hill Station	15 – 30
	96R ⁽¹⁾	Wong Shek Pier – Diamond Hill Station	25 – 30
	292P	Sai Kung – Kwun Tong (Yue Man Square)	07:30
	792M	Sai Kung – Tseung Kwan O Station	15 – 30
GMB	1	Sai Kung – Kowloon Bay (Telford Gardens)	8 – 20
	1A	Sai Kung – Diamond Hill (Choi Hung Road) Public Transport Interchange	4
	1S ⁽²⁾	Sai Kung – Diamond Hill (Choi Hung Road) Public Transport Interchange	10 – 15
	2	Sai Kung – Ho Chung	15 – 30
	12	Sai Kung – Po Lam	10 – 15

Mode	Route No.	Terminating Points	Frequency (min)
	101M	Sai Kung – Hang Hau Station (via Sai Kung North Public Transport Interchange)	3 – 30

Note: (1) Service on Saturdays, Sundays and Holidays.
(2) Overnight Service.

3.5.2 An on-site observation was carried out at the nearest bus stops, "Pak Wai (SK144)" and "Pak Wai (SK458)" on 13 October 2025 (Monday) during the peak period of 07:30 – 09:30 and 17:30 – 19:30, to identify the occupancy of the franchised bus and green minibus services in the AM and PM peak. The identified weekday AM and weekday PM peak hours were 08:00 – 09:00 and 17:30 – 18:30, respectively and the results are summarized in Tables 3.4 – 3.5.

Table 3.4 Occupancy of Existing Franchised Bus and Green Minibus Services during AM Peak Hour

Route No.	Observed Vehicular Trips	Passenger Capacity ⁽¹⁾	Passengers on Bus upon Arrival	Total No. of passengers		Passengers on Bus upon Leave	Occupancy
				Boarding	Alighting		
		[a]	[b]	[c]	[d]	[e] = [b]+[c]-[d]	[f] = [e] / [a]
Sai Kung Bound (at Bus Stop "Pak Wai SK144")							
Bus 92	3	360	85	1	5	81	23%
Bus 792M	3	360	60	0	0	60	17%
GMB 1	7	112	58	3	1	60	54%
GMB 1A	22	352	227	4	7	224	64%
GMB 2	3	48	26	0	0	26	54%
GMB 12	4	64	25	0	1	24	38%
GMB 101M	20	320	180	0	3	177	55%
Total	62	1,616	661	8	17	652	40%
Kowloon Bound (at Bus Stop "Pak Wai SK458")							
Bus 92	2	240	95	4	2	97	40%
Bus 792M	2	240	85	7	0	92	38%
GMB 1	4	64	64	0	0	64	100%
GMB 1A	23	368	359	3	0	362	98%
GMB 2	2	32	18	0	0	18	56%
GMB 12	2	32	32	0	0	32	100%
GMB 101M	17	272	272	0	0	272	100%
Total	52	1248	925	14	2	937	75%

Note: (1) Assume the capacity of each franchised bus and green minibus is 120 pax and 16 pax, respectively.

Table 3.5 Occupancy of Existing Franchised Bus and Green Minibus Services during PM Peak Hour

Route No.	Observed Vehicular Trips	Passenger Capacity ⁽¹⁾	Passengers on Bus upon Arrival	Total No. of passengers		Passengers on Bus upon Leave	Occupancy
				Boarding	Alighting		
		[a]	[b]	[c]	[d]	[e] = [b]+[c]-[d]	[f] = [e] / [a]
Sai Kung Bound (at Bus Stop "Pak Wai SK144")							
Bus 92	2	240	90	0	3	87	36%
Bus 792M	3	360	150	0	2	148	41%
GMB 1	1	16	15	0	0	15	94%
GMB 1A	29	464	464	0	7	457	98%
GMB 2	2	32	15	1	0	16	50%
GMB 12	1	16	10	0	0	10	63%
GMB 101M	21	336	328	0	8	320	95%
Total	59	1,464	1,072	1	20	1,053	72%
Kowloon Bound (at Bus Stop "Pak Wai SK458")							
Bus 92	4	480	350	7	0	357	74%
Bus 792M	2	240	150	0	0	150	63%
GMB 1	1	16	16	0	0	16	100%
GMB 1A	25	400	400	1	1	400	100%
GMB 2	2	32	32	0	0	32	100%
GMB 12	0	0	0	0	0	0	0%
GMB 101M	26	416	400	2	0	402	97%
Total	60	1,584	1,348	10	1	1,357	86%

Note: (1) Assume the capacity of each franchised bus and green minibus is 120 pax and 16 pax, respectively.

4 FUTURE TRAFFIC SITUATION

4.1 Design Year

4.1.1 The completion year of the proposed development is expected to be 2031. As a result, the design year of the traffic impact assessment should be three years after the completion year, i.e., 2034.

4.2 Traffic Forecast

ATC Historical Data

4.2.1 Reference was made to the 2019 to 2023 Annual Traffic Census Reports, published by the Transport Department, to determine the traffic growth. The traffic data recorded at the counting stations in the vicinity of the Development Site is shown in **Table 4.1**.

Table 4.1 Annual Traffic Census Data

Stn. No.	Road Section			AADT ⁽¹⁾					Average Growth%
	Road	From	To	2019	2020	2021	2022	2023	
5017	Clear Water Bay Rd	On Sau Rd	Hiram's Highway	28,980	28,900 (-0.3%)	29,100 (0.7%)	27,720 (-4.7%)	29,080 (4.9%)	0.1%
5466	Clear Water Bay Rd	Hang Hau Rd	Hiram's Highway	20,240	19,110 (-5.6%)	20,020 (4.8%)	19,140 (-4.4%)	19,160 (0.1%)	-1.4%
6055	Hiram's Highway	Clear Water Bay Rd	Po Tung Rd	24,280	23,360 (-3.8%)	24,460 (4.7%)	23,480 (-4%)	22,860 (-2.6%)	-1.5%
Total				73,500	71,370 (-2.9%)	73,580 (3.1%)	70,340 (-4.4%)	71,100 (1.1%)	-0.8%

Note: (1) Figures in bracket indicated the % increase between two years.

4.2.2 **Table 4.1** shows that the AADT at the concerned ATC stations has an overall annual growth of negative 0.8% in between the years 2019 to 2023.

Territorial Population and Employment Data Matrix (TPEDM) Projection Data

4.2.3 Reference was also made to the 2019–based TPEDM published by the Planning Department. The population and employment data of year 2019 and 2031 in the Southeast New Territories (Other Area) are summarized in **Table 4.2**.

Table 4.2 Population and Employment Data in Southeast New Territories (Other Area)

Year	2019	2026	2031
Population	68,900	65,800	59,750
Employment	27,250	27,750	28,100
Total	96,150	93,550	87,850
Average Annual Growth %		-0.4% (2019 to 2026)	-1.2% (2026 to 2031)

4.2.4 As shown in **Table 4.2**, the projected average annual growth rates of the population and employment total number under the TPEDM in Southeast New Territories (Other Area) are negative 0.4% and negative 1.2% between the years 2019 – 2026 and 2026 – 2031, respectively. Having considered the rates derived from ATC and TPEDM data, to be conservative, a nominal growth rate of +1.0% will be adopted for the subsequent traffic forecast.

4.3 Traffic Generation of the Proposed Development

4.3.1 Reference was also made to the latest set of traffic generation and attraction rates documented in Chapter 3 "Transport Considerations of Town Plans" of the TPDM, for the estimation of the traffic generated by the proposed development. The traffic generation and attraction numbers were shown in **Table 4.3**

Table 4.3 Development Traffic Generation

Proposed Use	Unit / Content	Weekday AM Peak Hour			Weekday PM Peak Hour			Weekend Peak Hour ⁽¹⁾		
		Gen.	Att.	Total	Gen.	Att.	Total	Gen.	Att.	Total
Mean Trip rates from TPDM										
Residential – 80m ²	pcu/hr/flat	0.1058	0.0605	-	0.0426	0.0590	-	0.0426	0.0590	-
Traffic Generation/Attraction										
Proposed Development	96 flats	11	6	17	5	6	11	5	6	11

Note: Gen. – Generation; Att. – Attraction.

(1) The trip rates for PM peak hour are adopted for the weekend peak hour.

4.3.2 In view of the above, the proposed development would generate two-way traffic flows of 17 pcu/hr in the weekday AM peak hour, 11 pcu/hr in the weekday PM peak hour and 11 pcu/hr in the weekend peak hour. The traffic distribution is shown diagrammatically in **Figure 4.1**.

4.4 Planned and Approved Developments

4.4.1 To estimate the future traffic flows generated and attracted by the nearby planned and approved developments, updated information has been obtained from available information regarding the planned and approved developments in the vicinity of the proposed development site, the details of these developments are listed in **Table 4.4**.

Table 4.4 Planned and Approved Developments

Ref.	Location	Use	Development Parameters
A	Various Lot in D.D. 210, Ho Chung	Residential	2,422 m ² GFA (15 flats)
B	Lot 1003 in D.D. 214, Ho Chung	Residential	5,344 m ² GFA (90 flats)
C	Lot 2189 in D.D. 244, Nam Pin Wai	Residential	8,320 m ² GFA (139 flats)
D	Various Lots in D.D. 244 and Adjoining Government Land, Ho Chung, Sai Kung	Residential	13,719 m ² GFA (58 flats)
E	Various Lots in D.D. 210 and 244 and Adjoining Government Land, Ho Chung, Sai Kung	Residential	2,393 m ² GFA (8 flats)

4.4.2 Reference is made to Volume 1 of the TPDM published by the TD on the trip rates of the foregoing developments to estimate their traffic generation and attraction. The total traffic generation and attraction by these adjacent planned/committed developments are summarized in **Table 4.5**.

Table 4.5 Traffic Generation of the Planned and Approved Developments

Use	Use / Content	AM Peak Hour			PM Peak Hour			Weekend Peak Hour ⁽¹⁾		
		Gen.	Att.	Total	Gen.	Att.	Total	Gen.	Att.	Total
Adopted TPDM Mean Trip Rates										
Residential – 60m ²	pcu/hr/flat	0.0718	0.0425	-	0.0286	0.0370	-	0.0286	0.0370	-
Residential – 180m ²	pcu/hr/flat	0.2772	0.1769	-	0.1635	0.2394	-	0.1635	0.2394	-
Residential – 240m ²	pcu/hr/flat	0.3012	0.2189	-	0.2235	0.3234	-	0.2235	0.3234	-
Residential – 300m ²	pcu/hr/flat	0.3252	0.2609	-	0.2835	0.4074	-	0.2835	0.4074	-
Traffic Generation										
Site A	15 flats	5	3	8	3	4	7	3	4	7
Site B	90 flats	7	4	11	3	4	7	3	4	7
Site C	139 flats	10	6	16	4	6	10	4	6	10
Site D	58 flats	18	13	31	13	19	32	13	19	32
Site E	8 flats	3	3	6	3	4	7	3	4	7
Total		43	29	72	26	37	63	26	37	63

Note: Gen. – Generation; Att. – Attraction.

(1) The trip rates for PM peak hour are adopted for weekend peak hour.

4.4.3 As shown in **Table 4.5**, the planned/committed developments will generate a total two-way traffic of 72, 63 and 63 pcu/hr during weekday AM, weekday PM peak hour and weekend peak hour respectively. The estimated traffic generation will be assumed to be travelling in the local road network in the same proportions as the existing traffic demands when traffic forecast is prepared in this Study.

4.5 Reference and Design Flows

4.5.1 The 2034 Reference Flows, i.e. the traffic flows in the vicinity without the proposed development, were estimated based on the following equation.

$$2034 \text{ Reference Flows} = 2025 \text{ Existing Traffic Flows} \times (1 + 1.0\%)^9$$

4.5.2 The 2034 Design Flows, i.e. the traffic flows in the local road network with the traffic generated by the proposed residential development, were estimated based on the following equation:

$$2034 \text{ Design Flows} = 2034 \text{ Reference Flows} + \text{Traffic Flows Generated by the Proposed Development}$$

4.5.3 The 2034 Reference and Design Flows are shown in **Figures 4.2** and **4.3**, respectively.

4.6 Junction Capacity Assessment

4.6.1 Junction capacity analysis was carried out for the assessment year 2034. The assessment results are shown in **Table 4.6** and the detailed calculation sheets are attached in **Appendix B**.

Table 4.6 2034 Junction Capacity Assessments

No.	Junction Location	Type/ Index ⁽¹⁾	Reference			Design		
			AM	PM	WN	AM	PM	WN
J1	Hiram's Highway/Hing Keng Shek Road Roundabout	Roundabout /DFC	0.56	0.47	0.55	0.56	0.47	0.55
J2	Hiram's Highway/Ho Chung Road	Signalized /RC	86%	69%	80%	85%	67%	80%
J3	Hiram's Highway/ New Hiram's Highway/ Nam Pin Wai Road	Roundabout/ DFC	0.65	0.63	0.56	0.65	0.64	0.56

Notes: AM – Weekday AM Peak Hour; PM – Weekday PM Peak Hour; WN – Weekend Peak Hour.
(1) RC = Reserved Capacity; DFC = Design Flow to Capacity Ratio.

4.6.2 As shown in **Table 4.6**, all the concerned junctions will perform with spare capacity for both the Reference and Design Scenarios in 2034. Therefore, the adjacent road network will be able to cope with the traffic generated by the proposed development.

4.7 Link Capacity Assessment

4.7.1 The V/C Ratios of Hiram's Highway were assessed and the results are presented in **Table 4.7**.

Table 4.7 Year 2034 Link Capacity Assessments

Direction	Bound	Capacity (pcu/hr) ⁽¹⁾	Traffic Flow (pcu/hr)			V/C Ratio		
			AM	PM	WN	AM	PM	WN
2034 Reference Scenario								
Hiram's Highway ⁽²⁾⁽³⁾	EB	3,120	835	1,102	1,067	0.27	0.35	0.34
	WB	3,120	1,130	862	1,105	0.36	0.28	0.35
Hiram's Highway ⁽⁴⁾	NB	3,120	823	1,116	1,101	0.26	0.36	0.35
	SB	3,120	1,186	877	1,139	0.38	0.28	0.37
Hiram's Highway ⁽⁵⁾	NB	3,120	869	1,236	1,176	0.28	0.40	0.38
	SB	3,120	1,294	936	1,171	0.41	0.30	0.38
Hiram's Highway ⁽⁶⁾	NB	3,120	987	1,331	1,253	0.32	0.43	0.40
	SB	3,120	1,412	1,028	1,248	0.45	0.33	0.40
Hing Keng Shek Road	2-way	120	56	41	75	0.47	0.34	0.63
2034 Design Scenario								
Hiram's Highway ⁽²⁾⁽³⁾	EB	3,120 ⁽³⁾	836	1,102	1,067	0.27	0.35	0.34
	WB	3,120 ⁽³⁾	1,131	863	1,106	0.36	0.28	0.35

Direction	Bound	Capacity (pcu/hr) ⁽¹⁾	Traffic Flow (pcu/hr)			V/C Ratio		
			AM	PM	WN	AM	PM	WN
Hiram's Highway ⁽⁴⁾	NB	3,120	828	1,121	1,106	0.27	0.36	0.35
	SB	3,120	1,196	882	1,144	0.38	0.28	0.37
Hiram's Highway ⁽⁵⁾	NB	3,120	874	1,241	1,181	0.28	0.40	0.38
	SB	3,120	1,304	941	1,176	0.42	0.30	0.38
Hiram's Highway ⁽⁶⁾	NB	3,120	992	1,336	1,258	0.32	0.43	0.40
	SB	3,120	1,422	1,033	1,253	0.46	0.33	0.40
Hing Keng Shek Road	2-way	960 ⁽⁷⁾	73	52	86	0.08	0.05	0.09

- Notes: AM – Weekday AM Peak Hour; PM – Weekday PM Peak Hour; WN – Weekend Peak Hour
- (1) Capacity refers to TPDM Vol.2 Ch. 2.4. A factor of 1.2 (based on the traffic count survey result) is adopted to convert the capacity from veh/hr to pcu/hr.
 - (2) The section between Hing Keng Shek Road and Pak Sha Wan Street.
 - (3) The section of Hiram's Highway will be widened to dual two-lane carriageway under Hiram's Highway Improvement Stage 2.
 - (4) The section between access of Luk Cheung Road and Hing Keng Shek Road.
 - (5) The section between Ho Chung Road and Luk Mei Tsuen Road.
 - (6) The section between Nam Pin Wai Road and Ho Chung Road.
 - (7) The section between proposed vehicular access and Hiram's Highway will be widened (discussed in **Section 5.1** below).

4.7.2 As shown in **Table 4.6**, all the concerned road sections will operate with capacity during weekday AM, weekday PM and weekend peak hours in both reference and design scenarios.

4.8 Review of Public Transport Facilities

4.8.1 Based on the tentative flat mix, the overall population of the proposed development is about 288. Reference has been made to the published "Travel Characteristics Survey (TCS) 2011 Final Report". According to the Report, the daily mechanized trip rate per population is 1.83 trips (two-way) and the morning peak hour accounted for about 12% of the daily trips for the two-way trips. It is assumed that 90% of the trips are outbound direction in the AM peak hour, while in the PM peak hour, it is anticipated that most of the trips would be inbound direction, 10% of the trips is assumed to be outbound direction. Based on the above and most of residents would use public transport services, the estimated public transport demand of the proposed development in outbound direction in AM and PM peak hour is about 57 pax/hr (i.e. $288 \times 1.83 \times 0.12 \times 0.9$) and 7 pax/hr (i.e. $288 \times 1.83 \times 0.12 \times 0.1$), respectively.

4.8.2 The public transport demand induced by the planned developments mentioned in **Section 4.4** is also considered. According to "Hong Kong Annual Digest of Statistics" published by the Census and Statistic Department, the average household size for the territory in year 2022 is 2.7, this figure is adopted for estimating the population of these developments. By following the methodology described in the aforesaid paragraph, the estimated public transport demand of the planned developments in outbound direction in AM and PM peak hour is about 166 pax/hr (i.e. $(15+90+139+58+8) \times 2.7 \times 1.83 \times 0.12 \times 0.9$) and 19 pax/hr (i.e. $(15+90+139+58+8) \times 2.7 \times 1.83 \times 0.12 \times 0.1$), respectively.

4.8.3 Based on the existing public transport vacancy (as estimated in **Tables 3.4 and 3.5**) and the above projected demand, the existing bus/green minibus services will still operate with capacity after accommodating the future demand induced by the proposed development and the planned developments.

5 PROVISION OF TRANSPORT FACILITIES

5.1 Vehicular Access Arrangement

5.1.1 The vehicular access of the proposed development will be located at Hing Keng Shek Road. It is proposed to widen the existing section of Hing Keng Shek Road between the proposed vehicular access and Hiram's Highway to 6.0m for a 2-lane single carriageway. A 2.0m wide footpath will be also provided within the Site connecting the proposed development and Hiram's Highway and the footpath will also be opened for public use. The proposed traffic arrangement is shown in **Figure 5.1**. The project proponent will be responsible for implementing the improvement works and will undertake the management and maintenance responsibility for the footpath within the Site.

5.1.2 Swept path analysis is conducted to demonstrate the manoeuvring of vehicles entering and leaving the Site via the proposed vehicular access and shown in **Figures 5.2 – 5.3**. To ensure sufficient sightline is provided for the proposed run-out, a sightline analysis is conducted and presented in **Figure 5.4**.

5.2 Internal Transport Facilities

5.2.1 The internal transport facilities for the proposed development will be provided in accordance with the Hong Kong Planning Standards and Guidelines (HKPSG). The required and the proposed provisions for the proposed development are shown in **Table 5.1**.

Table 5.1 Proposed Car Parking and Loading/Unloading Facilities

Type	HKPSG's Requirements						Required Provision	Proposed Provision
Proposed Residential Development (96 flats)								
Car Parking Space	<u>For Residents</u> Parking Requirements = GPS x R1 x R2 x R3 where						2 – 4	4
	Unit Size	No. of Unit	GPS	R1	R2	R3		
	40 m ² <FS ≤ 70 m ²	8	1 space per 4 – 7 units	1.2	1	1.3		
	70 m ² < FS ≤ 100 m ²	88		2.4	1	1.3		
	<u>For Visitors</u> Visitor car parking for private residential developments with more than 75 units per block should be provided at 5 visitor spaces per block in addition to the requirements, or as determined by the Authority. For private residential developments with 75 units or less per block, the visitor car parking provision will be determined by TD on a case-by-case basis.						6	6
TOTAL CAR PARKING						48 – 79	79	
Motorcycle Parking Space	1 space per 100 - 150 flats						1	1
Loading / Unloading Bay	1 bay per residential block or as determined by the Authority.						4	4

5.2.2 **Table 5.2** lists out a summary of the numbers and the dimensions required for each type of spaces in the proposed development. The proposed car park layout plan is enclosed in **Appendix C**.

Table 5.2 Summary of Overall Transport Facilities Provision

Facilities	Dimensions	Proposed Provision
Car Parking Space	2.5m (W) x 5.0m (L) x 2.4 (H)	77
Disable Car Parking Space	3.5m (W) x 5.0m (L) x 2.4 (H)	2
Goods Vehicle Loading and Unloading Bay	3.5m (W) x 11.0m (L) x 4.7m(H)	4
Motorcycle Parking Space	1.0m (W) x 2.4m (L) x 2.4 (H)	1

5.3 Pedestrian Access Arrangement

5.3.1 At present, there is a cautionary crossing for pedestrians to walk across Hiram's Highway to reach the bus layby for Kowloon/TKO-bound buses. For the pedestrians to walk to the bus layby for Sai Kung-bound buses, it is proposed to improve the pedestrian connectivity by providing a cautionary crossing to walk across Hing Keng Shek Road as shown in **Figure 5.5**.

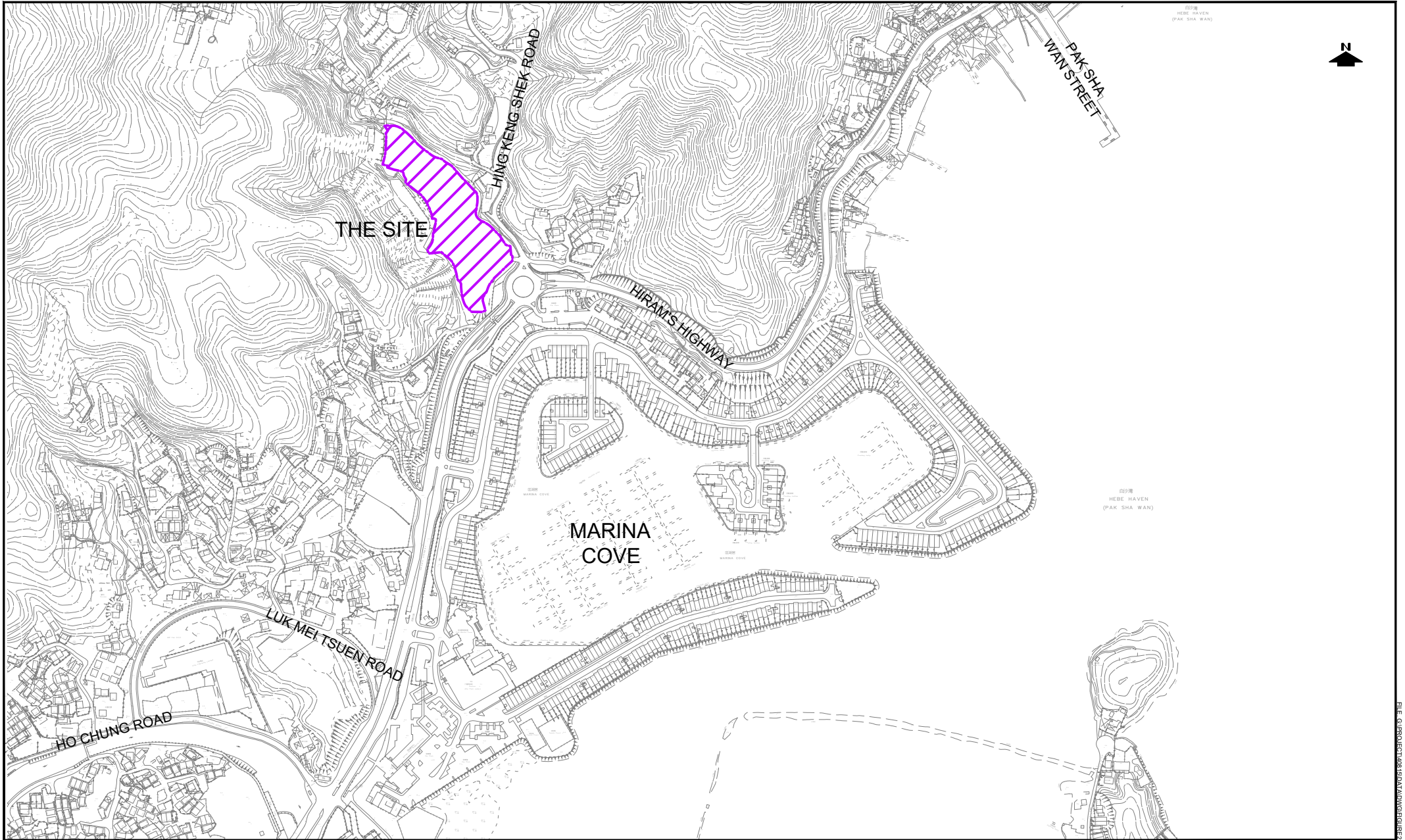
6 SUMMARY AND CONCLUSION

6.1 Summary

- 6.1.1 The Applicant intends to develop the Site into a residential development at various lots in D.D.210, Pak Wai, Sai Kung. The proposal will have about 96 residential units.
- 6.1.2 Traffic count surveys were carried out on 13 October 2025 (Monday) during 07:30 – 09:30 and 17:30 – 19:30 and 11 October 2025 (Saturday) from 12:00 to 19:00. The identified weekday AM, weekday PM and weekend peak hours were 08:00 – 09:00, 17:45 – 18:45 and 16:45 – 17:45, respectively. Junction and link capacity assessment based on the observed flows shows that all concerned junctions and road links are performing satisfactorily during weekday AM, weekday PM and weekend peak hours, except the section of Hiram's Highway between Hing Keng Shek Road and Pak Sha Wan Street which is operating at its capacity.
- 6.1.3 The proposed development would generate two-way traffic flows of 17 pcu/hr in the weekday AM peak hour, 11 pcu/hr in the weekday PM peak hour and 11 pcu/hr in the weekend peak hour. By assigning the additional development traffic to the 2034 Reference Flows, the 2034 Design Flows were obtained.
- 6.1.4 Junction and link capacity assessments were carried out for the key junctions and road links in the vicinity for the year 2034. The results indicated that all junctions and road links will operate satisfactorily for both reference and design scenarios. Therefore, it is anticipated that the proposed development will not induce significant traffic impact to the surrounding road network.
- 6.1.5 Public transport facilities has also been reviewed, the existing bus/green minibus services will still operate with capacity after accommodating the future demand induced by the proposed development and the planned developments.
- 6.1.6 The vehicular access of the proposed development will be located at Hing Keng Shek Road. It is proposed to widen the existing section of Hing Keng Shek Road between the proposed vehicular access and Hiram's Highway to 6.0m for a 2-lane single carriageway. A 2.0m wide footpath will be also provided within the Site connecting the proposed development and Hiram's Highway and the footpath will also be opened for public use. The project proponent will be responsible for implementing the improvement works and will undertake the management and maintenance responsibility for the footpath within the Site. In order to improve the pedestrian connectivity, the project proponent will also be responsible for the construction of a cautionary crossing to walk across Hing Keng Shek Road under the proposed development.
- 6.1.7 The internal transport facilities of the proposed development will be provided in accordance with the recommendations in the HKPSG. The proposed development will provide a total of 79 private car parking spaces (including 2 nos. of parking space for disabled users), 1 motorcycle parking space and 4 goods vehicle loading / unloading bays.

6.2 Conclusion

- 6.2.1 The findings of the traffic impact assessment indicated that the road network in the vicinity of the Site would be able to cope with the proposed development and the project is considered acceptable in traffic viewpoint.

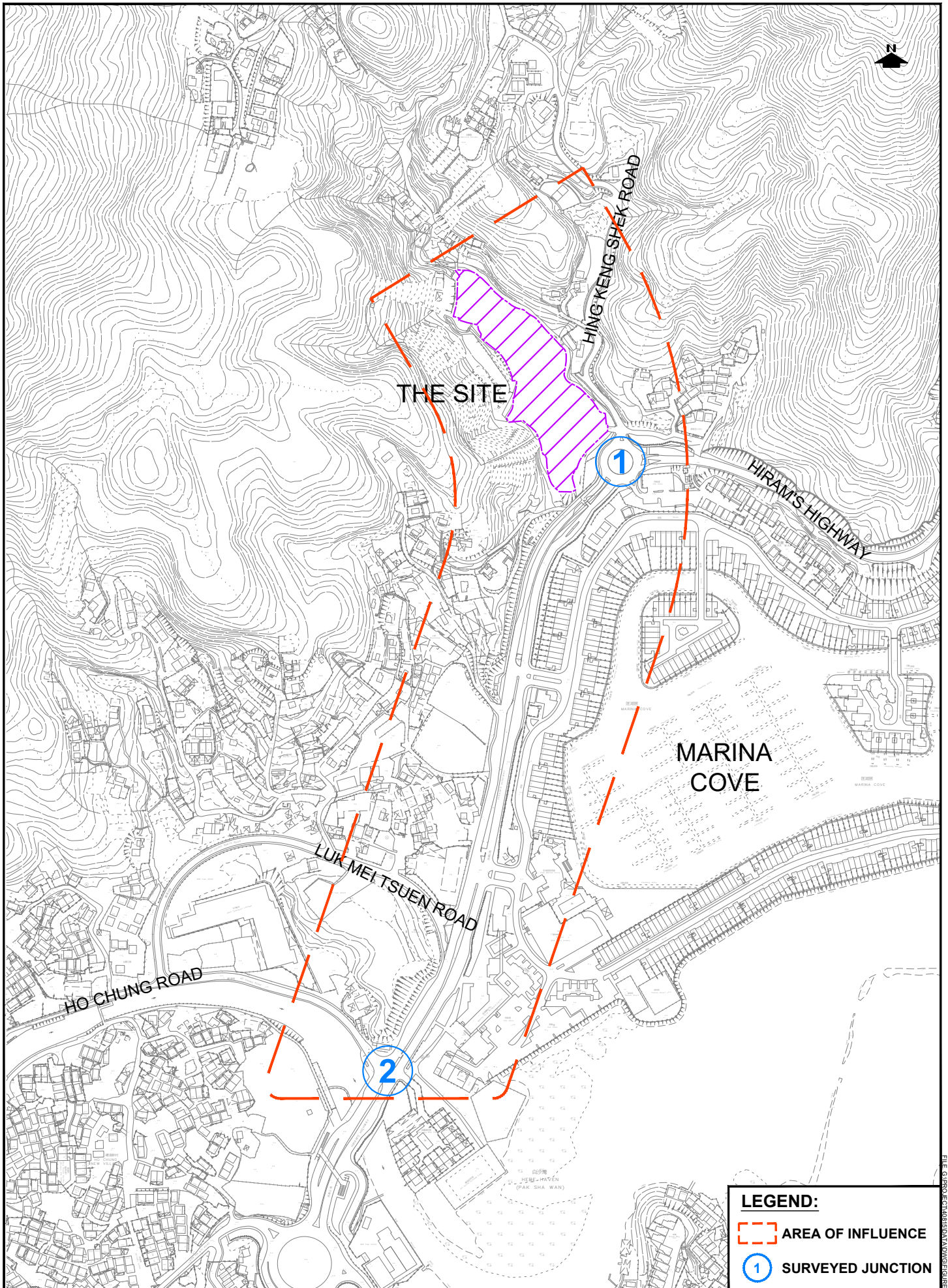


PROJECT NO.	40815	
DESIGNED	SLN	DATE JUL 2025
DRAWN	CLL	SCALE 1:6000
CHECKED	SLN	

PROJECT TITLE APPLICATION FOR AMENDMENT OF PLAN UNDER SECTION 12A OF THE TOWN PLANNING ORDINANCE (CAP. 131) TO REZONE THE APPLICATION SITE FROM "GREEN BELT" AND AREA SHOWN AS "ROAD" TO "RESIDENTIAL (GROUP C)5" FOR PROPOSED RESIDENTIAL DEVELOPMENT AT VARIOUS LOTS IN D.D. 210 AND ADJOINING GOVERNMENT LAND, PAK WAI, SAI KUNG

DRAWING TITLE	LOCATION PLAN	
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DRAWING NO.	FIGURE 2.1	REV.	B
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PROJECT NO.	40815
DESIGNED	SLN
DRAWN	CLL
CHECKED	SLN
DATE	JUL 2025
SCALE	1:5000

PROJECT TITLE	APPLICATION FOR AMENDMENT OF PLAN UNDER SECTION 12A OF THE TOWN PLANNING ORDINANCE (CAP. 131) TO REZONE THE APPLICATION SITE FROM "GREEN BELT" AND AREA SHOWN AS "ROAD" TO "RESIDENTIAL (GROUP C)" FOR PROPOSED RESIDENTIAL DEVELOPMENT AT VARIOUS LOTS IN D.D. 210 AND ADJOINING GOVERNMENT LAND, PAK WAI, SAI KUNG
DRAWING TITLE	LOCATION OF SURVEYED JUNCTIONS

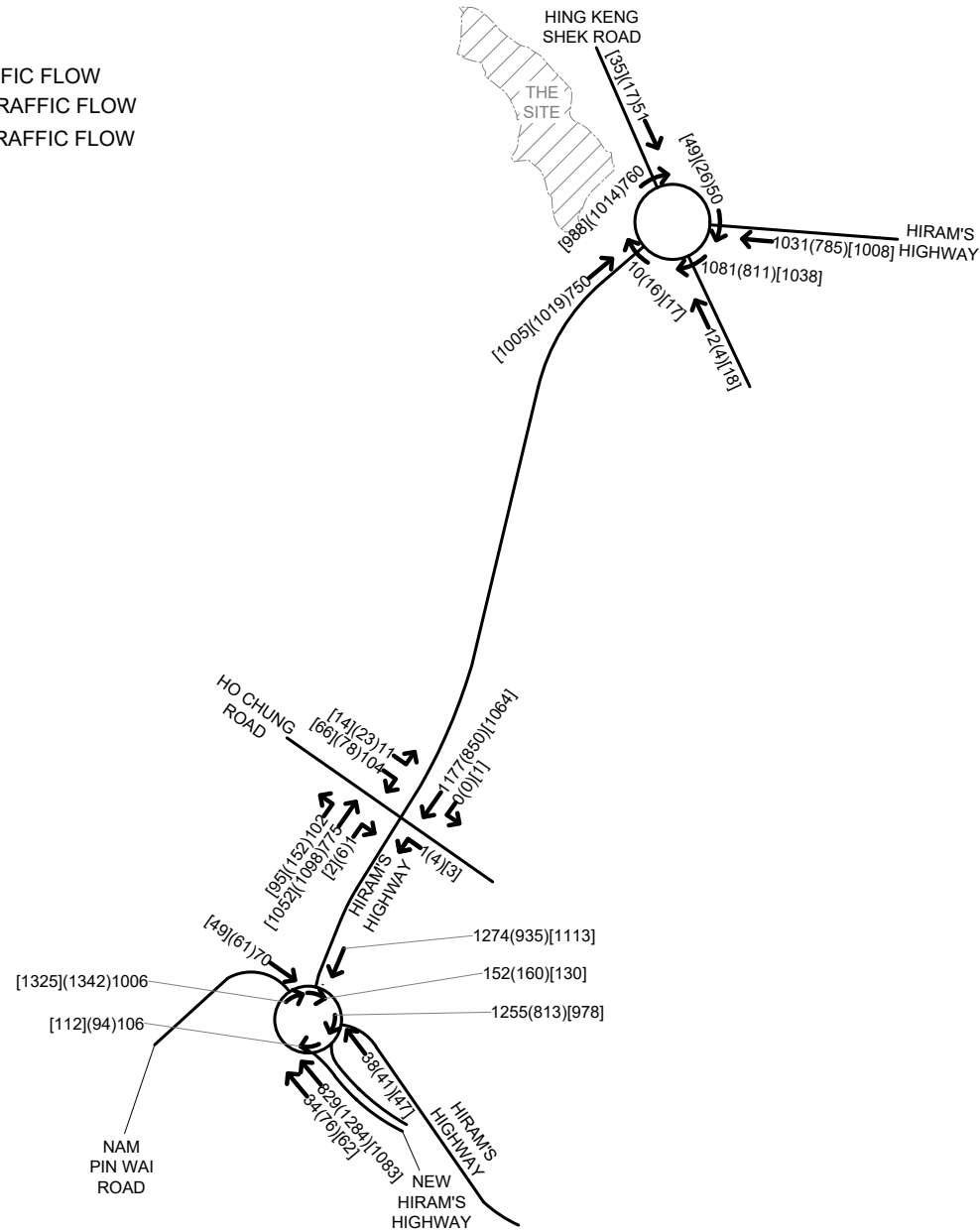
DRAWING NO.	FIGURE 3.1	REV.	B
		顧問有限公司 Consultancy Limited	

LEGEND:

- 312(158)[361] ← WEEKEND PEAK HOUR TRAFFIC FLOW
- ↑ WEEKDAY PM PEAK HOUR TRAFFIC FLOW
- ↑ WEEKDAY AM PEAK HOUR TRAFFIC FLOW

NOTE:

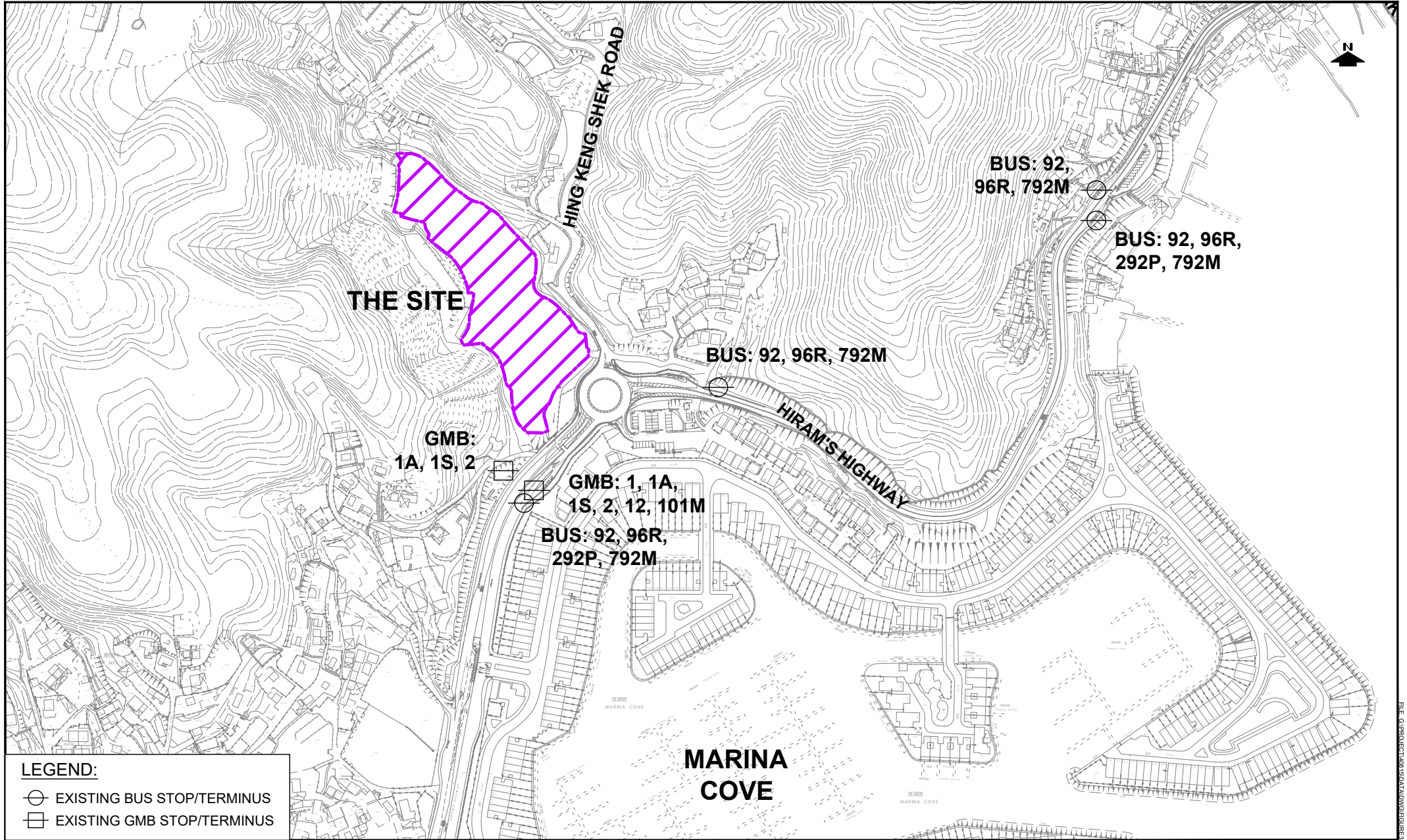
1. ALL TRAFFIC FLOWS ARE IN PCU/HOUR
2. MINOR ROADS ARE NOT SHOWN FOR CLARITY



PROJECT NO.	40815	
DESIGNED	SLN	DATE OCT 2025
DRAWN	CLL	SCALE N.T.S.
CHECKED	SLN	

PROJECT TITLE	APPLICATION FOR AMENDMENT OF PLAN UNDER SECTION 12A OF THE TOWN PLANNING ORDINANCE (CAP. 131) TO REZONE THE APPLICATION SITE FROM "GREEN BELT" AND AREA SHOWN AS "ROAD" TO "RESIDENTIAL (GROUP C)5" FOR PROPOSED RESIDENTIAL DEVELOPMENT AT VARIOUS LOTS IN D.D. 210 AND ADJOINING GOVERNMENT LAND, PAK WAI, SAI KUNG
DRAWING TITLE	2025 EXISTING TRAFFIC FLOWS

DRAWING NO.	FIGURE 3.2	REV.	D
顧問有限公司 Consultancy Limited			



LEGEND:

- ⊕ EXISTING BUS STOP/TERMINUS
- ⊞ EXISTING GMB STOP/TERMINUS

PROJECT NO.	40815	
DESIGNED	SLN	DATE JUL 2025
DRAWN	CLL	SCALE 1:4000
CHECKED	SLN	

PROJECT TITLE APPLICATION FOR AMENDMENT OF PLAN UNDER SECTION 12A OF THE TOWN PLANNING ORDINANCE (CAP. 131) TO REZONE THE APPLICATION SITE FROM "GREEN BELT" AND AREA SHOWN AS "ROAD" TO "RESIDENTIAL (GROUP C)" FOR PROPOSED RESIDENTIAL DEVELOPMENT AT VARIOUS LOTS IN D.D. 210 AND ADJOINING GOVERNMENT LAND, PAK WAI, SAI KUNG

DRAWING TITLE	PUBLIC TRANSPORT FACILITIES IN THE VICINITY	
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DRAWING NO.	FIGURE 3.3	REV.	B
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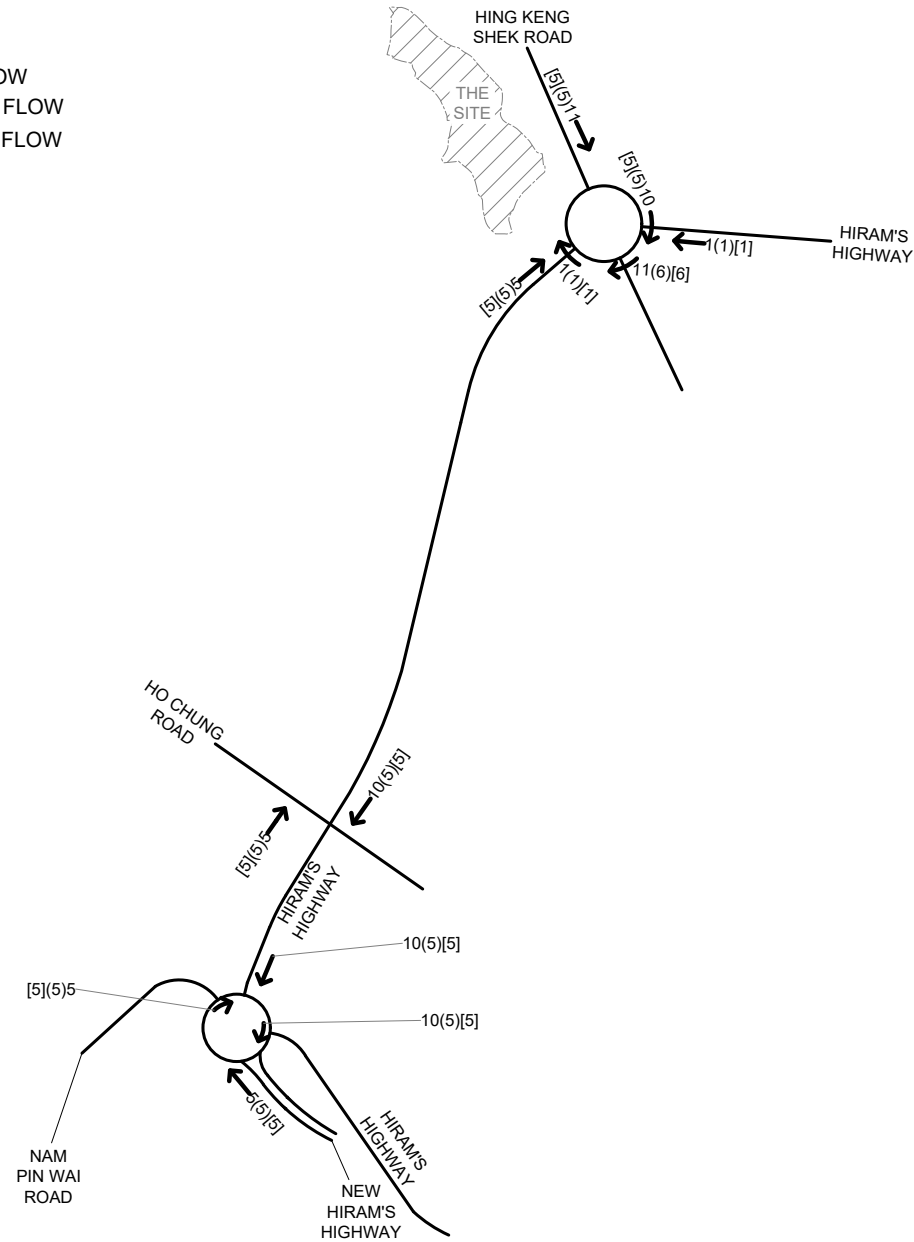
LLA 顧問有限公司
Consultancy Limited

LEGEND:

- 312(158)[361] ← WEEKEND PEAK HOUR TRAFFIC FLOW
- ↑ WEEKDAY PM PEAK HOUR TRAFFIC FLOW
- ↑ WEEKDAY AM PEAK HOUR TRAFFIC FLOW

NOTE:

- 1. ALL TRAFFIC FLOWS ARE IN PCU/HOUR
- 2. MINOR ROADS ARE NOT SHOWN FOR CLARITY



PROJECT NO.	40815	
DESIGNED	SLN	DATE DEC 2025
DRAWN	CLL	SCALE N.T.S.
CHECKED	SLN	

PROJECT TITLE	APPLICATION FOR AMENDMENT OF PLAN UNDER SECTION 12A OF THE TOWN PLANNING ORDINANCE (CAP. 131) TO REZONE THE APPLICATION SITE FROM "GREEN BELT" AND AREA SHOWN AS "ROAD" TO "RESIDENTIAL (GROUP C)5" FOR PROPOSED RESIDENTIAL DEVELOPMENT AT VARIOUS LOTS IN D.D. 210 AND ADJOINING GOVERNMENT LAND, PAK WAI, SAI KUNG
DRAWING TITLE	DEVELOPMENT TRAFFIC FLOWS

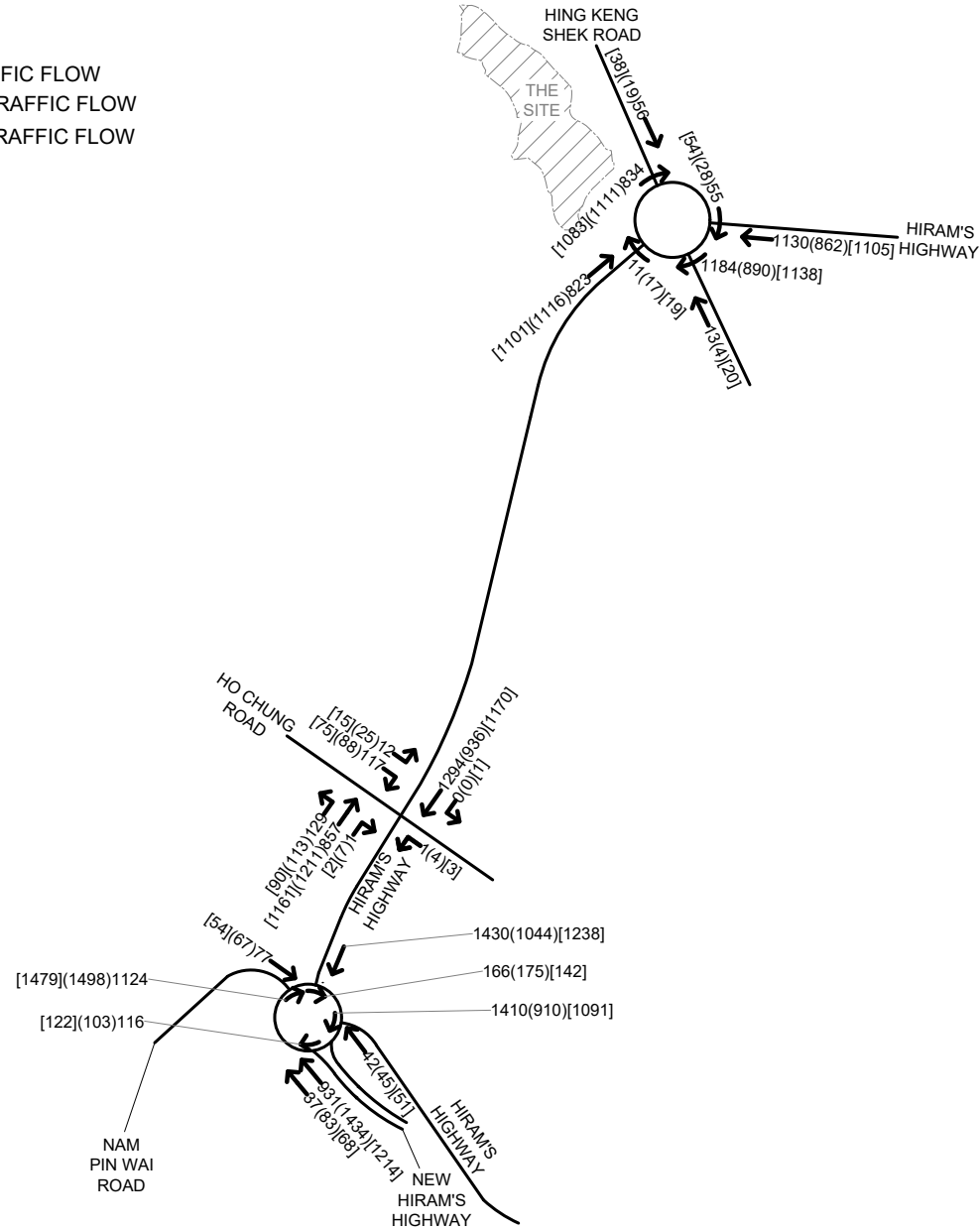
DRAWING NO.	FIGURE 4.1	REV.	F
LLA 顧問有限公司 Consultancy Limited			

LEGEND:

- 312(158)[361] ← WEEKEND PEAK HOUR TRAFFIC FLOW
- ↑ WEEKDAY PM PEAK HOUR TRAFFIC FLOW
- ↑ WEEKDAY AM PEAK HOUR TRAFFIC FLOW

NOTE:

- 1. ALL TRAFFIC FLOWS ARE IN PCU/HOUR
- 2. MINOR ROADS ARE NOT SHOWN FOR CLARITY



PROJECT NO.	40815	
DESIGNED	SLN	DATE
DRAWN	CLL	SCALE
CHECKED	SLN	N.T.S.

PROJECT TITLE	APPLICATION FOR AMENDMENT OF PLAN UNDER SECTION 12A OF THE TOWN PLANNING ORDINANCE (CAP. 131) TO REZONE THE APPLICATION SITE FROM "GREEN BELT" AND AREA SHOWN AS "ROAD" TO "RESIDENTIAL (GROUP C)5" FOR PROPOSED RESIDENTIAL DEVELOPMENT AT VARIOUS LOTS IN D.D. 210 AND ADJOINING GOVERNMENT LAND, PAK WAI, SAI KUNG	
DRAWING TITLE	2034 REFERENCE TRAFFIC FLOWS	

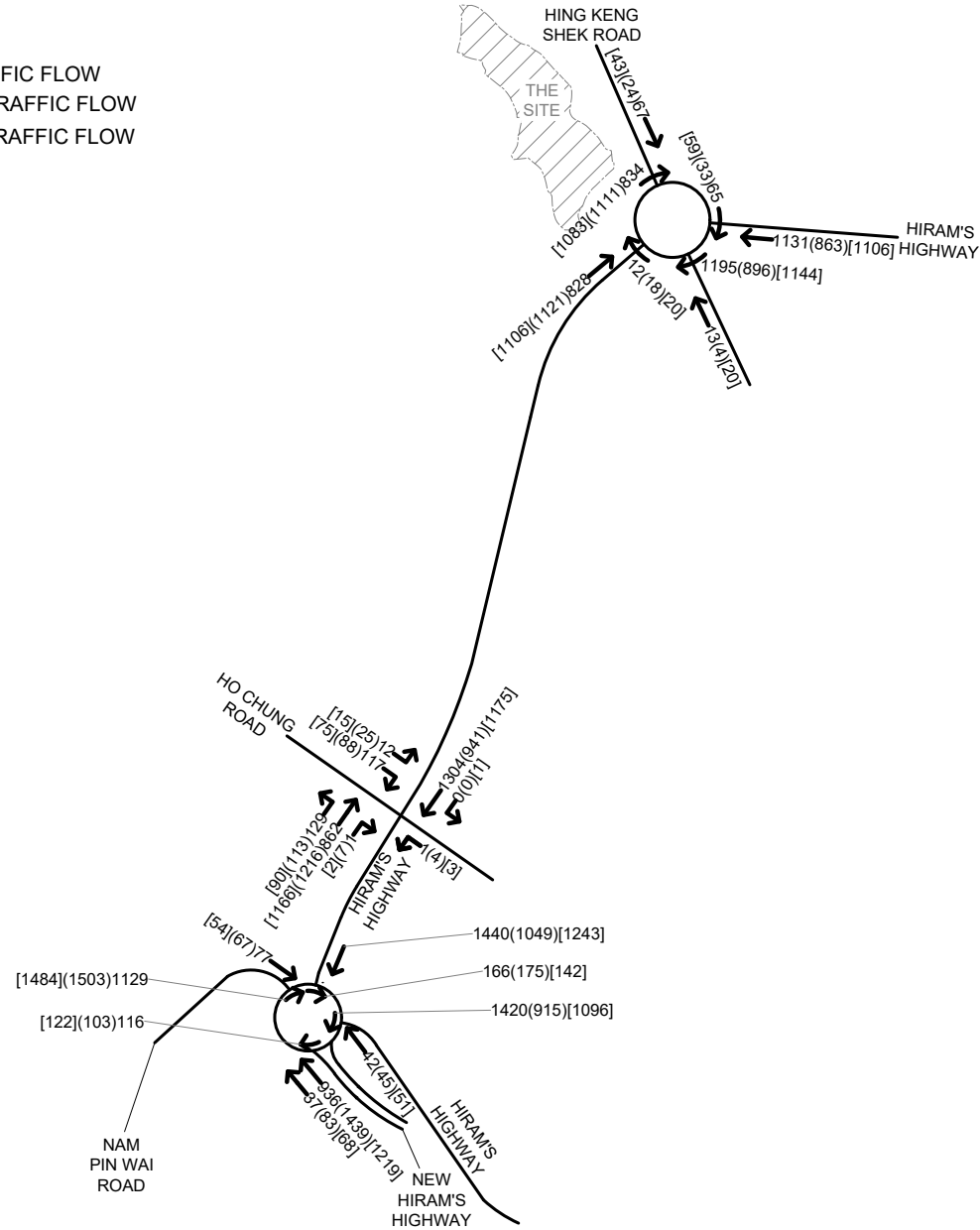
DRAWING NO.	FIGURE 4.2	REV.	E
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LEGEND:

- 312(158)[361] ← WEEKEND PEAK HOUR TRAFFIC FLOW
- ↑ WEEKDAY PM PEAK HOUR TRAFFIC FLOW
- ← WEEKDAY AM PEAK HOUR TRAFFIC FLOW

NOTE:

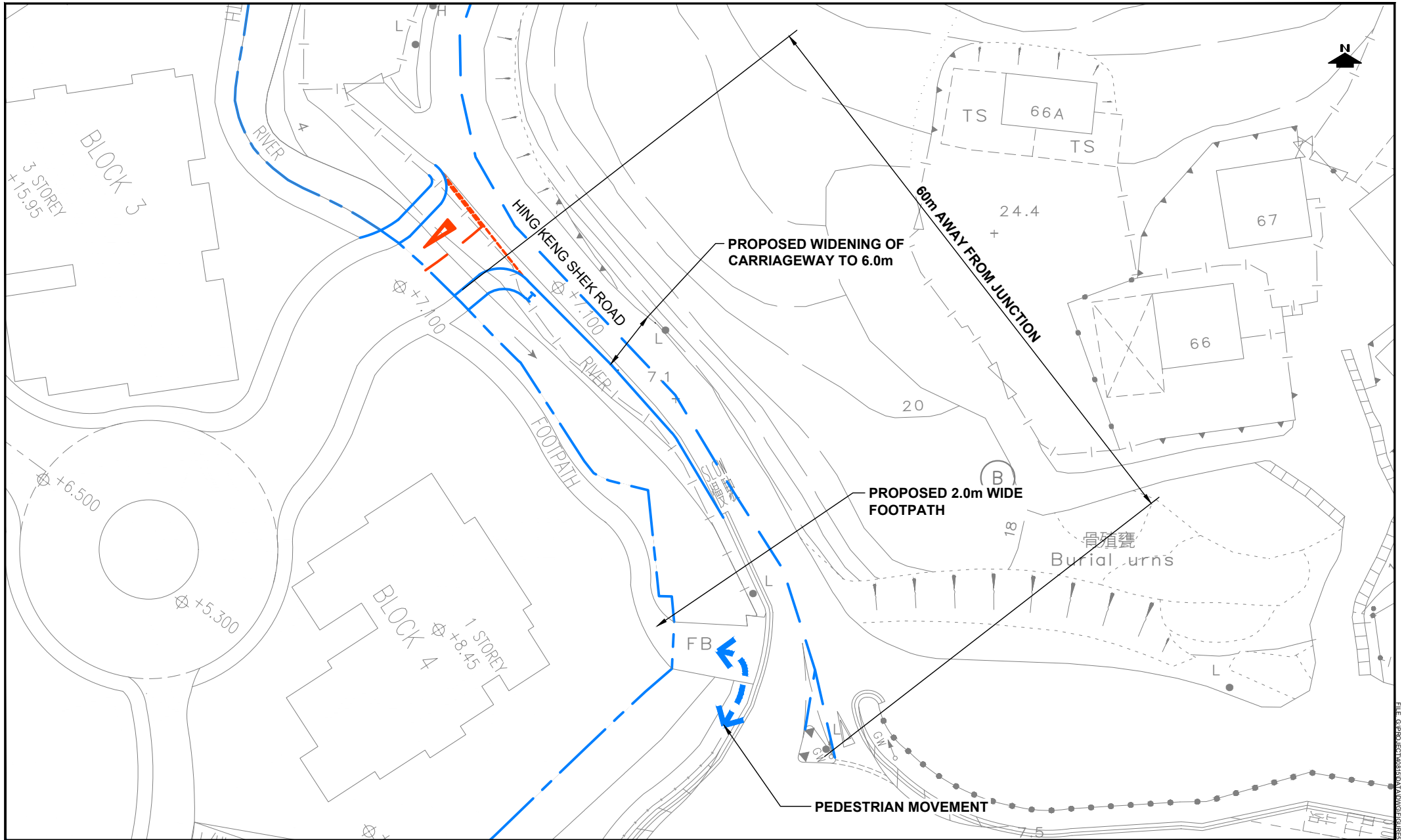
1. ALL TRAFFIC FLOWS ARE IN PCU/HOUR
2. MINOR ROADS ARE NOT SHOWN FOR CLARITY



PROJECT NO.	40815	
DESIGNED	SLN	DATE DEC 2025
DRAWN	CLL	SCALE N.T.S.
CHECKED	SLN	

PROJECT TITLE	APPLICATION FOR AMENDMENT OF PLAN UNDER SECTION 12A OF THE TOWN PLANNING ORDINANCE (CAP. 131) TO REZONE THE APPLICATION SITE FROM "GREEN BELT" AND AREA SHOWN AS "ROAD" TO "RESIDENTIAL (GROUP C)5" FOR PROPOSED RESIDENTIAL DEVELOPMENT AT VARIOUS LOTS IN D.D. 210 AND ADJOINING GOVERNMENT LAND, PAK WAI, SAI KUNG
DRAWING TITLE	2034 DESIGN TRAFFIC FLOWS

DRAWING NO.	FIGURE 4.3	REV.	G

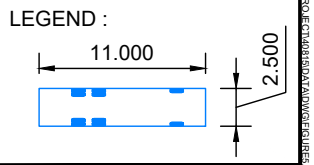
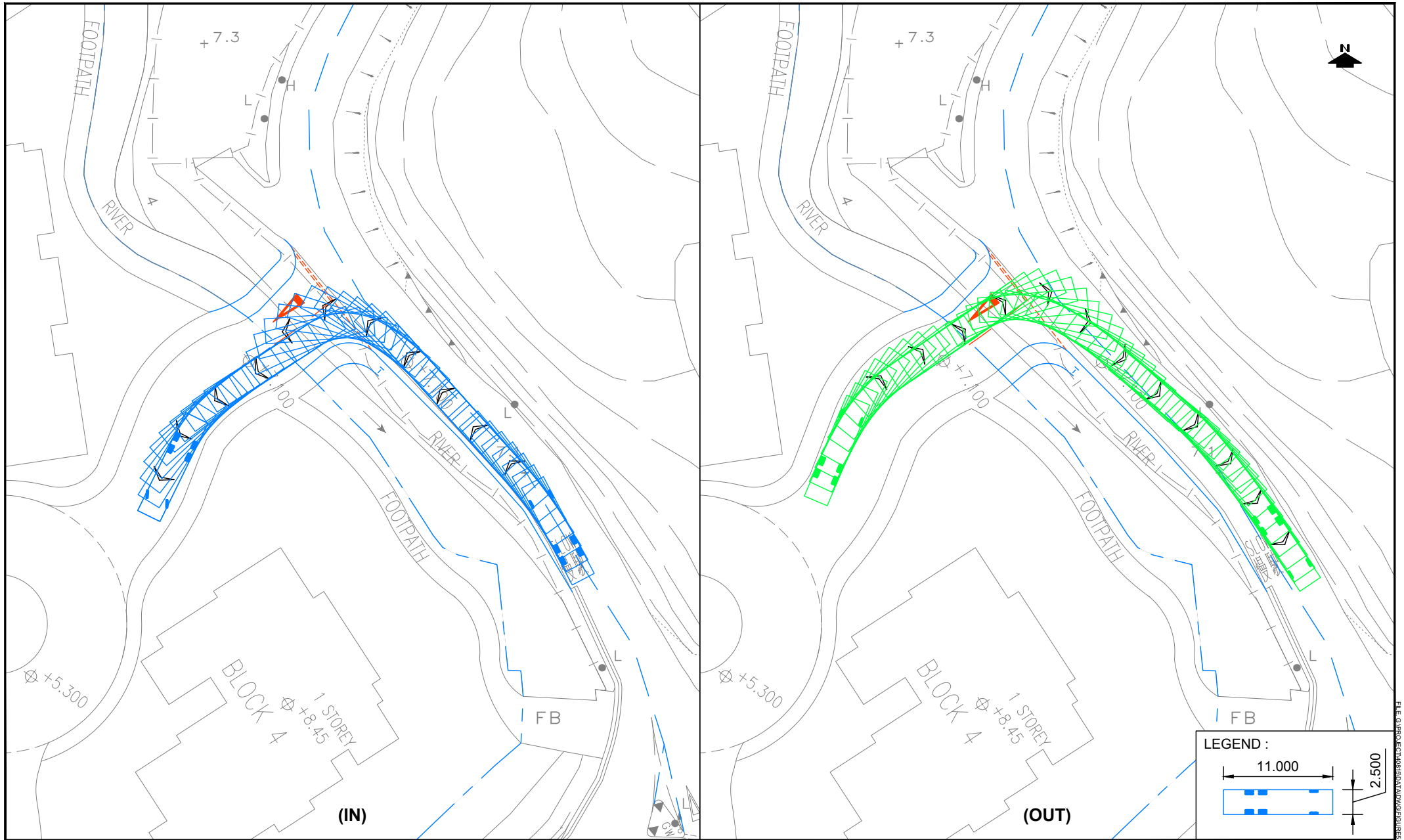


PROJECT NO.	40815	
DESIGNED	SLN	DATE AUG 2024
DRAWN	CLL	SCALE 1:500
CHECKED	SLN	

PROJECT TITLE: APPLICATION FOR AMENDMENT OF PLAN UNDER SECTION 12A OF THE TOWN PLANNING ORDINANCE (CAP. 131) TO REZONE THE APPLICATION SITE FROM "GREEN BELT" AND AREA SHOWN AS "ROAD" TO "RESIDENTIAL (GROUP C)S" FOR PROPOSED RESIDENTIAL DEVELOPMENT AT VARIOUS LOTS IN D.D. 210 AND ADJOINING GOVERNMENT LAND, PAK WAI, SAI KUNG

DRAWING TITLE	PROPOSED TRAFFIC ARRANGEMENT	
DRAWING NO.	FIGURE 5.1	
REV.	F	

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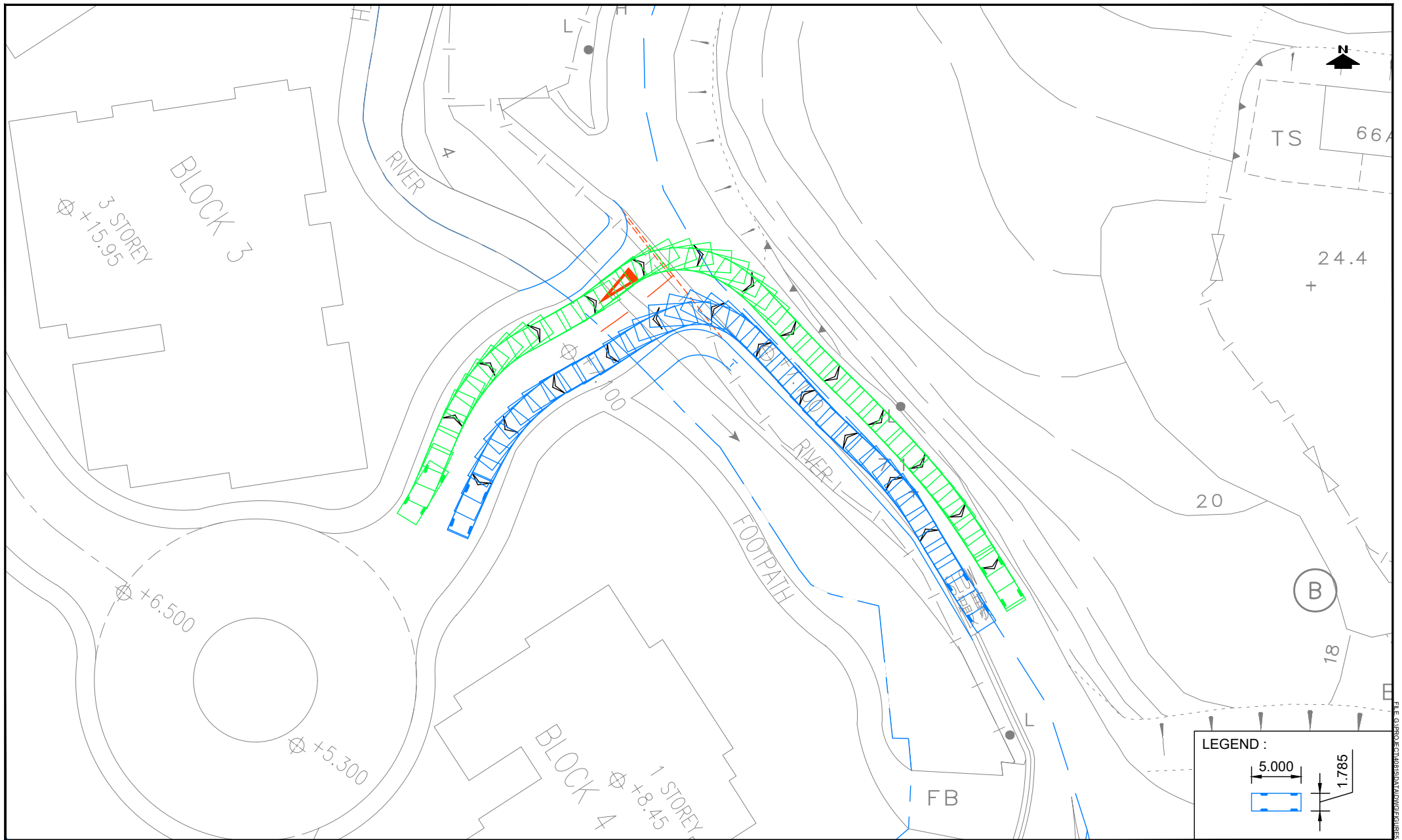
PROJECT NO.	40815	
DESIGNED	SLN	DATE JUL 2025
DRAWN	CLL	SCALE 1:500
CHECKED	SLN	

PROJECT TITLE: APPLICATION FOR AMENDMENT OF PLAN UNDER SECTION 12A OF THE TOWN PLANNING ORDINANCE (CAP. 131) TO REZONE THE APPLICATION SITE FROM "GREEN BELT" AND AREA SHOWN AS "ROAD" TO "RESIDENTIAL (GROUP C)5" FOR PROPOSED RESIDENTIAL DEVELOPMENT AT VARIOUS LOTS IN D.D. 210 AND ADJOINING GOVERNMENT LAND, PAK WAI, SAI KUNG

DRAWING NO.	FIGURE 5.2	REV.	C
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SWEPT PATH ANALYSIS - HGV

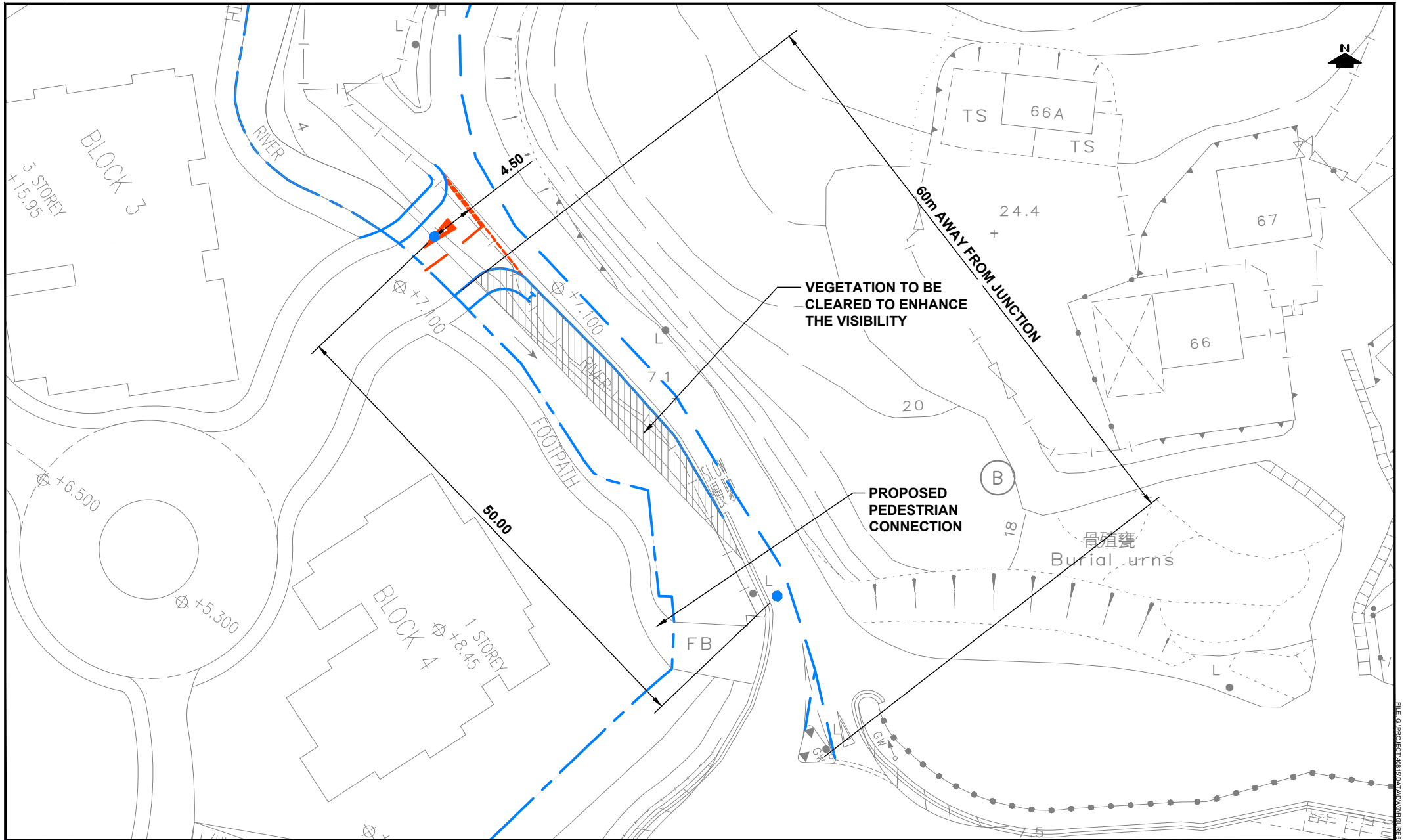
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PROJECT NO.	40815	
DESIGNED	SLN	DATE AUG 2025
DRAWN	CLL	SCALE
CHECKED	SLN	1:500

PROJECT TITLE	APPLICATION FOR AMENDMENT OF PLAN UNDER SECTION 12A OF THE TOWN PLANNING ORDINANCE (CAP. 131) TO REZONE THE APPLICATION SITE FROM "GREEN BELT" AND AREA SHOWN AS "ROAD" TO "RESIDENTIAL (GROUP C)5" FOR PROPOSED RESIDENTIAL DEVELOPMENT AT VARIOUS LOTS IN D.D. 210 AND ADJOINING GOVERNMENT LAND, PAK WAI, SAI KUNG	
DRAWING TITLE	SWEPT PATH ANALYSIS - PRIVATE CARS TRAVEL SIMULTANEOUSLY	

DRAWING NO.	FIGURE 5.3	REV.	D
LLA 顧問有限公司 Consultancy Limited			

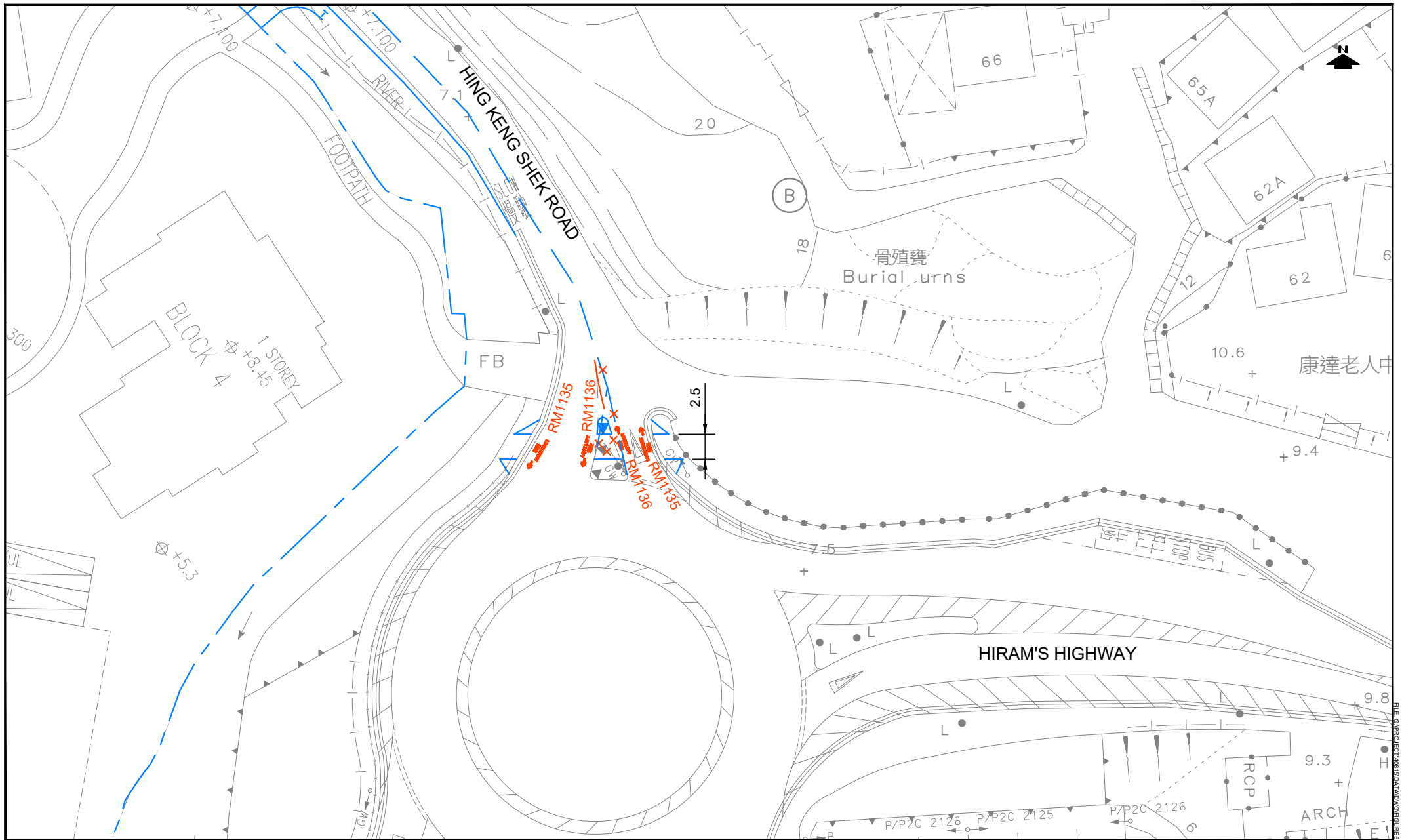


PROJECT NO.	40815	
DESIGNED	SLN	DATE AUG 2025
DRAWN	CLL	SCALE
CHECKED	SLN	1:500

PROJECT TITLE: APPLICATION FOR AMENDMENT OF PLAN UNDER SECTION 12A OF THE TOWN PLANNING ORDINANCE (CAP. 131) TO REZONE THE APPLICATION SITE FROM "GREEN BELT" AND AREA SHOWN AS "ROAD" TO "RESIDENTIAL (GROUP C)S" FOR PROPOSED RESIDENTIAL DEVELOPMENT AT VARIOUS LOTS IN D.D. 210 AND ADJOINING GOVERNMENT LAND, PAK WAI, SAI KUNG

DRAWING TITLE	SIGHTLINE ANALYSIS OF PROPOSED VEHICULAR ACCESS	
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DRAWING NO.	FIGURE 5.4	REV.	A
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PROJECT NO.	40815	
DESIGNED	SLN	DATE OCT 2025
DRAWN	CLL	SCALE 1:500
CHECKED	SLN	

PROJECT TITLE: APPLICATION FOR AMENDMENT OF PLAN UNDER SECTION 12A OF THE TOWN PLANNING ORDINANCE (CAP. 131) TO REZONE THE APPLICATION SITE FROM "GREEN BELT" AND AREA SHOWN AS "ROAD" TO "RESIDENTIAL (GROUP C)" FOR PROPOSED RESIDENTIAL DEVELOPMENT AT VARIOUS LOTS IN D.D. 210 AND ADJOINING GOVERNMENT LAND, PAK WAI, SAI KUNG

DRAWING TITLE: **PROPOSED CAUTIONARY CROSSING AT HING KENG SHEK ROAD**

DRAWING NO.	FIGURE 5.5	REV.	A
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Appendix A
Junction Capacity Assessment
– Existing Scenario

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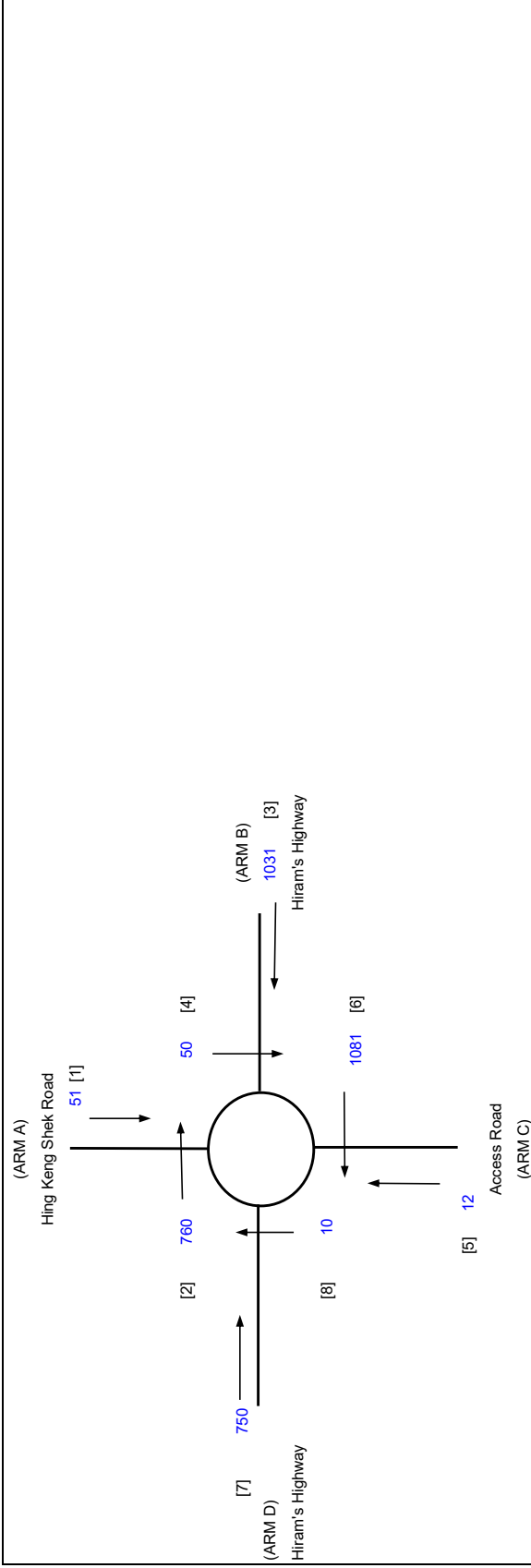
Application for Amendment of Plan under Section 12A of the Town Planning Ordinance (Cap. 131) to Rezone the Application Site from "Green Belt" and Area Shown as "Road" to "Residential (Group C)5" for Proposed Residential Development at Various Lots in D.D. 210 and Adjoining Government Land, Pak Wai, Sai Kung

J1 Hiram's Highway/Hing Keng Shek Road Roundabout

PRIORITY JUNCTION CALCULATION

PROJECT NO.: 40815
 FILENAME: J1_HH_HKSR.xlsx
 REFERENCE NO.:
 PREPARED BY:
 CHECKED BY:
 REVIEWED BY:

INITIALS DATE
 SKL Oct-25
 SLN Oct-25
 SLN Oct-25



ARM	A	B	C	D
V = Approach half width (m)	2.50	3.65	3.50	8.00
E = Entry width (m)	4.50	8.00	3.50	8.00
L = Effective length of flare (m)	9.00	50.00	1.00	1.00
R = Entry radius (m)	24.00	20.00	12.50	21.00
D = Inscribed circle diameter (m)	46.00	46.00	46.00	46.00
A = Entry angle (degree)	27.00	41.00	40.00	35.00
Q = Entry flow (pcuh)	51	1031	12	750
Qc = Circulating flow across entry (pcuh)	760	50	1081	10
OUTPUT PARAMETERS:				
S = Sharpness of flare = 1.6(E-V)/L	0.36	0.14	0.00	0.00
K = 1-0.00347(A-30)-0.978(1/R-0.05)	1.02	0.96	0.94	0.98
X2 = V + ((E-V)/(1+2S))	3.67	7.05	3.50	8.00
M = EXP((D-60)/10)	0.25	0.25	0.25	0.25
F = 303*X2	1112	2137	1061	2424
Td = 1+(0.5/(1+M))	1.40	1.40	1.40	1.40
Fc = 0.21*Td(1+0.2*X2)	0.51	0.71	0.50	0.76
Qe = K(F-Fc*Qc) *	737	2021	487	2380
Total In Sum = 1844 PCU				
DFC = Design flow/Capacity = Q/Qe	0.07	0.51	0.02	0.32
DFC of Critical Approach = 0.51				

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Application for Amendment of Plan under Section 12A of the Town Planning Ordinance (Cap. 131) to Rezone the Application Site from "Green Belt" and Area Shown as "Road" to "Residential (Group C)5" for Proposed Residential Development at Various Lots in D.D. 210 and Adjoining Government Land, Pak Wai, Sai Kung

J1 Hiram's Highway/Hing Keng Shek Road Roundabout

PRIORITY JUNCTION CALCULATION

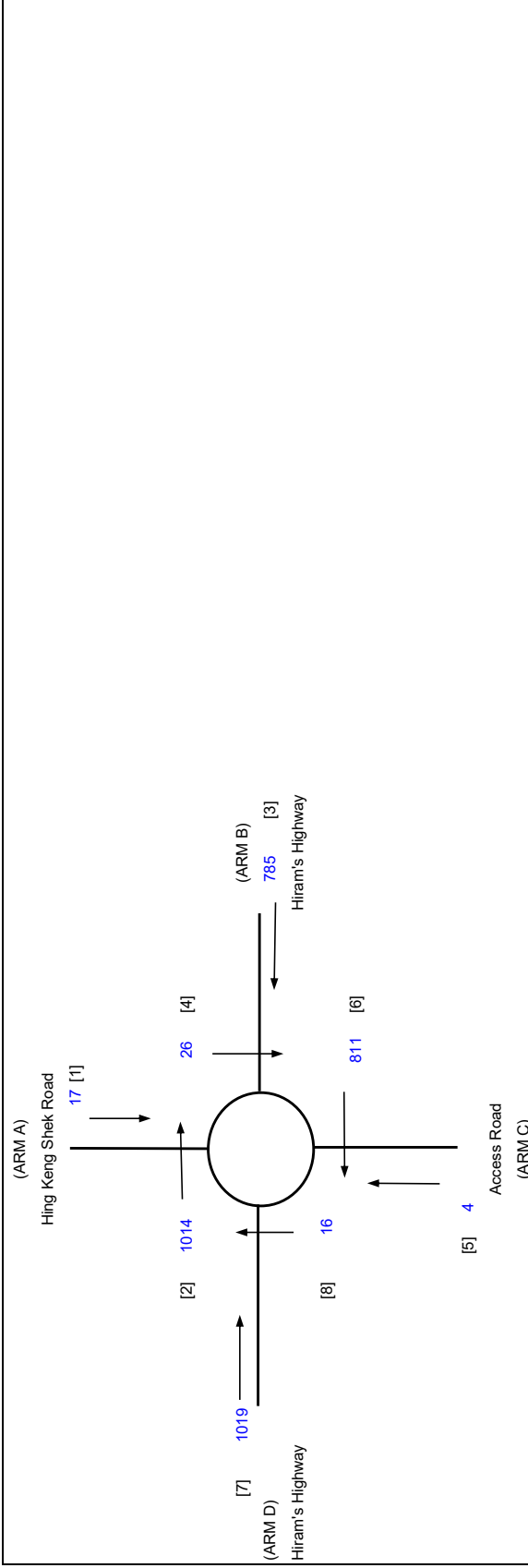
PROJECT NO.: 40815
 FILENAME : J1_HH_HKSR.xlsx
 REFERENCE NO.:

PREPARED BY: SKL
 CHECKED BY: SLN
 REVIEWED BY: SLN

INITIALS

DATE

2025 Existing PM



ARM

INPUT PARAMETERS:

	A	B	C	D
V = Approach half width (m)	2.50	3.65	3.50	8.00
E = Entry width (m)	4.50	8.00	3.50	8.00
L = Effective length of flare (m)	9.00	50.00	1.00	1.00
R = Entry radius (m)	24.00	20.00	12.50	21.00
D = Inscribed circle diameter (m)	46.00	46.00	46.00	46.00
A = Entry angle (degree)	27.00	41.00	40.00	35.00
Q = Entry flow (pcuh)	17	785	4	1019
Qc = Circulating flow across entry (pcuh)	1014	26	811	16

OUTPUT PARAMETERS:

S = Sharpness of flare = 1.6(E-V)/L	0.36	0.14	0.00	0.00
K = 1-0.00347(A-30)-0.978(1/R-0.05)	1.02	0.96	0.94	0.98
X2 = V + ((E-V)/(1+2S))	3.67	7.05	3.50	8.00
M = EXP((D-60)/10)	0.25	0.25	0.25	0.25
F = 303*X2	1112	2137	1061	2424
Td = 1+(0.5/(1+M))	1.40	1.40	1.40	1.40
Fc = 0.21*Td(1+0.2*X2)	0.51	0.71	0.50	0.76
Qe = K(F-Fc*Qc) *	605	2038	613	2376

DFC = Design flow/Capacity = Q/Qe

Total In Sum = 1825 PCU

DFC of Critical Approach = 0.43

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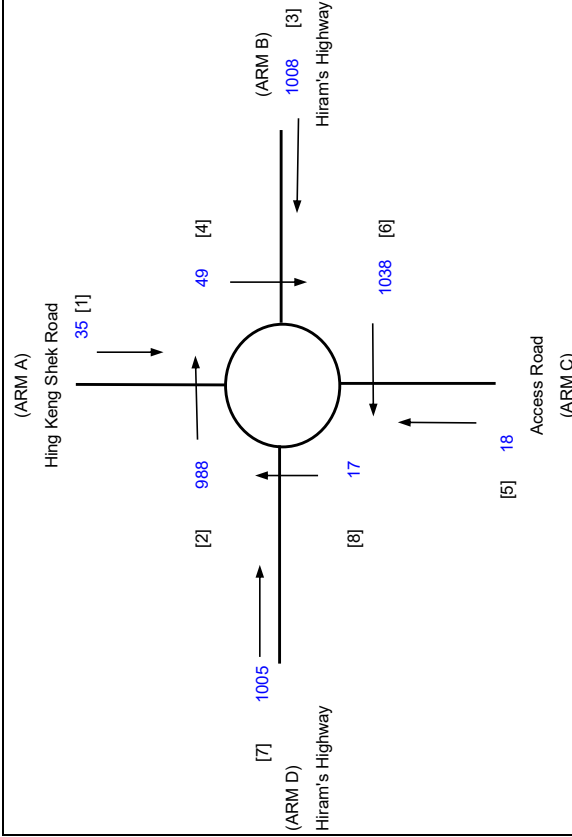
Application for Amendment of Plan under Section 12A of the Town Planning Ordinance (Cap. 131) to Rezone the Application Site from "Green Belt" and Area Shown as "Road" to "Residential (Group C)5" for Proposed Residential Development at Various Lots in D.D. 210 and Adjoining Government Land, Pak Wai, Sai Kung

J1 Hiram's Highway/Hing Keng Shek Road Roundabout

PRIORITY JUNCTION CALCULATION

PROJECT NO.: 40815
 FILENAME: J1_HH_HKSR.xlsx
 REFERENCE NO.:
 PREPARED BY:
 CHECKED BY:
 REVIEWED BY:

INITIALS DATE
 SKL Oct-25
 SLN Oct-25
 SLN Oct-25



ARM

INPUT PARAMETERS:

	A	B	C	D
V = Approach half width (m)	2.50	3.65	3.50	8.00
E = Entry width (m)	4.50	8.00	3.50	8.00
L = Effective length of flare (m)	9.00	50.00	1.00	1.00
R = Entry radius (m)	24.00	20.00	12.50	21.00
D = Inscribed circle diameter (m)	46.00	46.00	46.00	46.00
A = Entry angle (degree)	27.00	41.00	40.00	35.00
Q = Entry flow (pcuh)	35	1008	18	1005
Qc = Circulating flow across entry (pcuh)	988	49	1038	17

OUTPUT PARAMETERS:

S = Sharpness of flare = $1.6(E-V)/L$	0.36	0.14	0.00	0.00
K = $1-0.00347(A-30)-0.978(1/R-0.05)$	1.02	0.96	0.94	0.98
X2 = $V + ((E-V)/(1+2S))$	3.67	7.05	3.50	8.00
M = $EXP((D-60)/10)$	0.25	0.25	0.25	0.25
F = $303 \times X2$	1112	2137	1061	2424
Td = $1+(0.5/(1+M))$	1.40	1.40	1.40	1.40
Fc = $0.21 \times Td(1+0.2 \times X2)$	0.51	0.71	0.50	0.76
Qe = $K(F-Fc) \times Qc$	619	2022	507	2375

DFC = Design flow/Capacity = Q/Qe

Total In Sum =

2066 PCU

DFC of Critical Approach = 0.50

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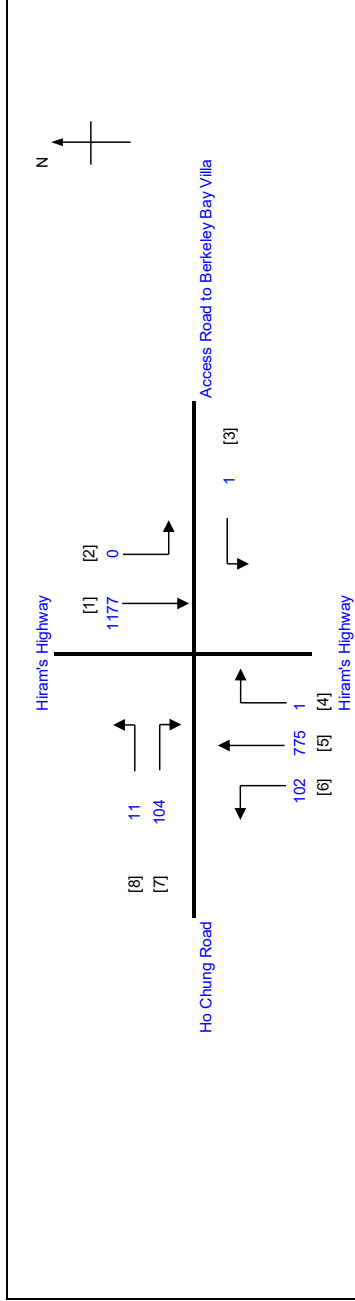
Application for Amendment of Plan under Section 12A of the Town Planning Ordinance (Cap. 131) to Rezone the Application Site from "Green Belt" and Area Shown as "Road to Residential (Group C)s" for Proposed Residential Development at Various Lots in D. 2 to Adjoining Government Land, Pak Wai, Sai Kung

TRAFFIC SIGNAL CALCULATION

PROJECT NO.: 40815
 FILENAME: J2_HH_HCR.xlsx

Prepared By:
 Checked By:
 Reviewed By:

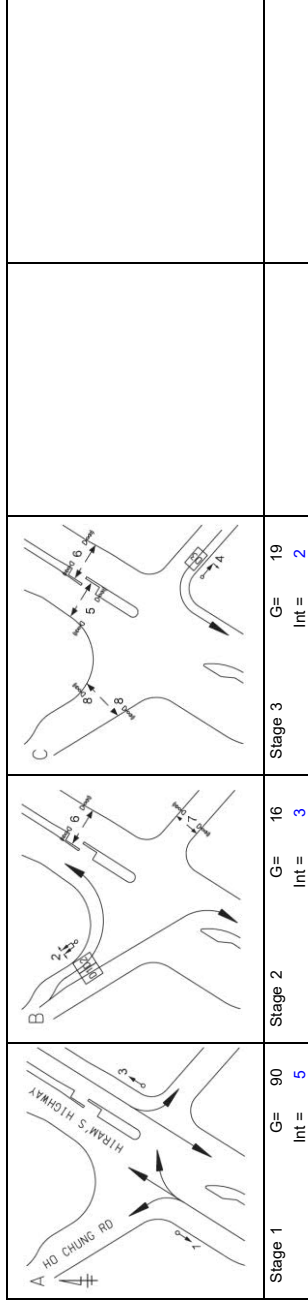
INITIALS DATE
 SKL Oct-25
 SLN Oct-25
 SLN Oct-25



No. of stages per cycle = 3

Cycle time = 135 sec
 Sum(y) = 0.344
 Loss time = 29 sec
 Total Flow = 2171 pcu
 Co = (1.5*L+5)/(1-Y) = 73.9 sec
 Crm = L/(1-Y) = 44.2 sec
 Yult = 0.683
 R.C.ult = (Yult-Y)*100% = 98.6 %
 Cp = 0.9*L/(0.9-Y) = 46.9 sec
 Ymax = 1-L/C = 0.785

R.C.(C) = (0.9*Ymax-Y)*100% = 106 %



Stage	Green Time Required SG	Green Time Provided SG	Delay	Green Time Provided FG
1	10	13	0	8
2	13	12	8	20
3	8	7	4	8
4	8	7	4	10

Move-ment	Stage	Lane Width m.	No. of lane	Radius m.	O	N	Straight Ahead Sat. Flow	Movement			Total Flow pcu/h	Proportion of Turning Vehicles	Sat. Flow pcu/h	Flare Lane m.	Flare Effect pcu/hr	Site Factor	Site Effect pcu/hr	Gradient %	Gradient Effect pcu/hr	Revised Sat. Flow pcu/h	y	Greater y	L sec	g (required) sec	g (input) sec	Degree of Saturation X	Queue Length (m /lane)	Average Delay (seconds)
								Left pcu/h	Straight pcu/h	Right pcu/h																		
1,2	1	3.20	1	15		N	1935	0	568	0.00	1935								1935	0.294	0.294	8	91	91	0.438	42	10	
1	1	3.20	1	12		N	2075	609	609	0.00	2075								2075	0.293	0.293		91	91	0.438	42	10	
5,6	1	3.50	1	12	O	N	1965	102	474	0.22	1914	18	119						2032	0.233	0.233		91	91	0.438	48	18	
4,5	1	3.50	1	12		N	1965	403	404	0.00	1734								1734	0.233	0.233		91	91	0.438	42	19	
7,8	2	3.50	1	12		N	2105	11	115	1.00	1871	12	424						2295	0.050	0.050	2	15	17	0.438	18	55	
3	3	3.30	1	15		N	1945	1	1	1.00	1768								1768	0.001	0.001	19	0	19	0.438	0	612	
PED	3																											

NOTE : O - OPPOSING TRAFFIC N - NEAR SIDE LANE SG - STEADY GREEN FG - FLASHING GREEN PEDESTRAIN WALKING SPEED = 1.2m/s QUEUING LENGTH = AVERAGE QUEUE * 6m

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Application for Amendment of Plan under Section 12A of the Town Planning Ordinance (Cap. 131) to Rezone the Application Site from "Green Belt" and Area Shown as "Road to Residential (Group C)s" for Proposed Residential Development at Various Lots in D. 2 to Adjoining Government Land, Pak Wai, Sai Kung

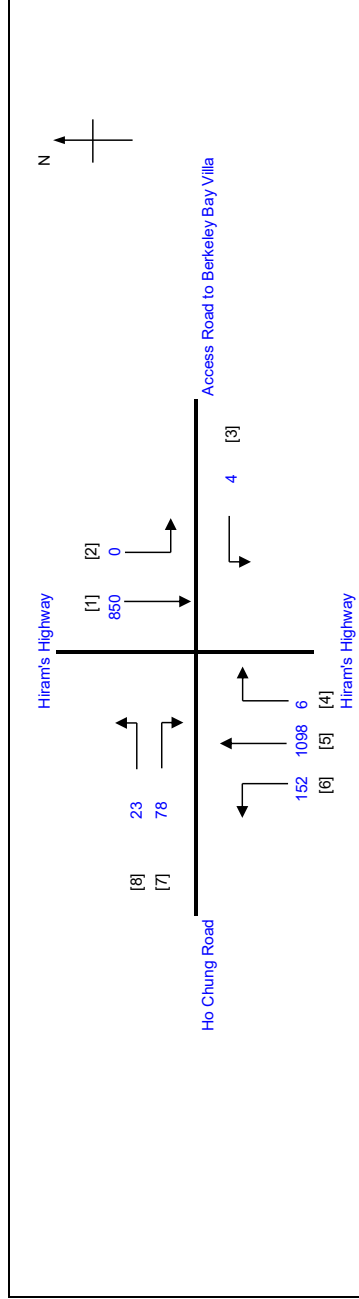
J2 Hiram's Highway/Ho Chung Road

TRAFFIC SIGNAL CALCULATION

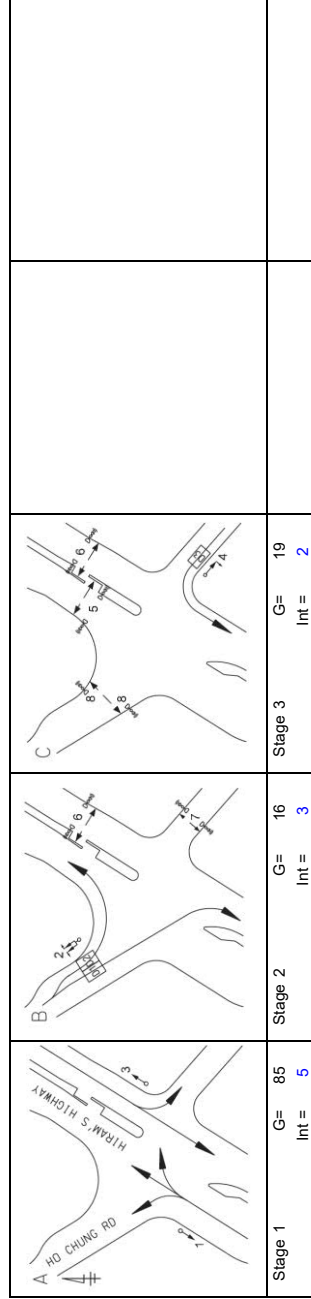
PROJECT NO.: 40815
 FILENAME: J2_HH_HCR.xlsx

Prepared By:
 Checked By:
 Reviewed By:

INITIALS DATE
 SKL Oct-25
 SLN Oct-25
 SLN Oct-25



No. of stages per cycle	N = 3
Cycle time	C = 130 sec
Sum(y)	Y = 0.377
Loss time	L = 33 sec
Total Flow	= 2211 pcu
Co	= 87.5 sec
Cm	= 53.0 sec
Yult	= 0.653
R.C.ult	= 72.9 %
Cp	= 56.8 sec
Ymax	= 0.746
R.C.(C)	= (0.9*Ymax-Y)*Y*100% = 78 %



Stage	Green Time Required SG	Green Time Provided SG	Delay	Green Time Provided FG
3	10	13	0	13
2,3	8	12	8	20
2	8	7	4	8
3	8	7	4	10

Move-ment	Stage	Lane Width m.	No. of lane	Radius m.	O	N	Straight Ahead Sat. Flow	Movement			Total Flow pcu/h	Proportion of Turning Vehicles	Sat. Flow pcu/h	Flare Lane m.	Flare Effect pcu/hr	Site Factor	Site Effect pcu/hr	Gradient %	Gradient Effect pcu/hr	Revised Sat. Flow pcu/h	y	Greater y	L sec	g (required) sec	g (input) sec	Degree of Saturation X	Queue Length (m /lane)	Average Delay (seconds)
								Left pcu/h	Straight pcu/h	Right pcu/h																		
1,2	1	3.20	1	15		N	1935	0	410	0.00	1935								1935	0.212		8	54	86	0.506	48	27	
1	1	3.20	1	12		N	2075	440	440	0.00	2075	18	126						2075	0.212			55	86	0.506	54	27	
5,6	1	3.50	1	12	O	N	1965	152	679	0.22	1912								2037	0.333	0.333		86	86	0.506	48	11	
4,5	1	3.50	1	12		N	1965	571	577	0.01	1733								1733	0.333			86	86	0.506	42	12	
7,8	2	3.50	1	12		N	2105	23	101	1.00	1871	12	424						2295	0.044	0.044	6	11	17	0.506	18	59	
3	3	3.30	1	15		N	1945	4	4	1.00	1768								1768	0.002		19	1	19	0.506	0	268	
PED	3																											

NOTE : O - OPPOSING TRAFFIC N - NEAR SIDE LANE SG - STEADY GREEN FG - FLASHING GREEN PEDESTRAIN WALKING SPEED = 1.2m/s QUEUING LENGTH = AVERAGE QUEUE * 6m

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Application for Amendment of Plan under Section 12A of the Town Planning Ordinance (Cap. 131) to Rezone the Application Site from "Green Belt" and Area Shown as "Road" to "Residential (Group C)s" for Proposed Residential Development at Various Lots in D. 2 to Adjoining Government Land, Pak Wai, Sai Kung

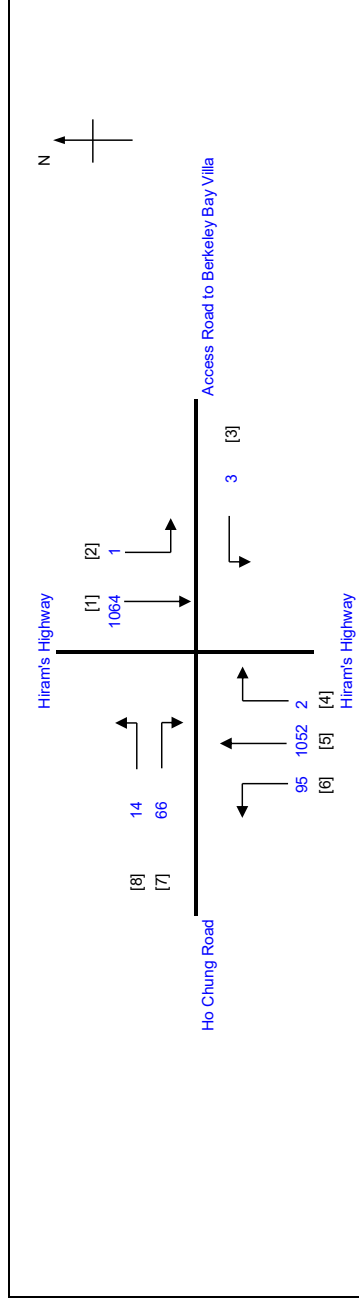
TRAFFIC SIGNAL CALCULATION

PROJECT NO.: 40815
 FILENAME: J2_HH_HCR.xlsx

Prepared By:
 Checked By:
 Reviewed By:

INITIALS
 SKL
 SLN
 SLN

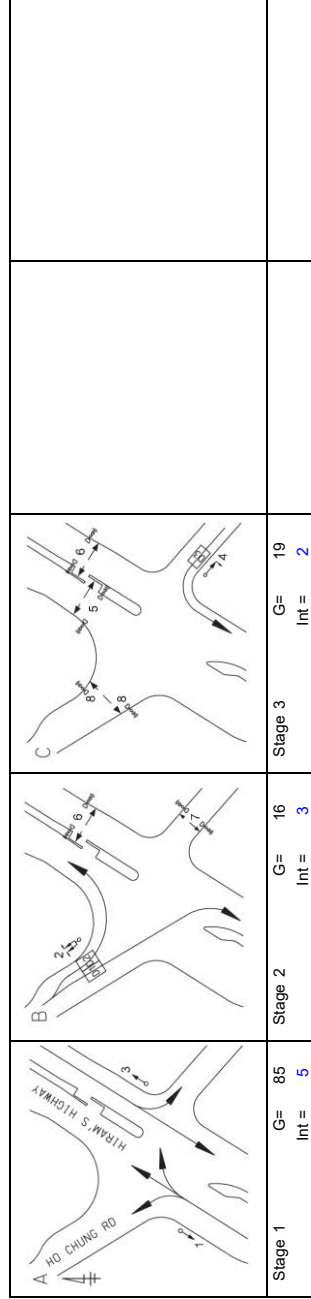
DATE
 Oct-25
 Oct-25
 Oct-25



No. of stages per cycle
 Cycle time
 Sum(y)
 Loss time
 Total Flow
 Co
 Crm
 Yult
 R.C.ult
 Cp
 Ymax

N = 3
 C = 130 sec
 Y = 0.338
 L = 34 sec
 = 2297 pcu
 = 84.6 sec
 = 51.4 sec
 = 0.645
 = 90.7 %
 = 54.5 sec
 = 0.738

R.C.(C) = $(0.9 \cdot Y_{max} \cdot Y) / Y \cdot 100\%$ = 97 %



Stage	Green Time Required SG	Green Time Provided SG	Delay	Green Time Provided FG
3	10	13	0	13
2,3	13	12	8	20
2	8	7	4	8
3	8	7	4	10

Movement	Stage	Lane Width m.	No. of lane	Radius m.	O	N	Straight Ahead Sat. Flow	Movement Left pcu/h	Movement Straight Right pcu/h	Total Flow pcu/h	Proportion of Turning Vehicles	Sat. Flow pcu/h	Flare Lane m.	Flare Effect pcu/hr	Site Factor	Site Effect pcu/hr	Gradient Effect pcu/hr	Revised Sat. Flow pcu/h	y	Greater y	L sec	g (required) sec	g (input) sec	Degree of Saturation X	Queue Length (m /lane)	Average Delay (seconds)	
1,2	1	3.20	1	15		N	1935	1	513	514	0.00	1935						1935	0.266		8	75	86	0.458	42	15	
1	1	3.20	1			N	2075	551	551	551	0.00	2075						2075	0.266			75	86	0.458	48	15	
5,6	1	3.50	1	12		N	1965	95	528	623	0.15	1928	18	126				2054	0.303	0.303		86	86	0.458	42	11	
4,5	1	3.50	1	12	O	N	1965	524	524	526	0.00	1734						1734	0.303			86	86	0.458	36	11	
7,8	2	3.50	1	12		N	2105	14	66	80	1.00	1871	12	424				2295	0.035	0.035	7	10	17	0.458	12	60	
3	3	3.30	1	15		N	1945	3	3	3	1.00	1768						1768	0.002		19	0	19	0.458	0	267	
PED	3																										

NOTE : O - OPPOSING TRAFFIC N - NEAR SIDE LANE SG - STEADY GREEN FG - FLASHING GREEN PEDESTRAIN WALKING SPEED = 1.2m/s QUEUING LENGTH = AVERAGE QUEUE * 6m

LLA CONSULTANCY LIMITED

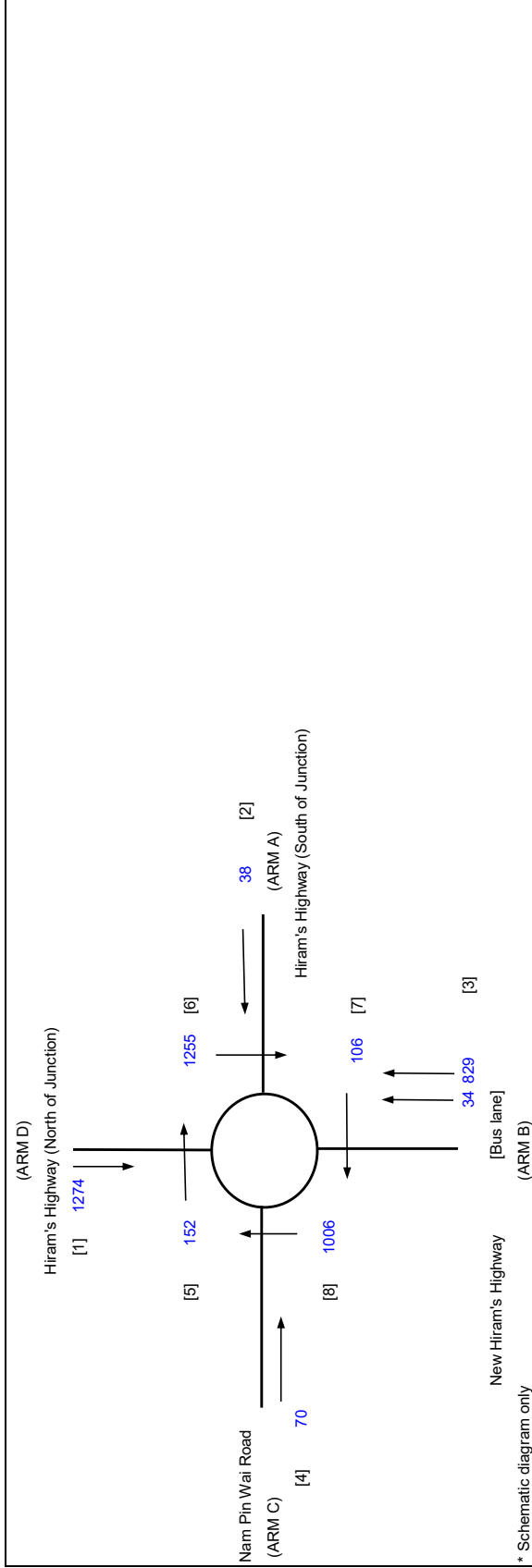
Application for Amendment of Plan under Section 12A of the Town Planning Ordinance (Cap. 131) to Rezone the Application Site from "Green Belt" and Area Shown as "Road" to "Residential (Group C)S" for Proposed Residential Development at Various Lots in D.D. 210 and Adjoining Government Land, Pak Wai, Sai Kung

J3 Hiram's Highway/ New Hiram's Highway/ Nam Pin Wai Road

ROUNDABOUT CALCULATION

PROJECT NO.: 40815
 FILENAME : J3_HH_NHH.xlsx
 REFERENCE NO.:
 PREPARED BY: SKL
 CHECKED BY: SLN
 REVIEWED BY: SLN

INITIALS DATE
 SKL Oct-25
 SLN Oct-25
 SLN Oct-25



ARM	A	B	C	D
V = Approach half width (m)	3.70	7.40	4.30	7.70
E = Entry width (m)	7.30	11.00	7.30	7.80
L = Effective length of flare (m)	11.00	1.00	20.00	1.00
R = Entry radius (m)	15.00	55.00	23.00	18.00
D = Inscribed circle diameter (m)	78.00	78.00	78.00	78.00
A = Entry angle (degree)	32.00	40.00	50.00	36.00
Q = Entry flow (pcu/h)	38	829	70	1274
Qc = Circulating flow across entry (pcu/h)	1255	106	1006	152

OUTPUT PARAMETERS:	A	B	C	D
S = Sharpness of flare = 1.6(E-V)/L	0.52	5.76	0.24	0.16
K = 1-0.00347(A-30)-0.978(1/R-0.05)	0.98	1.00	0.94	0.97
X2 = V + ((E-V)/(1+2S))	5.46	7.69	6.33	7.78
M = EXP((D-60)/10)	6.05	6.05	6.05	6.05
F = 303*X2	1654	2329	1917	2356
Td = 1+(0.5/(1+M))	1.07	1.07	1.07	1.07
Fc = 0.21*Td(1+0.2*X2)	0.47	0.57	0.51	0.57
Qe = K(F-Fc)*Qc	1039	2261	1316	2209

DFC = Design flow/Capacity = Q/Qe	0.04	0.37	0.05	0.58
Total In Sum =				2211 PCU
DFC of Critical Approach =				0.58

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Application for Amendment of Plan under Section 12A of the Town Planning Ordinance (Cap. 131) to Rezone the Application Site from "Green Belt" and Area Shown as "Road" to "Residential (Group C)S" for Proposed Residential Development at Various Lots in D.D. 210 and Adjoining Government Land, Pak Wai, Sai Kung

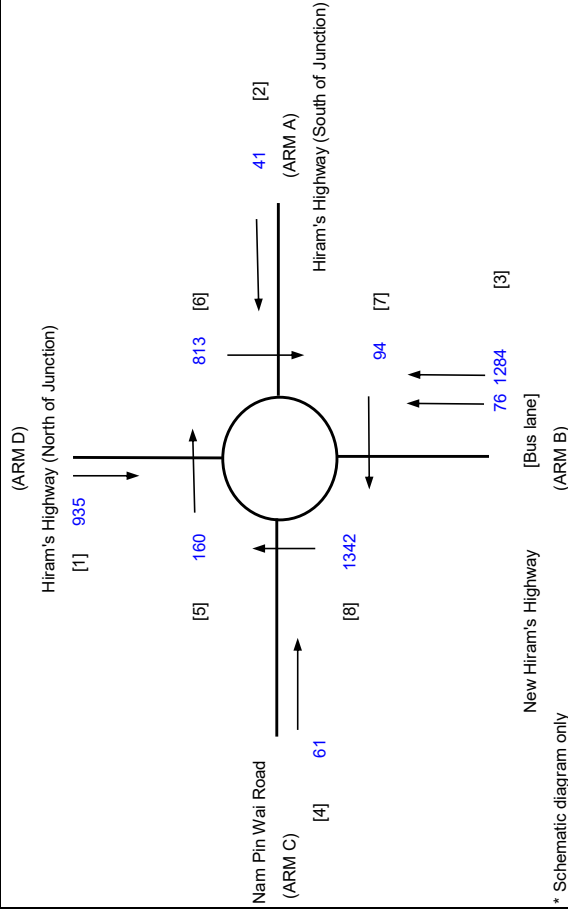
ROUNDABOUT CALCULATION

PROJECT NO.: 40815
 FILENAME : J3_HH_NHH.xlsx
 REFERENCE NO.:
 PREPARED BY: SKL
 CHECKED BY: SLN
 REVIEWED BY: SLN

INITIALS
 DATE
 SKL Oct-25
 SLN Oct-25
 SLN Oct-25

J3 Hiram's Highway/ New Hiram's Highway/ Nam Pin Wai Road

2025 Existing PM



ARM

INPUT PARAMETERS:

	A	B	C	D
V = Approach half width (m)	3.70	7.40	4.30	7.70
E = Entry width (m)	7.30	11.00	7.30	7.80
L = Effective length of flare (m)	11.00	1.00	20.00	1.00
R = Entry radius (m)	15.00	55.00	23.00	18.00
D = Inscribed circle diameter (m)	78.00	78.00	78.00	78.00
A = Entry angle (degree)	32.00	40.00	50.00	36.00
Q = Entry flow (pcuh)	41	1284	61	935
Qc = Circulating flow across entry (pcuh)	813	94	1342	160

OUTPUT PARAMETERS:

S = Sharpness of flare = 1.6(E-V)/L	0.52	5.76	0.24	0.16
K = 1-0.00347(A-30)-0.978(1/R-0.05)	0.98	1.00	0.94	0.97
X2 = V + ((E-V)/(1+2S))	5.46	7.69	6.33	7.78
M = EXP((D-60)/10)	6.05	6.05	6.05	6.05
F = 303*X2	1654	2329	1917	2356
Td = 1+(0.5/(1+M))	1.07	1.07	1.07	1.07
Fc = 0.21*Td(1+0.2*X2)	0.47	0.57	0.51	0.57
Qe = K(F-Fc)*Qc	1242	2268	1156	2205

DFC = Design flow/Capacity = Q/Qe

Total In Sum = 2321 PCU

DFC of Critical Approach = 0.57

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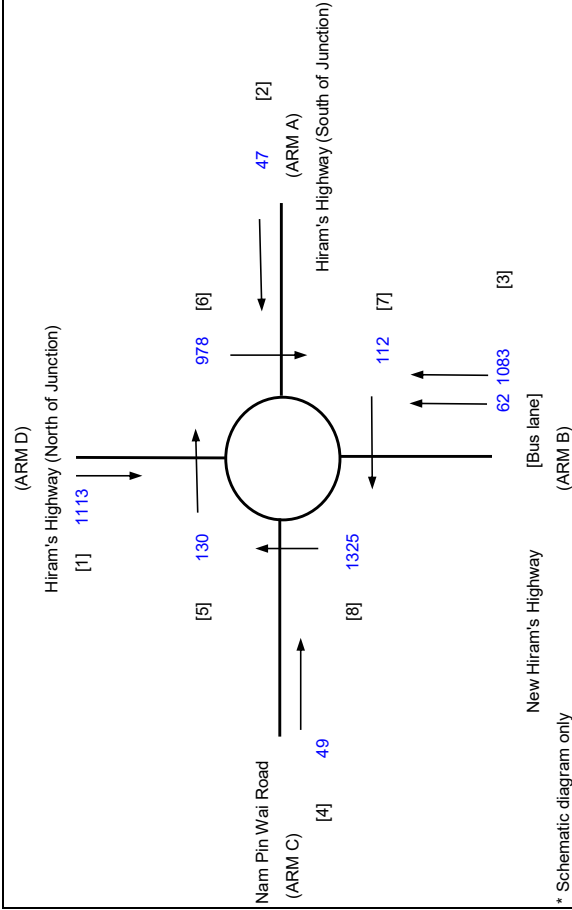
Application for Amendment of Plan under Section 12A of the Town Planning Ordinance (Cap. 131) to Rezone the Application Site from "Green Belt" and Area Shown as "Road" to "Residential (Group C)" for Proposed Residential Development at Various Lots in D.D. 210 and Adjoining Government Land, Pak Wai, Sai Kung

J3 Hiram's Highway/ New Hiram's Highway/ Nam Pin Wai Road

ROUNDABOUT CALCULATION

PROJECT NO.: 40815
 FILENAME : J3_HH_NHH.xlsx
 REFERENCE NO.:
 PREPARED BY: SKL
 CHECKED BY: SLN
 REVIEWED BY: SLN

INITIALS
 DATE
 SKL Oct-25
 SLN Oct-25
 SLN Oct-25



ARM	A	B	C	D
V = Approach half width (m)	3.70	7.40	4.30	7.70
E = Entry width (m)	7.30	11.00	7.30	7.80
L = Effective length of flare (m)	11.00	1.00	20.00	1.00
R = Entry radius (m)	15.00	55.00	23.00	18.00
D = Inscribed circle diameter (m)	78.00	78.00	78.00	78.00
A = Entry angle (degree)	32.00	40.00	50.00	36.00
Q = Entry flow (pcuh)	47	1083	49	1113
Qc = Circulating flow across entry (pcuh)	978	112	1325	130

S = Sharpness of flare = 1.6(E-V)/L	0.52	5.76	0.24	0.16
K = 1-0.00347(A-30)-0.978(1/R-0.05)	0.98	1.00	0.94	0.97
X2 = V + ((E-V)/(1+2S))	5.46	7.69	6.33	7.78
M = EXP((D-60)/10)	6.05	6.05	6.05	6.05
F = 303*X2	1654	2329	1917	2356
Td = 1+(0.5/(1+M))	1.07	1.07	1.07	1.07
Fc = 0.21*Td(1+0.2*X2)	0.47	0.57	0.51	0.57
Qe = K(F-Fc)*Qc	1166	2257	1164	2221

DFC = Design flow/Capacity = Q/Qe	0.04	0.48	0.04	0.50
Total In Sum =			2292	PCU
DFC of Critical Approach =				0.50

Appendix B
Junction Capacity Assessment
– Reference & Design Scenarios

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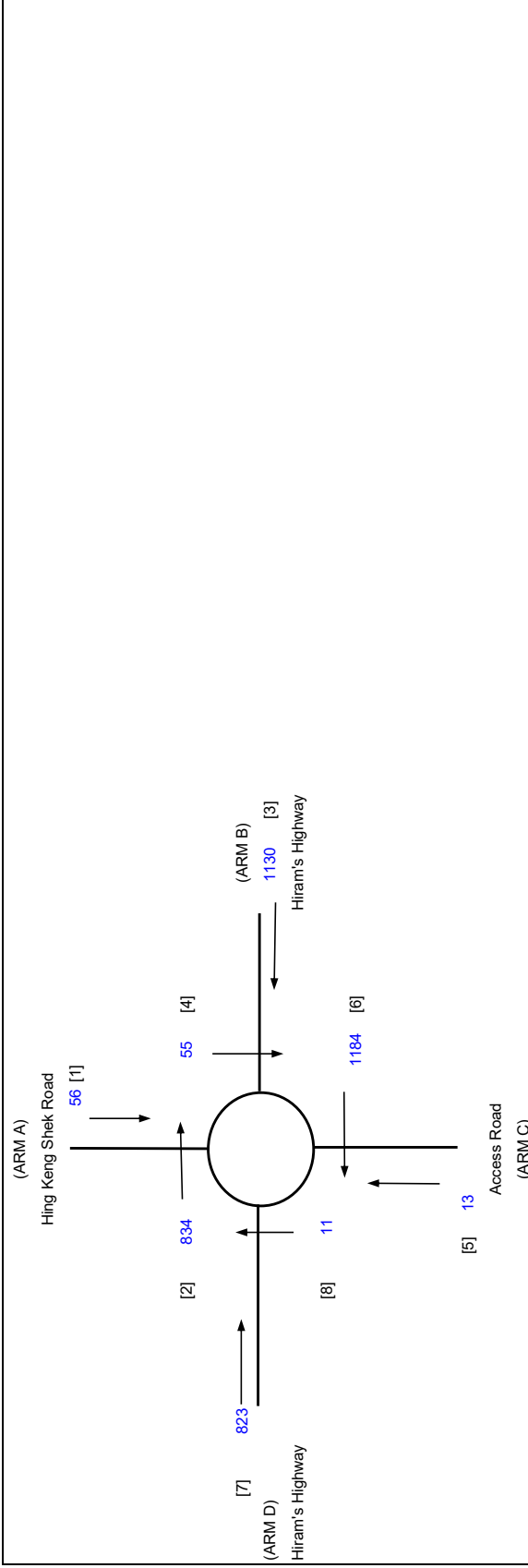
Application for Amendment of Plan under Section 12A of the Town Planning Ordinance (Cap. 131) to Rezone the Application Site from "Green Belt" and Area Shown as "Road" to "Residential (Group C)5" for Proposed Residential Development at Various Lots in D.D. 210 and Adjoining Government Land, Pak Wai, Sai Kung

J1 Hiram's Highway/Hing Keng Shek Road Roundabout

PRIORITY JUNCTION CALCULATION

PROJECT NO.: 40815
 FILENAME : J1_HH_HKSR.xlsx
 REFERENCE NO.:
 PREPARED BY:
 CHECKED BY:
 REVIEWED BY:

INITIALS DATE
 SKL Dec-25
 SLN Dec-25
 SLN Dec-25



ARM	A	B	C	D
V = Approach half width (m)	2.50	3.65	3.50	8.00
E = Entry width (m)	4.50	8.00	3.50	8.00
L = Effective length of flare (m)	9.00	50.00	1.00	1.00
R = Entry radius (m)	24.00	20.00	12.50	21.00
D = Inscribed circle diameter (m)	46.00	46.00	46.00	46.00
A = Entry angle (degree)	27.00	41.00	40.00	35.00
Q = Entry flow (pcuh)	56	1130	13	823
Qc = Circulating flow across entry (pcuh)	834	55	1184	11
OUTPUT PARAMETERS:				
S = Sharpness of flare = 1.6(E-V)/L	0.36	0.14	0.00	0.00
K = 1-0.00347(A-30)-0.978(1/R-0.05)	1.02	0.96	0.94	0.98
X2 = V + ((E-V)/(1+2S))	3.67	7.05	3.50	8.00
M = EXP((D-60)/10)	0.25	0.25	0.25	0.25
F = 303*X2	1112	2137	1061	2424
Td = 1+(0.5/(1+M))	1.40	1.40	1.40	1.40
Fc = 0.21*Td(1+0.2*X2)	0.51	0.71	0.50	0.76
Qe = K(F-Fc)*Qc *	699	2018	438	2379
Total In Sum = 2022 PCU				
DFC = Design flow/Capacity = Q/Qe	0.08	0.56	0.03	0.35
DFC of Critical Approach = 0.56				

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Application for Amendment of Plan under Section 12A of the Town Planning Ordinance (Cap. 131) to Rezone the Application Site from "Green Belt" and Area Shown as "Road" to "Residential (Group C)5" for Proposed Residential Development at Various Lots in D.D. 210 and Adjoining Government Land, Pak Wai, Sai Kung

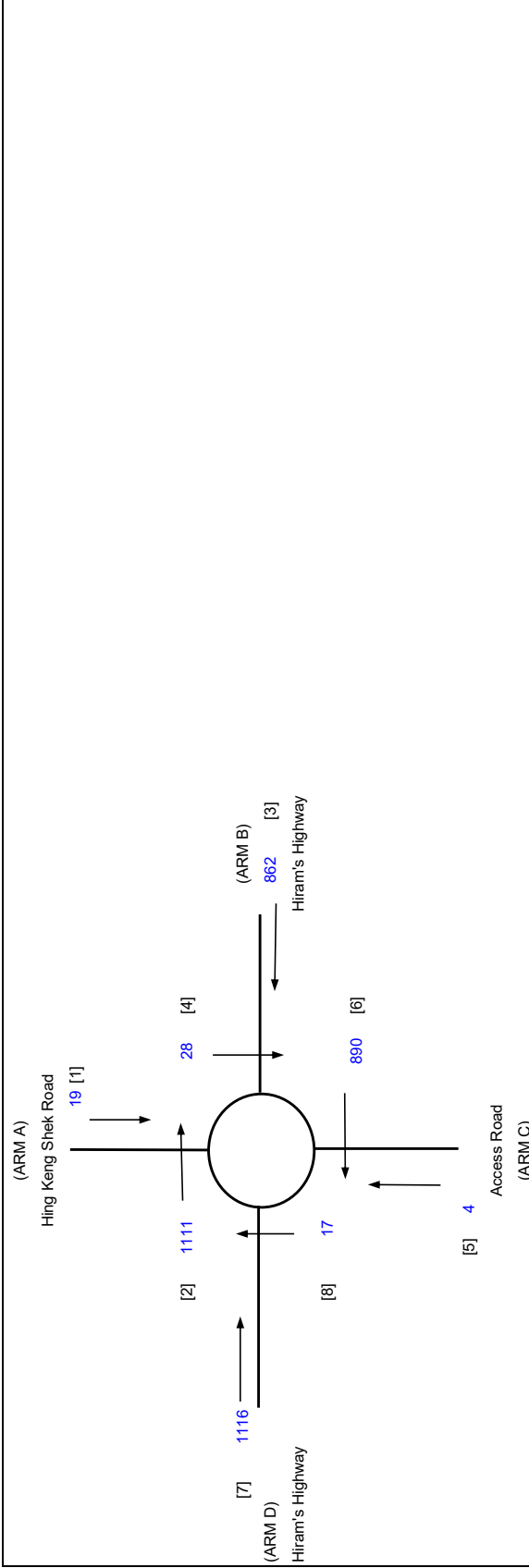
J1 Hiram's Highway/Hing Keng Shek Road Roundabout

PRIORITY JUNCTION CALCULATION

PROJECT NO.: 40815
 FILENAME : J1_HH_HKSR.xlsx
 REFERENCE NO.:
 PREPARED BY:
 CHECKED BY:
 REVIEWED BY:

INITIALS DATE
 SKL Dec-25
 SLN Dec-25
 SLN Dec-25

2034 Reference PM



ARM	A	B	C	D
V = Approach half width (m)	2.50	3.65	3.50	8.00
E = Entry width (m)	4.50	8.00	3.50	8.00
L = Effective length of flare (m)	9.00	50.00	1.00	1.00
R = Entry radius (m)	24.00	20.00	12.50	21.00
D = Inscribed circle diameter (m)	46.00	46.00	46.00	46.00
A = Entry angle (degree)	27.00	41.00	40.00	35.00
Q = Entry flow (pcuh)	19	862	4	1116
Qc = Circulating flow across entry (pcuh)	1111	28	890	17
OUTPUT PARAMETERS:				
S = Sharpness of flare = 1.6(E-V)/L	0.36	0.14	0.00	0.00
K = 1-0.00347(A-30)-0.978(1/R-0.05)	1.02	0.96	0.94	0.98
X2 = V + ((E-V)/(1+2S))	3.67	7.05	3.50	8.00
M = EXP((D-60)/10)	0.25	0.25	0.25	0.25
F = 303*X2	1112	2137	1061	2424
Td = 1+(0.5/(1+M))	1.40	1.40	1.40	1.40
Fc = 0.21*Td*(1+0.2*X2)	0.51	0.71	0.50	0.76
Qe = K(F-Fc*Qc) *	555	2036	576	2375
DFC = Design flow/Capacity = Q/Qe	0.03	0.42	0.01	0.47
Total In Sum = 2001 PCU				
DFC of Critical Approach = 0.47				

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Application for Amendment of Plan under Section 12A of the Town Planning Ordinance (Cap. 131) to Rezone the Application Site from "Green Belt" and Area Shown as "Road" to "Residential (Group C)5" for Proposed Residential Development at Various Lots in D.D. 210 and Adjoining Government Land, Pak Wai, Sai Kung

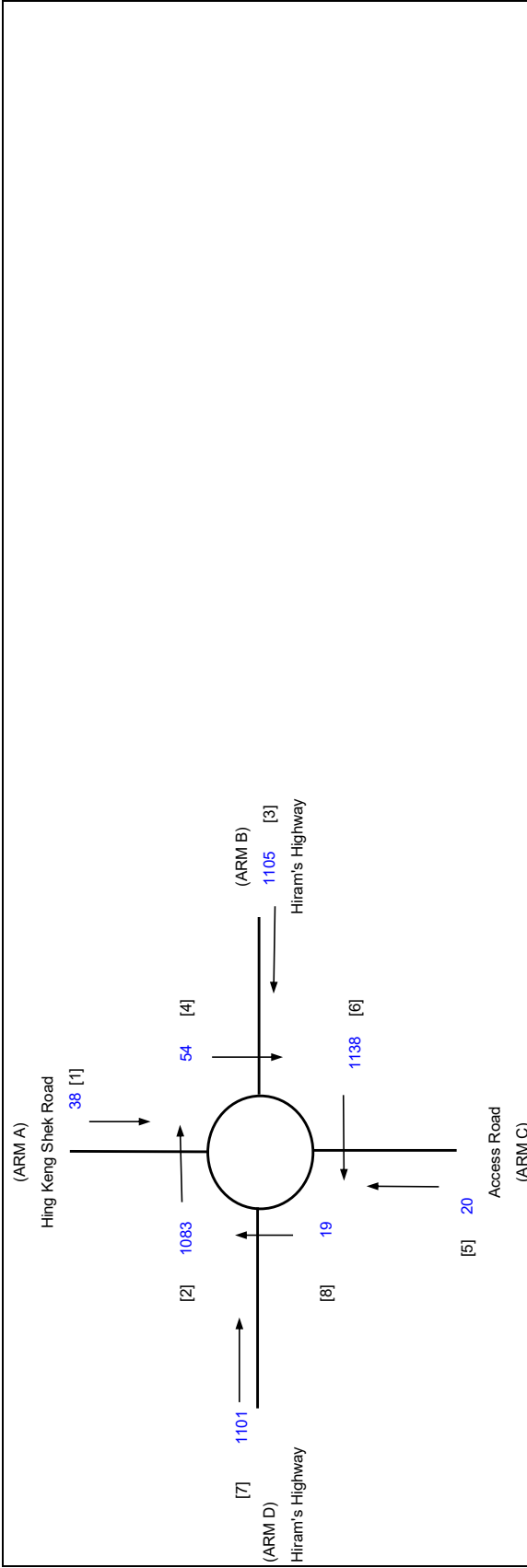
J1 Hiram's Highway/Hing Keng Shek Road Roundabout

PRIORITY JUNCTION CALCULATION

PROJECT NO.: 40815
 FILENAME : J1_HH_HKSR.xlsx
 REFERENCE NO.:

PREPARED BY: SKL
 CHECKED BY: SLN
 REVIEWED BY: SLN

INITIALS
 DATE
 Dec-25
 Dec-25
 Dec-25



ARM	A	B	C	D
V = Approach half width (m)	2.50	3.65	3.50	8.00
E = Entry width (m)	4.50	8.00	3.50	8.00
L = Effective length of flare (m)	9.00	50.00	1.00	1.00
R = Entry radius (m)	24.00	20.00	12.50	21.00
D = Inscribed circle diameter (m)	46.00	46.00	46.00	46.00
A = Entry angle (degree)	27.00	41.00	40.00	35.00
Q = Entry flow (pcuh)	38	1105	20	1101
Qc = Circulating flow across entry (pcuh)	1083	54	1138	19
OUTPUT PARAMETERS:				
S = Sharpness of flare = 1.6(E-V)/L	0.36	0.14	0.00	0.00
K = 1-0.00347(A-30)-0.978(1/R-0.05)	1.02	0.96	0.94	0.98
X2 = V + ((E-V)/(1+2S))	3.67	7.05	3.50	8.00
M = EXP((D-60)/10)	0.25	0.25	0.25	0.25
F = 303*X2	1112	2137	1061	2424
Td = 1+(0.5/(1+M))	1.40	1.40	1.40	1.40
Fc = 0.21*Td(1+0.2*X2)	0.51	0.71	0.50	0.76
Qe = K(F-Fc*Qc) *	570	2019	460	2373
Total In Sum = 2264 PCU				
DFC = Design flow/Capacity = Q/Qe	0.07	0.55	0.04	0.46
DFC of Critical Approach = 0.55				

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Application for Amendment of Plan under Section 12A of the Town Planning Ordinance (Cap. 131) to Rezone the Application Site from "Green Belt" and Area Shown as "Road" to "Residential (Group C)5" for Proposed Residential Development at Various Lots in D.D. 210 and Adjoining Government Land, Pak Wai, Sai Kung

J1 Hiram's Highway/Hing Keng Shek Road Roundabout

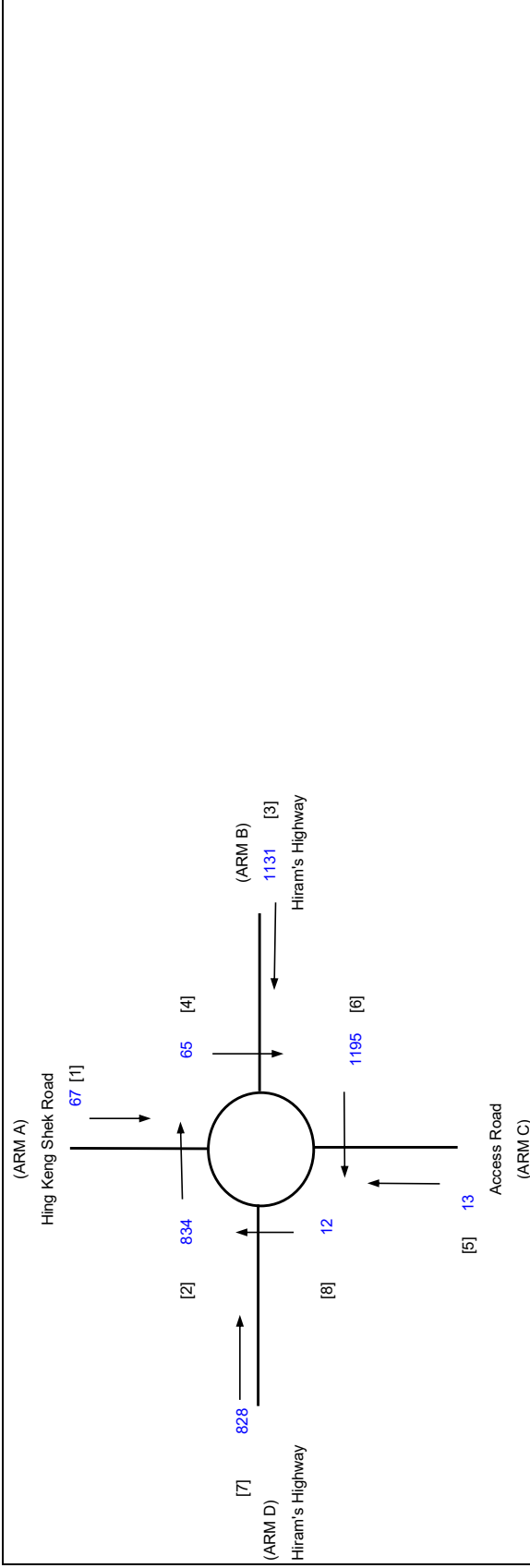
PRIORITY JUNCTION CALCULATION

PROJECT NO.: 40815
 FILENAME : J1_HH_HKSR.xlsx
 REFERENCE NO.:

2034 Design AM

PREPARED BY:
 CHECKED BY:
 REVIEWED BY:

INITIALS DATE
 SKL Dec-25
 SLN Dec-25
 SLN Dec-25



ARM	A	B	C	D
V = Approach half width (m)	2.50	3.65	3.50	8.00
E = Entry width (m)	4.50	8.00	3.50	8.00
L = Effective length of flare (m)	9.00	50.00	1.00	1.00
R = Entry radius (m)	24.00	20.00	12.50	21.00
D = Inscribed circle diameter (m)	46.00	46.00	46.00	46.00
A = Entry angle (degree)	27.00	41.00	40.00	35.00
Q = Entry flow (pcuh)	67	1131	13	828
Qc = Circulating flow across entry (pcuh)	834	65	1195	12
OUTPUT PARAMETERS:				
S = Sharpness of flare = 1.6(E-V)/L	0.36	0.14	0.00	0.00
K = 1-0.00347(A-30)-0.978(1/R-0.05)	1.02	0.96	0.94	0.98
X2 = V + ((E-V)/(1+2S))	3.67	7.05	3.50	8.00
M = EXP((D-60)/10)	0.25	0.25	0.25	0.25
F = 303*X2	1112	2137	1061	2424
Td = 1+(0.5/(1+M))	1.40	1.40	1.40	1.40
Fc = 0.21*Td(1+0.2*X2)	0.51	0.71	0.50	0.76
Qe = K(F-Fc*Qc) *	699	2011	433	2379
Total In Sum = 2039 PCU				
DFC = Design flow/Capacity = Q/Qe	0.10	0.56	0.03	0.35
DFC of Critical Approach = 0.56				

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Application for Amendment of Plan under Section 12A of the Town Planning Ordinance (Cap. 131) to Rezone the Application Site from "Green Belt" and Area Shown as "Road" to "Residential (Group C)5" for Proposed Residential Development at Various Lots in D.D. 210 and Adjoining Government Land, Pak Wai, Sai Kung

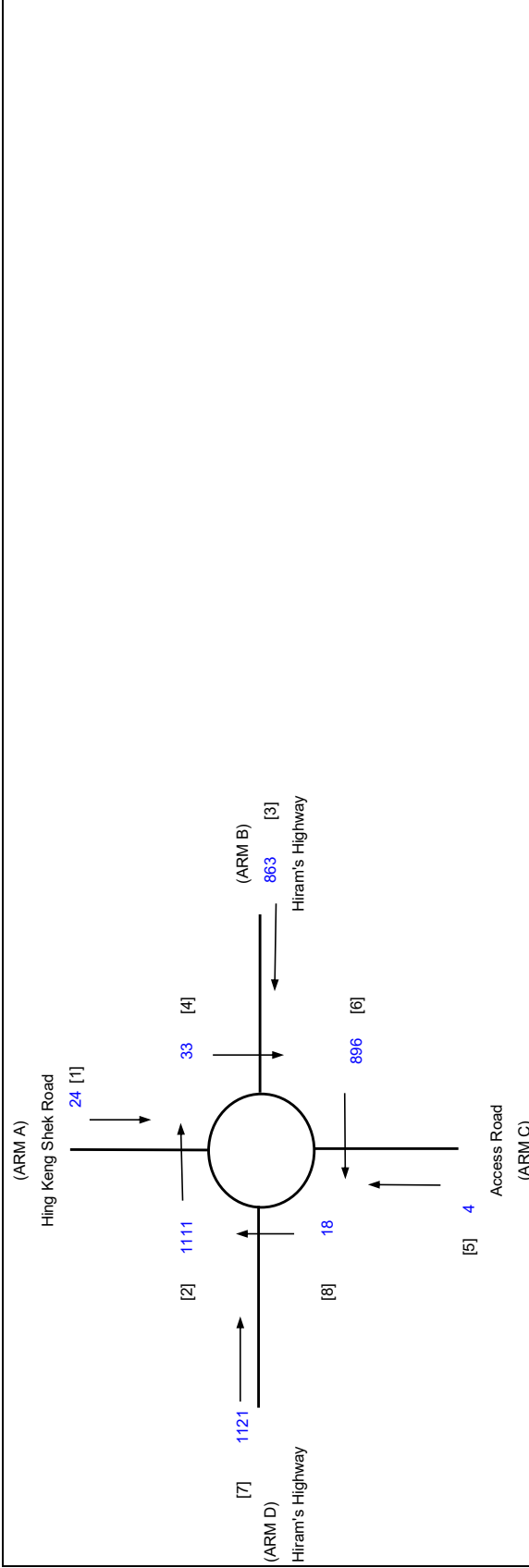
J1 Hiram's Highway/Hing Keng Shek Road Roundabout

PRIORITY JUNCTION CALCULATION

PROJECT NO.: 40815
 FILENAME : J1_HH_HKSR.xlsx
 REFERENCE NO.:
 PREPARED BY:
 CHECKED BY:
 REVIEWED BY:

INITIALS DATE
 SKL Dec-25
 SLN Dec-25
 SLN Dec-25

2034 Design PM



ARM	A	B	C	D
V = Approach half width (m)	2.50	3.65	3.50	8.00
E = Entry width (m)	4.50	8.00	3.50	8.00
L = Effective length of flare (m)	9.00	50.00	1.00	1.00
R = Entry radius (m)	24.00	20.00	12.50	21.00
D = Inscribed circle diameter (m)	46.00	46.00	46.00	46.00
A = Entry angle (degree)	27.00	41.00	40.00	35.00
Q = Entry flow (pcu/h)	24	863	4	1121
Qc = Circulating flow across entry (pcu/h)	1111	33	896	18
OUTPUT PARAMETERS:				
S = Sharpness of flare = 1.6(E-V)/L	0.36	0.14	0.00	0.00
K = 1-0.00347(A-30)-0.978(1/R-0.05)	1.02	0.96	0.94	0.98
X2 = V + ((E-V)/(1+2S))	3.67	7.05	3.50	8.00
M = EXP((D-60)/10)	0.25	0.25	0.25	0.25
F = 303*X2	1112	2137	1061	2424
Td = 1+(0.5/(1+M))	1.40	1.40	1.40	1.40
Fc = 0.21*Td*(1+0.2*X2)	0.51	0.71	0.50	0.76
Qe = K(F-Fc*Qc) *	555	2033	573	2374
DFC = Design flow/Capacity = Q/Qe	0.04	0.42	0.01	0.47
Total In Sum =				2012 PCU
DFC of Critical Approach =				0.47

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Application for Amendment of Plan under Section 12A of the Town Planning Ordinance (Cap. 131) to Rezone the Application Site from "Green Belt" and Area Shown as "Road" to "Residential (Group C)5" for Proposed Residential Development at Various Lots in D.D. 210 and Adjoining Government Land, Pak Wai, Sai Kung

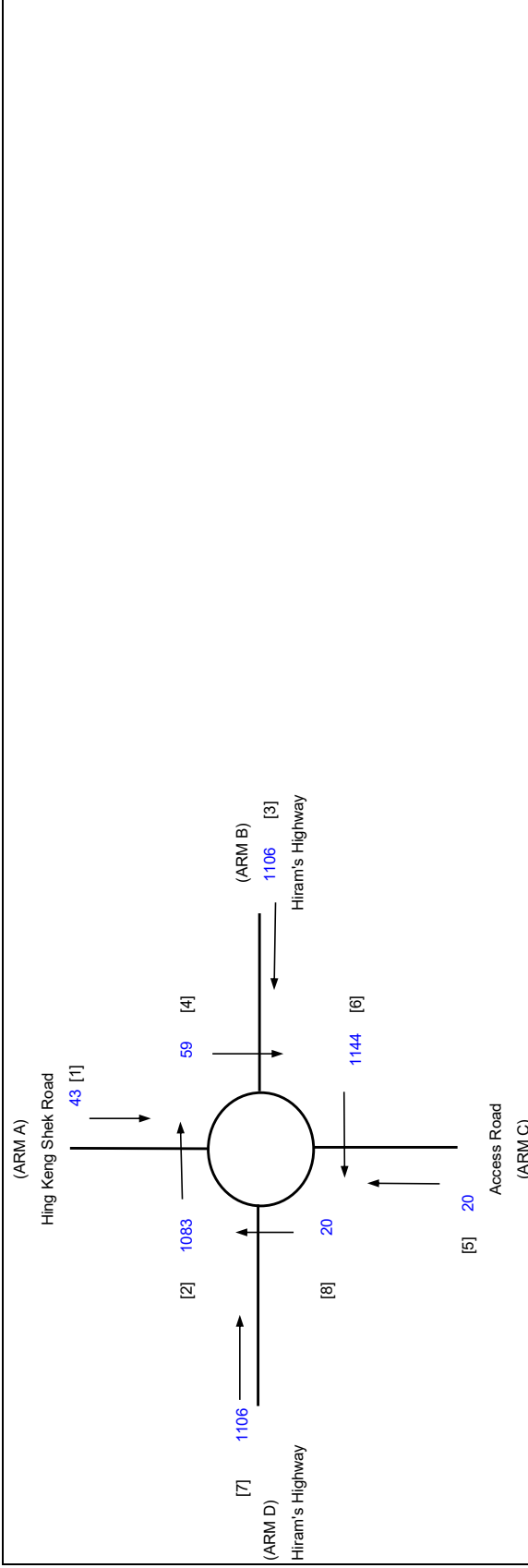
J1 Hiram's Highway/Hing Keng Shek Road Roundabout

PRIORITY JUNCTION CALCULATION

PROJECT NO.: 40815
 FILENAME: J1_HH_HKSR.xlsx
 REFERENCE NO.:
 PREPARED BY:
 CHECKED BY:
 REVIEWED BY:

INITIALS DATE
 SKL Dec-25
 SLN Dec-25
 SLN Dec-25

2034 Design WN



ARM

INPUT PARAMETERS:

	A	B	C	D
V = Approach half width (m)	2.50	3.65	3.50	8.00
E = Entry width (m)	4.50	8.00	3.50	8.00
L = Effective length of flare (m)	9.00	50.00	1.00	1.00
R = Entry radius (m)	24.00	20.00	12.50	21.00
D = Inscribed circle diameter (m)	46.00	46.00	46.00	46.00
A = Entry angle (degree)	27.00	41.00	40.00	35.00
Q = Entry flow (pcuh)	43	1106	20	1106
Qc = Circulating flow across entry (pcuh)	1083	59	1144	20

OUTPUT PARAMETERS:

S = Sharpness of flare = $1.6(E-V)/L$	0.36	0.14	0.00	0.00
K = $1-0.00347(A-30)-0.978(1/R-0.05)$	1.02	0.96	0.94	0.98
X2 = $V + ((E-V)/(1+2S))$	3.67	7.05	3.50	8.00
M = $EXP((D-60)/10)$	0.25	0.25	0.25	0.25
F = $303 \times X2$	1112	2137	1061	2424
Td = $1+(0.5/(1+M))$	1.40	1.40	1.40	1.40
Fc = $0.21 \times Td(1+0.2 \times X2)$	0.51	0.71	0.50	0.76
Qe = $K(F-Fc) \times Qc$	570	2015	457	2373
DFC = Design flow/Capacity = Q/Qe	0.08	0.55	0.04	0.47

Total In Sum =

2275 PCU

DFC of Critical Approach = 0.55

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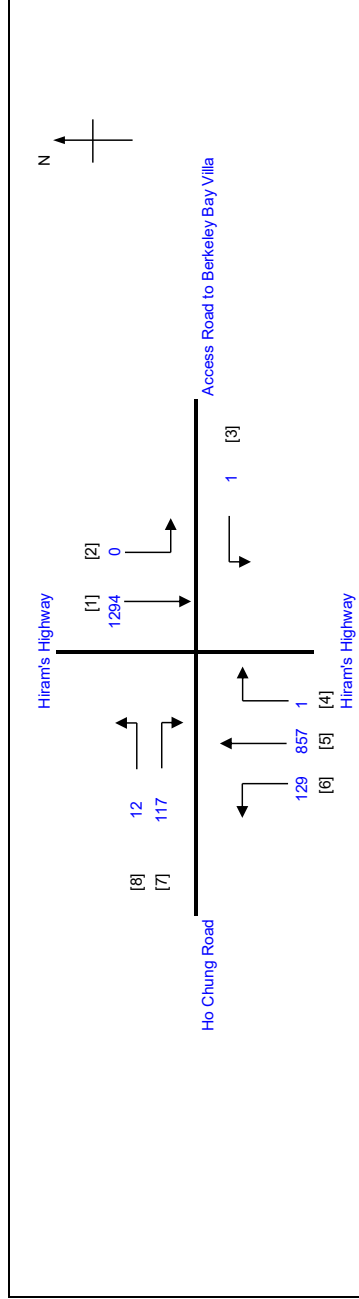
Application for Amendment of Plan under Section 12A of the Town Planning Ordinance (Cap. 131) to Rezone the Application Site from "Green Belt" and Area Shown as "Road to Residential (Group C)s" for Proposed Residential Development at Various Lots in D. 2 to Adjoining Government Land, Pak Wai, Sai Kung

TRAFFIC SIGNAL CALCULATION

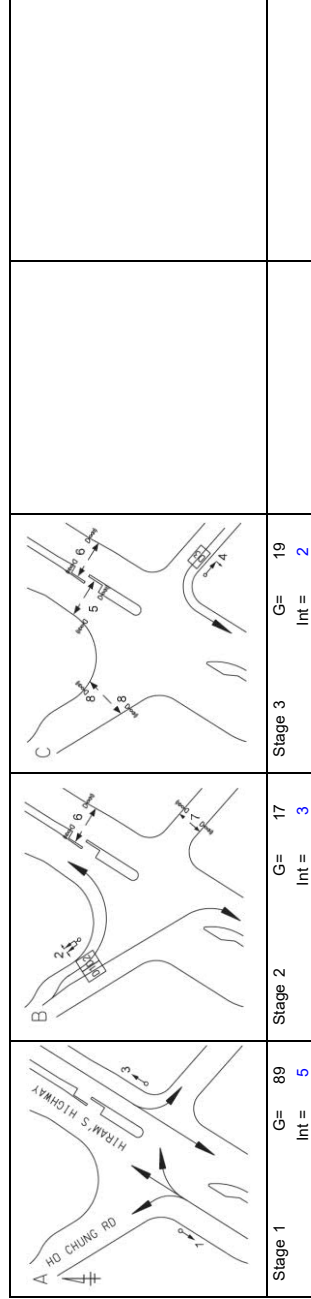
PROJECT NO.: 40815
 FILENAME: J2_HH_HCR.xlsx

Prepared By:
 Checked By:
 Reviewed By:

INITIALS DATE
 SKL Dec-25
 SLN Dec-25
 SLN Dec-25



No. of stages per cycle	N =	3
Cycle time	C =	135 sec
Sum(y)	Y =	0.380
Loss time	L =	29 sec
Total Flow	=	2411 pcu
Co	= (1.5*L+5)/(1-Y)	78.2 sec
Cm	= L/(1-Y)	46.8 sec
Yult	=	0.683
R.C.ult	= (Yult-Y)*100%	79.7 %
Cp	= 0.9*L/(0.9-Y)	50.2 sec
Ymax	= 1-L/C	0.785
R.C.(C)	= (0.9*Ymax-Y)*100%	86 %



Pedestrian Phase	Stage	Green Time Required SG	Delay FG	Green Time Provided SG	FG
P1	3	10	8	13	8
P2	2,3	13	12	21	12
P3	2	8	7	9	7
P4	3	8	7	10	7

Move-ment	Stage	Lane Width m.	No. of lane	Radius m.	O	N	Straight Ahead Sat. Flow	Movement			Total Flow pcu/h	Proportion of Turning Vehicles	Sat. Flow pcu/h	Flare Lane m.	Flare Effect pcu/hr	Site Factor	Site Effect pcu/hr	Gradient %	Gradient Effect pcu/hr	Revised Sat. Flow pcu/h	y	Greater y	L sec	g (required) sec	g (input) sec	Degree of Saturation X	Queue Length (m /lane)	Average Delay (seconds)
								Left pcu/h	Straight pcu/h	Right pcu/h																		
1,2	1	3.20	1	15		N	1935	0	625	0.00	1935								1935	0.323	0.323	8	90	90	0.484	42	11	
1	1	3.20	1			N	2075	669	669	0.00	2075								2075	0.322	0.322		90	90	0.484	48	11	
5,6	1	3.50	1	12		N	1965	129	532	0.24	1907	18	120						2027	0.262	0.262		73	90	0.484	54	19	
4,5	1	3.50	1	12	O	N	1965	454	455	0.00	1735								1735	0.262	0.262		73	90	0.484	42	19	
7,8	2	3.50	1	12		N	2105	12	129	1.00	1871	12	400						2271	0.057	0.057	2	16	18	0.484	24	56	
3	3	3.30	1	15		N	1945	1	1	1.00	1768								1768	0.001	0.001	19	0	19	0.484	0	795	
PED	3																											

NOTE : O - OPPOSING TRAFFIC N - NEAR SIDE LANE SG - STEADY GREEN FG - FLASHING GREEN PEDESTRAIN WALKING SPEED = 1.2m/s QUEUING LENGTH = AVERAGE QUEUE * 6m

LLA CONSULTANCY LIMITED

Application for Amendment of Plan under Section 12A of the Town Planning Ordinance (Cap. 131) to Rezone the Application Site from "Green Belt" and Area Shown as "Road to Residential (Group C)s" for Proposed Residential Development at Various Lots in D. 2 to Adjoining Government Land, Pak Wai, Sai Kung

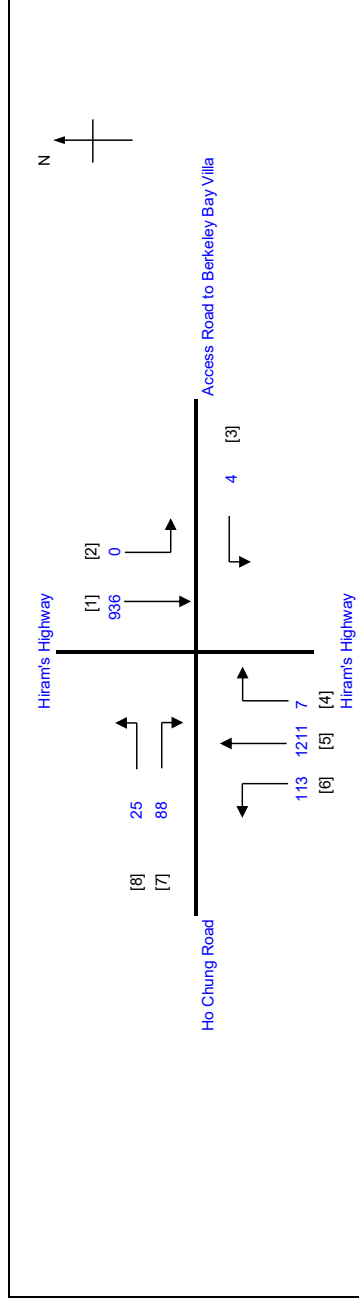
TRAFFIC SIGNAL CALCULATION

PROJECT NO.: 40815
 FILENAME: J2_HH_HCR.xlsx

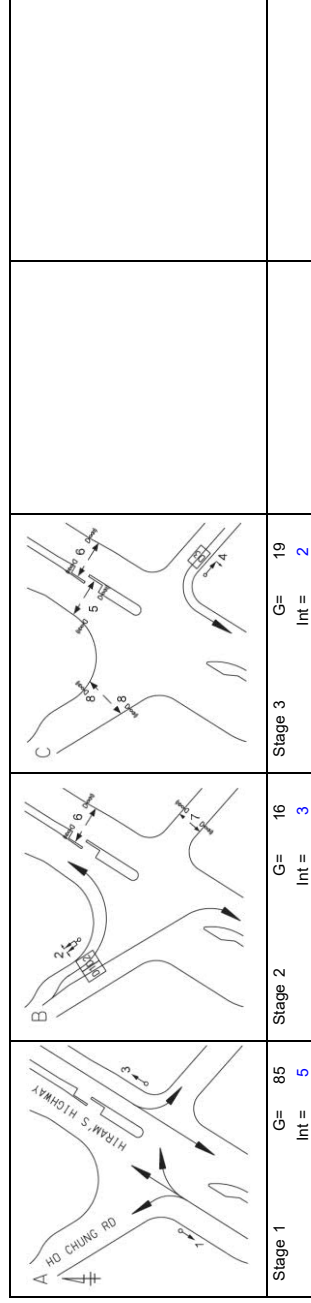
Prepared By:
 Checked By:
 Reviewed By:

2034 Reference PM

INITIALS DATE
 SKL Dec-25
 SLN Dec-25
 SLN Dec-25



No. of stages per cycle N = 3
 Cycle time C = 130 sec
 Sum(y) = 0.401
 Loss time L = 32 sec
 Total Flow = 2384 pcu
 Co = 88.5 sec
 Crm = 53.4 sec
 Yult = 0.660
 R.C.ult = (Yult-Y)*100% = 64.6 %
 Cp = 0.9*L/(0.9-Y)
 Ymax = 1-L/C = 0.754
R.C.(C) = (0.9*Ymax-Y)*100% = 69 %



Pedestrian Phase	Stage	Green Time Required SG	Delay	Green Time Provided SG	FG
P1	3	10	0	13	8
P2	2,3	13	8	20	12
P3	2	8	7	8	7
P4	3	8	7	10	7

Move-ment	Stage	Lane Width m.	No. of lane	Radius m.	O	N	Straight Ahead Sat. Flow	Movement			Total Flow pcu/h	Proportion of Turning Vehicles	Sat. Flow pcu/h	Flare Lane m.	Flare Effect pcu/hr	Site Factor	Site Effect pcu/hr	Gradient %	Gradient Effect pcu/hr	Revised Sat. Flow pcu/h	y	Greater y	L sec	g (required) sec	g (input) sec	Degree of Saturation X	Queue Length (m /lane)	Average Delay (seconds)
								Left pcu/h	Straight pcu/h	Right pcu/h																		
1,2	1	3.20	1	15		N	1935	0	452	0.00	1935								1935	0.234		8	57	86	0.532	54	26	
1	1	3.20	1			N	2075	484	484	0.00	2075								2075	0.233			57	86	0.532	54	26	
5,6	1	3.50	1	12		N	1965	113	722	0.16	1927	18	126						2053	0.352	0.352		86	86	0.532	48	12	
4,5	1	3.50	1	12	O	N	1965	602	609	0.01	1733								1733	0.352	0.352		86	86	0.532	42	12	
7,8	2	3.50	1	12		N	2105	25	113	1.00	1871	12	424						2295	0.049	0.049	5	12	17	0.532	18	59	
3	3	3.30	1	15		N	1945	4	4	1.00	1768								1768	0.002	0.002	19	19	0.532	0	303		
PED	3																											

NOTE : O - OPPOSING TRAFFIC N - NEAR SIDE LANE SG - STEADY GREEN FG - FLASHING GREEN PEDESTRAIN WALKING SPEED = 1.2m/s QUEUING LENGTH = AVERAGE QUEUE * 6m

LLA CONSULTANCY LIMITED

Application for Amendment of Plan under Section 12A of the Town Planning Ordinance (Cap. 131) to Rezone the Application Site from "Green Belt" and Area Shown as "Road to Residential (Group C)s" for Proposed Residential Development at Various Lots in D. 2 to Adjoining Government Land, Pak Wai, Sai Kung

J2 Hiram's Highway/Ho Chung Road

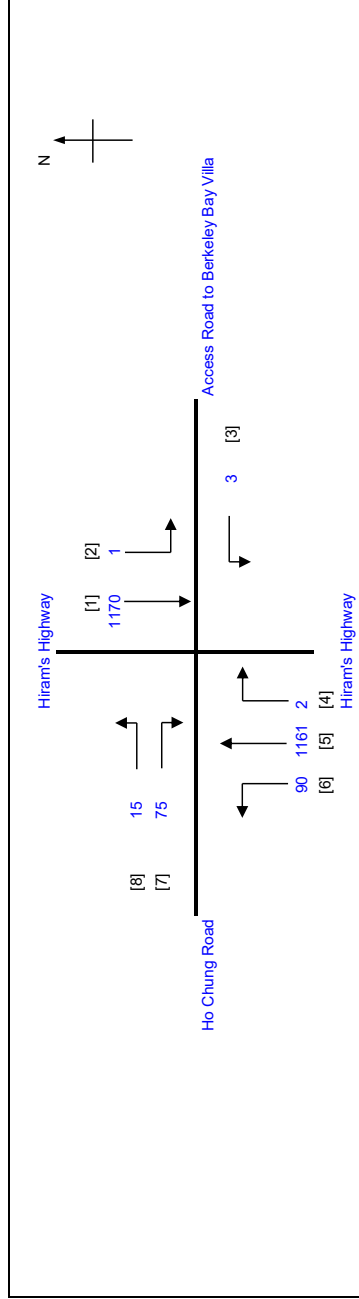
TRAFFIC SIGNAL CALCULATION

PROJECT NO.: 40815
 FILENAME: J2_HH_HCR.xlsx

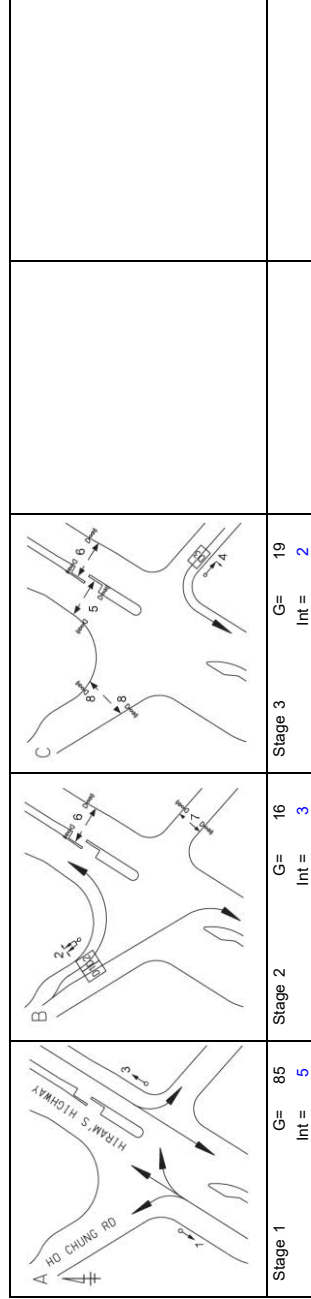
Prepared By:
 Checked By:
 Reviewed By:

INITIALS
 SKL
 SLN
 SLN

DATE
 Dec-25
 Dec-25
 Dec-25



No. of stages per cycle	N =	3
Cycle time	C =	130 sec
Sum(y)	Y =	0.370
Loss time	L =	34 sec
Total Flow	=	2517 pcu
Co	= (1.5*L+5)/(1-Y)	88.8 sec
Cm	= L/(1-Y)	53.9 sec
Yult	=	0.645
R.C.ult	= (Yult-Y)*100%	74.5 %
Cp	= 0.9*L/(0.9-Y)	57.7 sec
Ymax	= 1-L/C	0.738
R.C.(C)	= (0.9*Ymax-Y)*100%	= 80 %



Green Time Provided	Green Time Required	Delay	Green Time Provided
SG	FG	Delay	SG
FG	SG	FG	FG
10	8	0	13
13	12	8	20
8	7	4	8
8	7	4	10

Move-ment	Stage	Lane Width m.	No. of lane	Radius m.	O	N	Straight Ahead Sat. Flow	Movement			Total Flow pcu/h	Proportion of Turning Vehicles	Sat. Flow pcu/h	Flare Lane m.	Flare Effect pcu/hr	Site Factor	Site Effect pcu/hr	Gradient %	Gradient Effect pcu/hr	Revised Sat. Flow pcu/h	y	Greater y	L sec	g (required) sec	g (input) sec	Degree of Saturation X	Queue Length (m /lane)	Average Delay (seconds)
								Left pcu/h	Straight pcu/h	Right pcu/h																		
1,2	1	3.20	1	15		N	1935	564	1	565	0.00	1935							1935	0.292		8	76	86	0.501	48	16	
1	1	3.20	1	15		N	2075	606		606	0.00	2075							2075	0.292			76	86	0.501	54	16	
5,6	1	3.50	1	12		N	1965	590	90	680	0.13	1933	18	126					2059	0.330	0.330		86	86	0.501	48	11	
4,5	1	3.50	1	12	O	N	1965	571	2	573	0.00	1734							1734	0.330	0.330		86	86	0.501	42	12	
7,8	2	3.50	1	12		N	2105		15	90	1.00	1871	12	424					2295	0.039	0.039	7	10	17	0.501	18	61	
3	3	3.30	1	15		N	1945		3	3	1.00	1768							1768	0.002	0.002	19	0	19	0.501	0	329	
PED	3																											

NOTE : O - OPPOSING TRAFFIC N - NEAR SIDE LANE SG - STEADY GREEN FG - FLASHING GREEN PEDESTRAIN WALKING SPEED = 1.2m/s QUEUING LENGTH = AVERAGE QUEUE * 6m

LLA CONSULTANCY LIMITED

Application for Amendment of Plan under Section 12A of the Town Planning Ordinance (Cap. 131) to Rezone the Application Site from "Green Belt" and Area Shown as "Road to Residential (Group C)s" for Proposed Residential Development at Various Lots in D. 2 to Adjoining Government Land, Pak Wai, Sai Kung

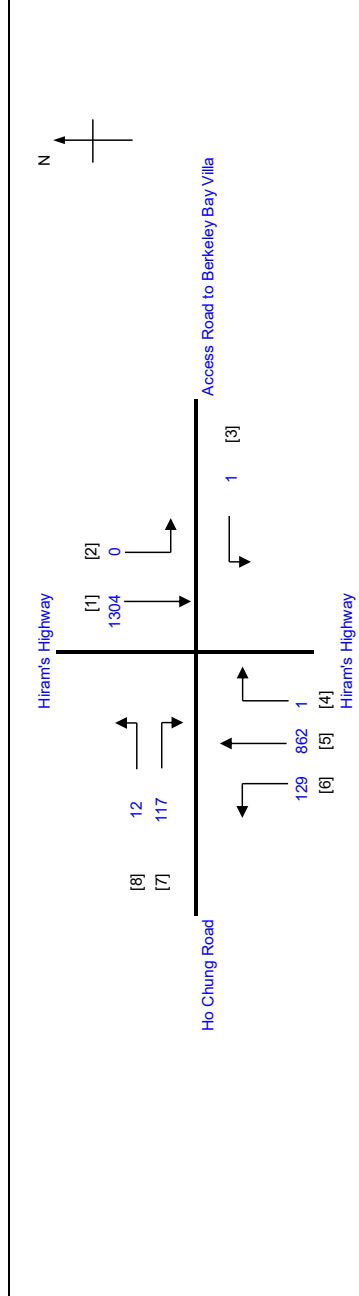
J2 Hiram's Highway/Ho Chung Road

TRAFFIC SIGNAL CALCULATION

PROJECT NO.: 40815
 FILENAME: J2_HH_HCR.xlsx

Prepared By:
 Checked By:
 Reviewed By:

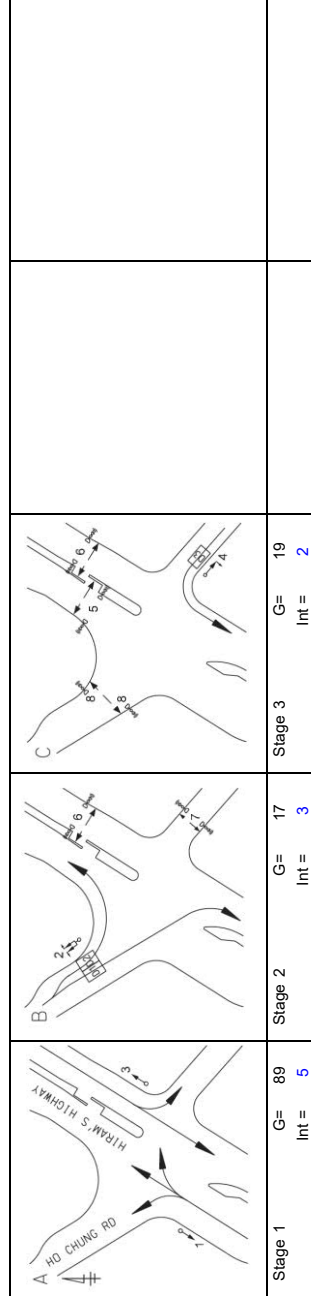
INITIALS DATE
 SKL Dec-25
 SLN Dec-25
 SLN Dec-25



No. of stages per cycle = 3

Cycle time = 135 sec
 Sum(y) = 0.382
 Loss time = 29 sec
 Total Flow = 2426 pcu
 Co = (1.5*L+5)/(1-Y) = 78.5 sec
 Crm = L/(1-Y) = 46.9 sec
 Yult = 0.683
 R.C.ult = (Yult-Y)*100% = 78.6 %
 Cp = 0.9*L/(0.9-Y) = 50.4 sec
 Ymax = 1-L/C = 0.785

R.C.(C) = (0.9*Ymax-Y)*100% = 85 %



Pedestrian Phase	Stage	Green Time Required SG	Delay FG	Green Time Provided SG	FG
P1	3	10	8	13	8
P2	2,3	13	12	21	12
P3	2	8	7	9	7
P4	3	8	7	10	7

Move-ment	Stage	Lane Width m.	No. of lane	Radius m.	O	N	Straight Ahead Sat. Flow	Movement			Total Flow pcu/h	Proportion of Turning Vehicles	Sat. Flow pcu/h	Flare Lane m.	Flare Effect pcu/hr	Site Factor	Site Effect pcu/hr	Gradient %	Gradient Effect pcu/hr	Revised Sat. Flow pcu/h	y	Greater y	L sec	g (required) sec	g (input) sec	Degree of Saturation X	Queue Length (m /lane)	Average Delay (seconds)
								Left pcu/h	Straight pcu/h	Right pcu/h																		
1,2	1	3.20	1	15		N	1935	0	629	0.00	1935								1935	0.325	0.325	8	90	90	0.487	42	11	
1	1	3.20	1	15		N	2075	675	675	0.00	2075								2075	0.325	0.325		90	90	0.487	48	11	
5,6	1	3.50	1	12	O	N	1965	129	535	0.24	1908	18	120						2028	0.264	0.264		73	90	0.487	54	19	
4,5	1	3.50	1	12		N	1965	456	457	0.00	1735								1735	0.263	0.263		73	90	0.487	42	19	
7,8	2	3.50	1	12		N	2105	12	129	1.00	1871	12	400						2271	0.057	0.057	2	16	18	0.487	24	56	
3	3	3.30	1	15		N	1945	1	1	1.00	1768								1768	0.001	0.001	19	0	19	0.487	0	808	
PED	3																											

NOTE : O - OPPOSING TRAFFIC N - NEAR SIDE LANE SG - STEADY GREEN FG - FLASHING GREEN PEDESTRAIN WALKING SPEED = 1.2m/s QUEUING LENGTH = AVERAGE QUEUE * 6m

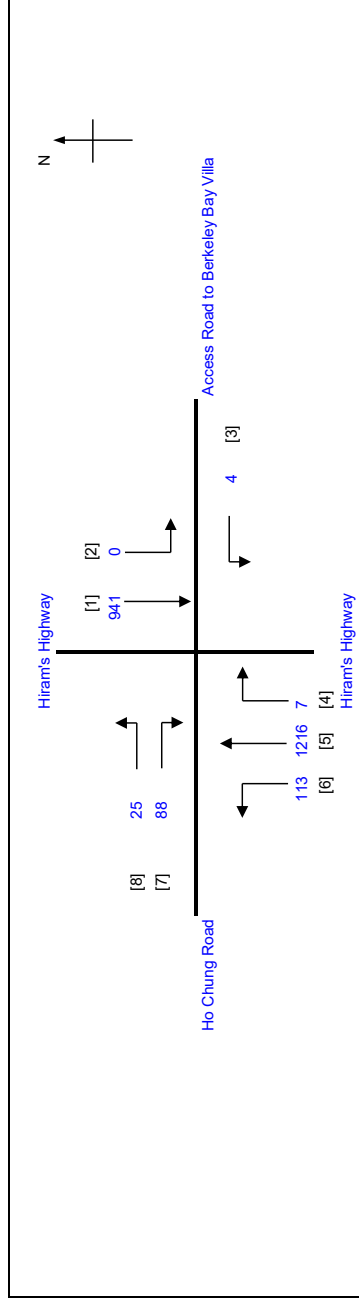
LLA CONSULTANCY LIMITED

Application for Amendment of Plan under Section 12A of the Town Planning Ordinance (Cap. 131) to Rezone the Application Site from "Green Belt" and Area Shown as "Road to Residential (Group C)s" for Proposed Residential Development at Various Lots in D. 2 to Adjoining Government Land, Pak Wai, Sai Kung

TRAFFIC SIGNAL CALCULATION

PROJECT NO.: 40815
 FILENAME: J2_HH_HCR.xlsx

Prepared By: SKL Dec-25
 Checked By: SLN Dec-25
 Reviewed By: SLN Dec-25



No. of stages per cycle: **3**

Cycle time: **130 sec**

Sum(y): **0.403**

Loss time: **33 sec**

Total Flow: **2394 pcu**

Co = $(1.5 \cdot L + 5) / (1 - Y)$ = **91.2 sec**

Cm = $L / (1 - Y)$ = **55.2 sec**

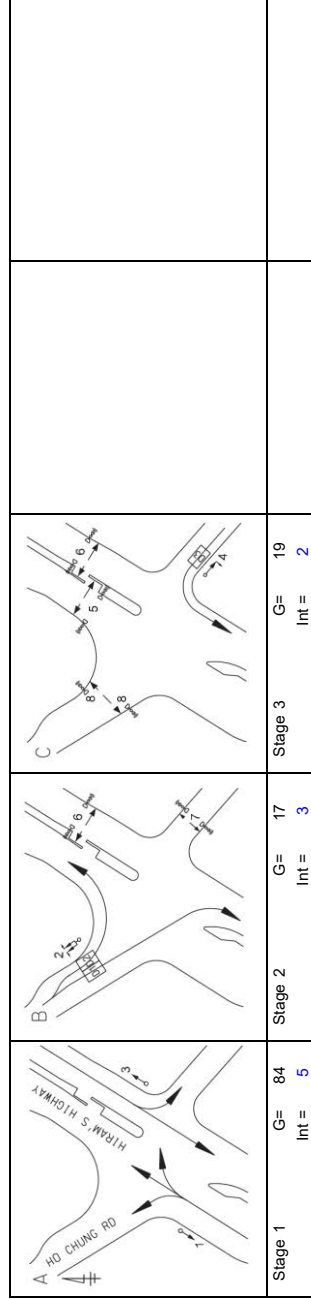
Yult = **0.653**

R.C.ult = $(Yult - Y) / Y * 100\%$ = **62.1 %**

Cp = $0.9 \cdot L / (0.9 - Y)$ = **59.7 sec**

Ymax = $1 - L / C$ = **0.746**

R.C.(C) = $(0.9 \cdot Ymax - Y) / Y * 100\%$ = 67 %



Stage	Green Time Required SG	Green Time Required FG	Delay	Green Time Provided SG	Green Time Provided FG
3	10	8	0	13	8
2,3	13	12	8	21	12
2	8	7	4	9	7
3	8	7	4	10	7

Move-ment	Stage	Lane Width m.	No. of lane	Radius m.	O	N	Straight Ahead Sat. Flow	Movement			Total Flow pcu/h	Proportion of Turning Vehicles	Sat. Flow pcu/h	Flare Lane m.	Flare Effect pcu/hr	Site Factor	Site Effect pcu/hr	Gradient %	Gradient Effect pcu/hr	Revised Sat. Flow pcu/h	y	Greater y	L sec	g (required) sec	g (input) sec	Degree of Saturation X	Queue Length (m /lane)	Average Delay (seconds)
								Left pcu/h	Straight pcu/h	Right pcu/h																		
1,2	1	3.20	1	15		N	1935	0	454	454	0.00	1935							1935	0.235		8	57	85	0.540	54	27	
1	1	3.20	1	15		N	2075	487	487	487	0.00	2075							2075	0.235			57	85	0.540	54	27	
5,6	1	3.50	1	12		N	1965	113	612	725	0.16	1927	18	127					2055	0.353	0.353		85	85	0.540	54	12	
4,5	1	3.50	1	12	O	N	1965	604	604	611	0.01	1733							1733	0.353	0.353		85	85	0.540	42	13	
7,8	2	3.50	1	12		N	2105	25	88	113	1.00	1871	12	400					2271	0.050	0.050	6	12	18	0.540	18	60	
3	3	3.30	1	15		N	1945	4	4	4	1.00	1768							1768	0.002	0.002	19	1	19	0.540	0	314	
PED	3																											

NOTE : O - OPPOSING TRAFFIC N - NEAR SIDE LANE SG - STEADY GREEN FG - FLASHING GREEN PEDESTRAIN WALKING SPEED = 1.2m/s QUEUING LENGTH = AVERAGE QUEUE * 6m

LLA CONSULTANCY LIMITED

Application for Amendment of Plan under Section 12A of the Town Planning Ordinance (Cap. 131) to Rezone the Application Site from "Green Belt" and Area Shown as "Road to Residential (Group C)s" for Proposed Residential Development at Various Lots in D. 2 to Adjoining Government Land, Pak Wai, Sai Kung

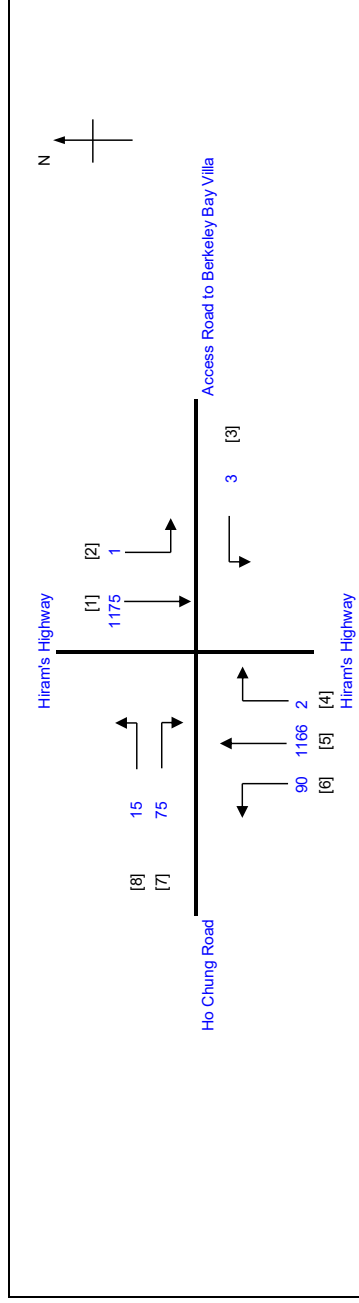
TRAFFIC SIGNAL CALCULATION

PROJECT NO.: 40815
 FILENAME: J2_HH_HCR.xlsx

Prepared By:
 Checked By:
 Reviewed By:

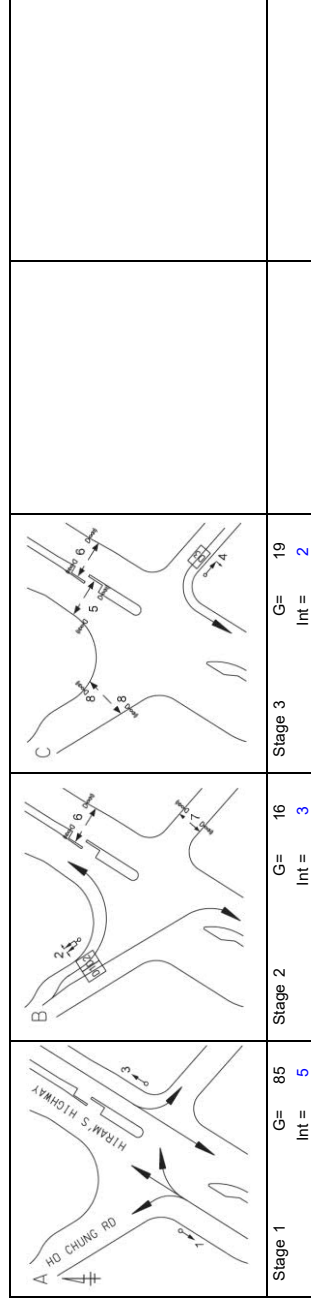
INITIALS
 SKL
 SLN
 SLN

DATE
 Dec-25
 Dec-25
 Dec-25



No. of stages per cycle
 Cycle time
 Sum(y)
 Loss time
 Total Flow
 Co
 Crm
 Yult
 R.C.ult
 Cp
 Ymax
R.C.(C) = (0.9*Ymax-Y)*100% = 79 %

N = 3
 C = 130 sec
 Y = 0.371
 L = 34 sec
 = 2527 pcu
 = 89.0 sec
 = 54.1 sec
 = 0.645
 = 73.9 %
 = 57.8 sec
 = 0.738



Green Time Required
 SG
 FG
 Delay
 Green Time Provided
 SG
 FG

Stage
 Pedestrian Phase
 P1
 P2
 P3
 P4

Movement	Stage	Lane Width m.	No. of lane	Radius m.	O	N	Straight Ahead Sat. Flow	Movement Left Straight Right	Total Flow	Proportion of Turning Vehicles	Sat. Flow	Flare Lane m.	Flare Effect	Site Factor	Site Effect	Gradient %	Gradient Effect	Revised Sat. Flow	y	Greater y	L sec	g (required) sec	g (input) sec	Degree of Saturation X	Queue Length (m /lane)	Average Delay (seconds)
1,2	1	3.20	1	15		N	1935	566	567	0.00	1935							1935	0.293		8	76	86	0.502	48	16
1	1	3.20	1			N	2075	609	609	0.00	2075							2075	0.293			76	86	0.502	54	16
5,6	1	3.50	1	12		N	1965	90	683	0.13	1933	18	126					2059	0.332	0.332		86	86	0.502	48	11
4,5	1	3.50	1	12	O	N	1965	573	575	0.00	1734							1734	0.332			86	86	0.502	42	12
7,8	2	3.50	1	12		N	2105	15	90	1.00	1871	12	424					2295	0.039	0.039	7	10	17	0.502	18	61
3	3	3.30	1	15		N	1945	3	3	1.00	1768							1768	0.002	0.002	19	0	19	0.502	0	332
PED	3																									

NOTE : O - OPPOSING TRAFFIC N - NEAR SIDE LANE SG - STEADY GREEN FG - FLASHING GREEN PEDESTRAIN WALKING SPEED = 1.2m/s QUEUING LENGTH = AVERAGE QUEUE * 6m

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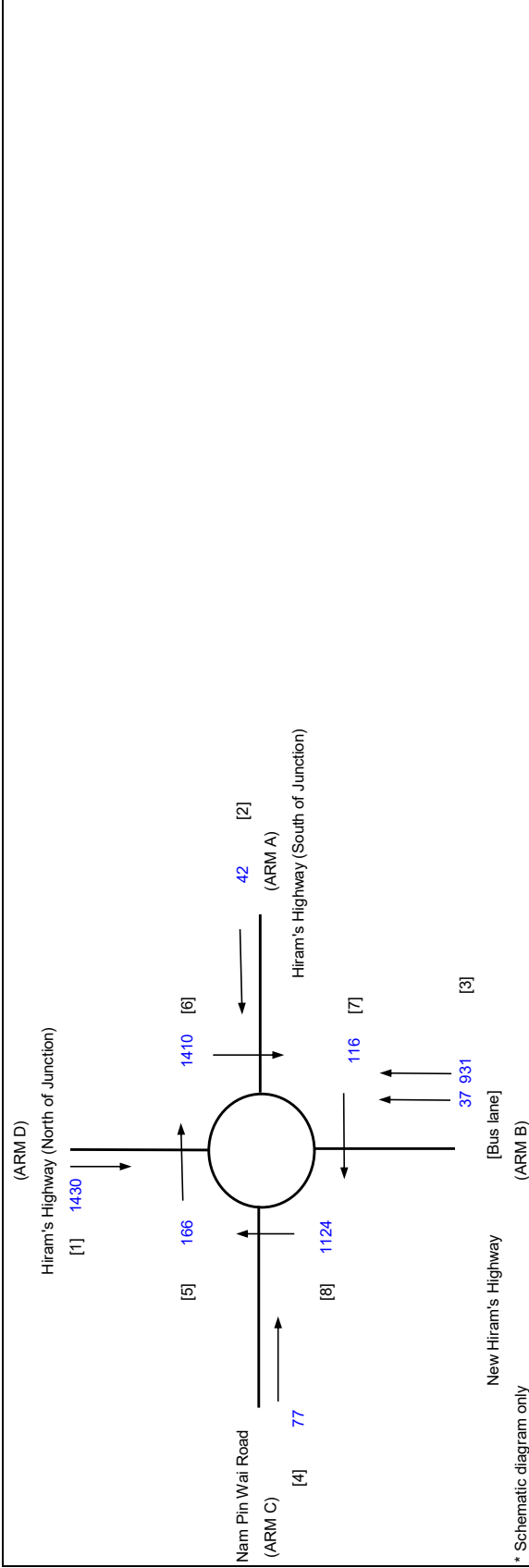
Application for Amendment of Plan under Section 12A of the Town Planning Ordinance (Cap. 131) to Rezone the Application Site from "Green Belt" and Area Shown as "Road" to "Residential (Group C)" for Proposed Residential Development at Various Lots in D.D. 210 and Adjoining Government Land, Pak Wai, Sai Kung

J3 Hiram's Highway/ New Hiram's Highway/ Nam Pin Wai Road

ROUNDABOUT CALCULATION

PROJECT NO.: 40815
 FILENAME : J3_HH_NHH.xlsx
 REFERENCE NO.:
 PREPARED BY: SKL
 CHECKED BY: SLN
 REVIEWED BY: SLN

INITIALS DATE
 SKL Dec-25
 SLN Dec-25
 SLN Dec-25



ARM	A	B	C	D
V = Approach half width (m)	3.70	7.40	4.30	7.70
E = Entry width (m)	7.30	11.00	7.30	7.80
L = Effective length of flare (m)	11.00	1.00	20.00	1.00
R = Entry radius (m)	15.00	55.00	23.00	18.00
D = Inscribed circle diameter (m)	78.00	78.00	78.00	78.00
A = Entry angle (degree)	32.00	40.00	50.00	36.00
Q = Entry flow (pcuh)	42	931	77	1430
Qc = Circulating flow across entry (pcuh)	1410	116	1124	166
OUTPUT PARAMETERS:				
S = Sharpness of flare = 1.6(E-V)/L	0.52	5.76	0.24	0.16
K = 1-0.00347(A-30)-0.978(1/R-0.05)	0.98	1.00	0.94	0.97
X2 = V + ((E-V)/(1+2S))	5.46	7.69	6.33	7.78
M = EXP((D-60)/10)	6.05	6.05	6.05	6.05
F = 303*X2	1654	2329	1917	2356
Td = 1+(0.5/(1+M))	1.07	1.07	1.07	1.07
Fc = 0.21*Td*(1+0.2*X2)	0.47	0.57	0.51	0.57
Qe = K(F-Fc)*Qc	968	2255	1260	2201
DFC = Design flow/Capacity = Q/Qe	0.04	0.41	0.06	0.65
Total In Sum =				2480 PCU
DFC of Critical Approach =				0.65

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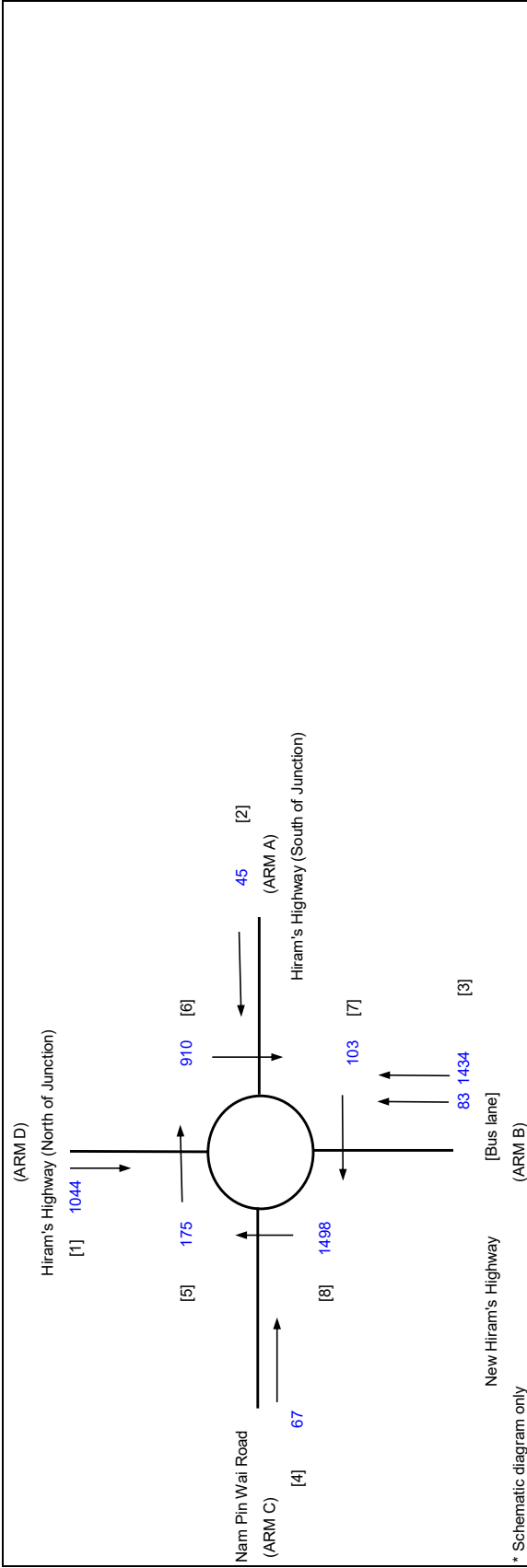
Application for Amendment of Plan under Section 12A of the Town Planning Ordinance (Cap. 131) to Rezone the Application Site from "Green Belt" and Area Shown as "Road" to "Residential (Group C)" for Proposed Residential Development at Various Lots in D.D. 210 and Adjoining Government Land, Pak Wai, Sai Kung

J3 Hiram's Highway/ New Hiram's Highway/ Nam Pin Wai Road

ROUNDABOUT CALCULATION

PROJECT NO.: 40815
 FILENAME : J3_HH_NHH.xlsx
 REFERENCE NO.:
 PREPARED BY: SKL
 CHECKED BY: SLN
 REVIEWED BY: SLN

INITIALS DATE
 SKL Dec-25
 SLN Dec-25
 SLN Dec-25



ARM	A	B	C	D
V = Approach half width (m)	3.70	7.40	4.30	7.70
E = Entry width (m)	7.30	11.00	7.30	7.80
L = Effective length of flare (m)	11.00	1.00	20.00	1.00
R = Entry radius (m)	15.00	55.00	23.00	18.00
D = Inscribed circle diameter (m)	78.00	78.00	78.00	78.00
A = Entry angle (degree)	32.00	40.00	50.00	36.00
Q = Entry flow (pcu/h)	45	1434	67	1044
Qc = Circulating flow across entry (pcu/h)	910	103	1498	175

OUTPUT PARAMETERS:	A	B	C	D
S = Sharpness of flare = 1.6(E-V)/L	0.52	5.76	0.24	0.16
K = 1-0.00347(A-30)-0.978(1/R-0.05)	0.98	1.00	0.94	0.97
X2 = V + ((E-V)/(1+2S))	5.46	7.69	6.33	7.78
M = EXP((D-60)/10)	6.05	6.05	6.05	6.05
F = 303*X2	1654	2329	1917	2356
Td = 1+(0.5/(1+M))	1.07	1.07	1.07	1.07
Fc = 0.21*Td(1+0.2*X2)	0.47	0.57	0.51	0.57
Qe = K(F-Fc)*Qc	1197	2262	1081	2196

DFC = Design flow/Capacity = Q/Qe	0.04	0.63	0.06	0.48
Total In Sum =			2590	PCU
DFC of Critical Approach =				0.63

LLA CONSULTANCY LIMITED

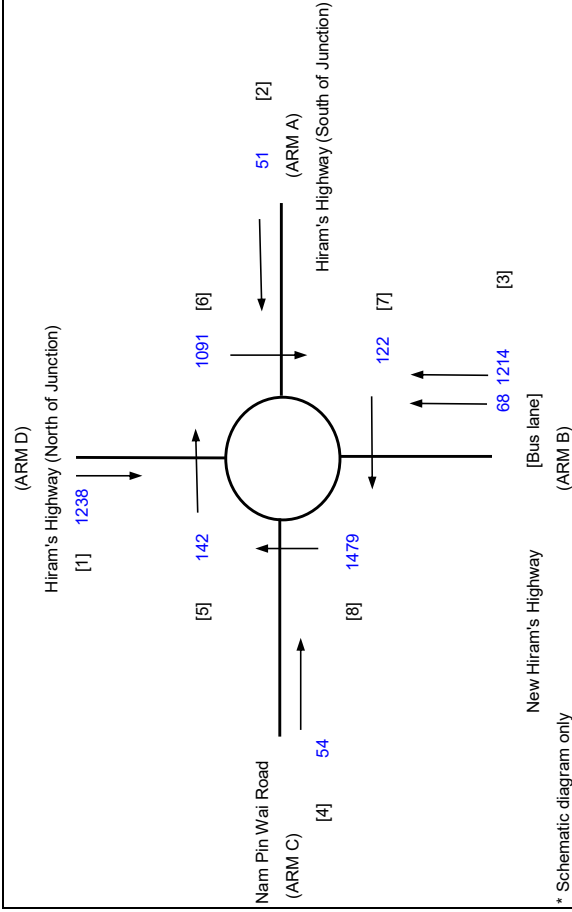
Application for Amendment of Plan under Section 12A of the Town Planning Ordinance (Cap. 131) to Rezone the Application Site from "Green Belt" and Area Shown as "Road" to "Residential (Group C)S" for Proposed Residential Development at Various Lots in D.D. 210 and Adjoining Government Land, Pak Wai, Sai Kung

J3 Hiram's Highway/ New Hiram's Highway/ Nam Pin Wai Road

ROUNDABOUT CALCULATION

PROJECT NO.: 40815
 FILENAME : J3_HH_NHH.xlsx
 REFERENCE NO.:
 PREPARED BY: SKL
 CHECKED BY: SLN
 REVIEWED BY: SLN

INITIALS DATE
 SKL Dec-25
 SLN Dec-25
 SLN Dec-25



ARM	A	B	C	D
V = Approach half width (m)	3.70	7.40	4.30	7.70
E = Entry width (m)	7.30	11.00	7.30	7.80
L = Effective length of flare (m)	11.00	1.00	20.00	1.00
R = Entry radius (m)	15.00	55.00	23.00	18.00
D = Inscribed circle diameter (m)	78.00	78.00	78.00	78.00
A = Entry angle (degree)	32.00	40.00	50.00	36.00
Q = Entry flow (pcuh)	51	1214	54	1238
Qc = Circulating flow across entry (pcuh)	1091	122	1479	142
OUTPUT PARAMETERS:				
S = Sharpness of flare = 1.6(E-V)/L	0.52	5.76	0.24	0.16
K = 1-0.00347(A-30)-0.978(1/R-0.05)	0.98	1.00	0.94	0.97
X2 = V + ((E-V)/(1+2S))	5.46	7.69	6.33	7.78
M = EXP((D-60)/10)	6.05	6.05	6.05	6.05
F = 303*X2	1654	2329	1917	2356
Td = 1+(0.5/(1+M))	1.07	1.07	1.07	1.07
Fc = 0.21*Td(1+0.2*X2)	0.47	0.57	0.51	0.57
Qe = K(F-Fc)*Qc	1114	2252	1090	2215
DFC = Design flow/Capacity = Q/Qe	0.05	0.54	0.05	0.56
Total In Sum =				2557 PCU
DFC of Critical Approach =				0.56

LLA CONSULTANCY LIMITED

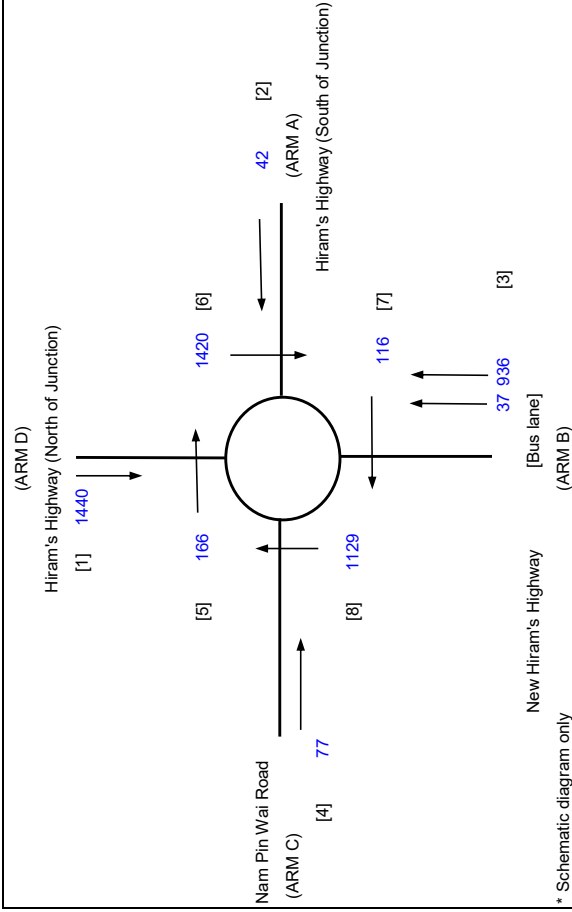
Application for Amendment of Plan under Section 12A of the Town Planning Ordinance (Cap. 131) to Rezone the Application Site from "Green Belt" and Area Shown as "Road" to "Residential (Group C)S" for Proposed Residential Development at Various Lots in D.D. 210 and Adjoining Government Land, Pak Wai, Sai Kung

J3 Hiram's Highway/ New Hiram's Highway/ Nam Pin Wai Road

ROUNDABOUT CALCULATION

PROJECT NO.: 40815
 FILENAME : J3_HH_NHH.xlsx
 REFERENCE NO.:
 PREPARED BY: SKL
 CHECKED BY: SLN
 REVIEWED BY: SLN

INITIALS
 DATE
 Dec-25
 Dec-25
 Dec-25



ARM	A	B	C	D
V = Approach half width (m)	3.70	7.40	4.30	7.70
E = Entry width (m)	7.30	11.00	7.30	7.80
L = Effective length of flare (m)	11.00	1.00	20.00	1.00
R = Entry radius (m)	15.00	55.00	23.00	18.00
D = Inscribed circle diameter (m)	78.00	78.00	78.00	78.00
A = Entry angle (degree)	32.00	40.00	50.00	36.00
Q = Entry flow (pcu/h)	42	936	77	1440
Qc = Circulating flow across entry (pcu/h)	1420	116	1129	166

OUTPUT PARAMETERS:	A	B	C	D
S = Sharpness of flare = 1.6(E-V)/L	0.52	5.76	0.24	0.16
K = 1-0.00347(A-30)-0.978(1/R-0.05)	0.98	1.00	0.94	0.97
X2 = V + ((E-V)/(1+2S))	5.46	7.69	6.33	7.78
M = EXP((D-60)/10)	6.05	6.05	6.05	6.05
F = 303*X2	1654	2329	1917	2356
Td = 1+(0.5/(1+M))	1.07	1.07	1.07	1.07
Fc = 0.21*Td(1+0.2*X2)	0.47	0.57	0.51	0.57
Qe = K(F-Fc)*Qc	963	2255	1257	2201

DFC = Design flow/Capacity = Q/Qe	0.04	0.42	0.06	0.65
Total In Sum =	2495 PCU			
DFC of Critical Approach =	0.65			

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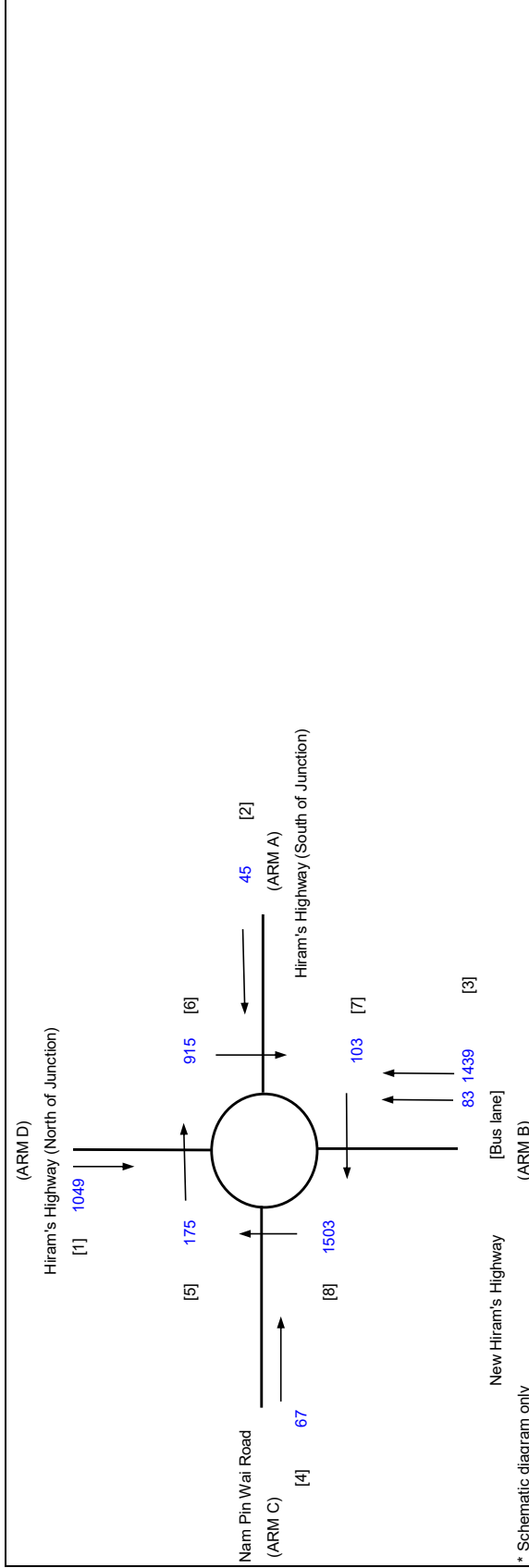
Application for Amendment of Plan under Section 12A of the Town Planning Ordinance (Cap. 131) to Rezone the Application Site from "Green Belt" and Area Shown as "Road" to "Residential (Group C)S" for Proposed Residential Development at Various Lots in D.D. 210 and Adjoining Government Land, Pak Wai, Sai Kung

J3 Hiram's Highway/ New Hiram's Highway/ Nam Pin Wai Road

ROUNDABOUT CALCULATION

PROJECT NO.: 40815
 FILENAME : J3_HH_NHH.xlsx
 REFERENCE NO.:
 PREPARED BY: SKL
 CHECKED BY: SLN
 REVIEWED BY: SLN

INITIALS
 DATE
 SKL Dec-25
 SLN Dec-25
 SLN Dec-25



ARM	A	B	C	D
V = Approach half width (m)	3.70	7.40	4.30	7.70
E = Entry width (m)	7.30	11.00	7.30	7.80
L = Effective length of flare (m)	11.00	1.00	20.00	1.00
R = Entry radius (m)	15.00	55.00	23.00	18.00
D = Inscribed circle diameter (m)	78.00	78.00	78.00	78.00
A = Entry angle (degree)	32.00	40.00	50.00	36.00
Q = Entry flow (pcu/h)	45	1439	67	1049
Qc = Circulating flow across entry (pcu/h)	915	103	1503	175
OUTPUT PARAMETERS:				
S = Sharpness of flare = 1.6(E-V)/L	0.52	5.76	0.24	0.16
K = 1-0.00347(A-30)-0.978(1/R-0.05)	0.98	1.00	0.94	0.97
X2 = V + ((E-V)/(1+2S))	5.46	7.69	6.33	7.78
M = EXP((D-60)/10)	6.05	6.05	6.05	6.05
F = 303*X2	1654	2329	1917	2356
Td = 1+(0.5/(1+M))	1.07	1.07	1.07	1.07
Fc = 0.21*Td(1+0.2*X2)	0.47	0.57	0.51	0.57
Qe = K(F-Fc)*Qc	1195	2262	1079	2196
Total In Sum = 2600 PCU				
DFC = Design flow/Capacity = Q/Qe	0.04	0.64	0.06	0.48
DFC of Critical Approach = 0.64				

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Application for Amendment of Plan under Section 12A of the Town Planning Ordinance (Cap. 131) to Rezone the Application Site from "Green Belt" and Area Shown as "Road" to "Residential (Group C)S" for Proposed Residential Development at Various Lots in D.D. 210 and Adjoining Government Land, Pak Wai, Sai Kung

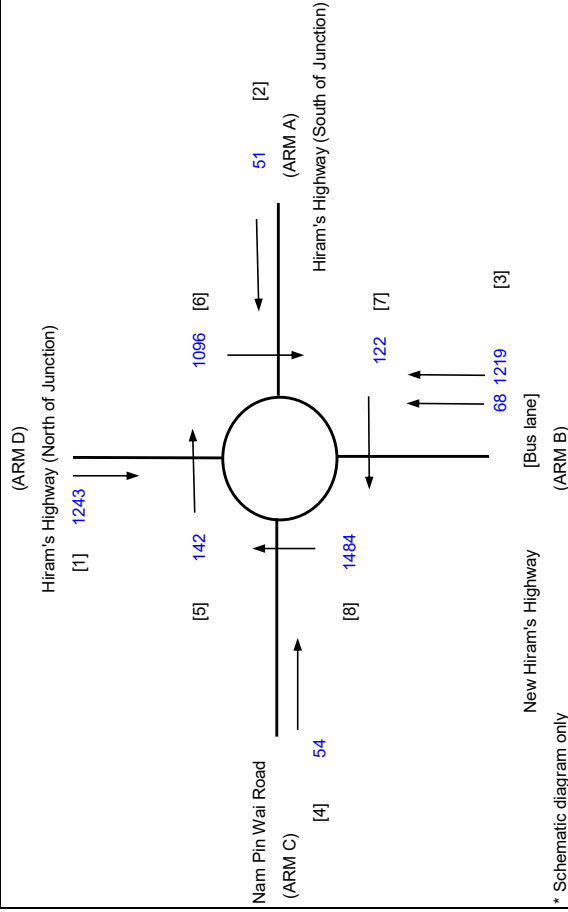
J3 Hiram's Highway/ New Hiram's Highway/ Nam Pin Wai Road

ROUNDABOUT CALCULATION

PROJECT NO.: 40815
 FILENAME : J3_HH_NHH.xlsx
 REFERENCE NO.:
 PREPARED BY: SKL
 CHECKED BY: SLN
 REVIEWED BY: SLN

INITIALS
 DATE
 SKL Dec-25
 SLN Dec-25
 SLN Dec-25

2034 Design WN



ARM

INPUT PARAMETERS:

	A	B	C	D
V = Approach half width (m)	3.70	7.40	4.30	7.70
E = Entry width (m)	7.30	11.00	7.30	7.80
L = Effective length of flare (m)	11.00	1.00	20.00	1.00
R = Entry radius (m)	15.00	55.00	23.00	18.00
D = Inscribed circle diameter (m)	78.00	78.00	78.00	78.00
A = Entry angle (degree)	32.00	40.00	50.00	36.00
Q = Entry flow (pcuh)	51	1219	54	1243
Qc = Circulating flow across entry (pcuh)	1096	122	1484	142

OUTPUT PARAMETERS:

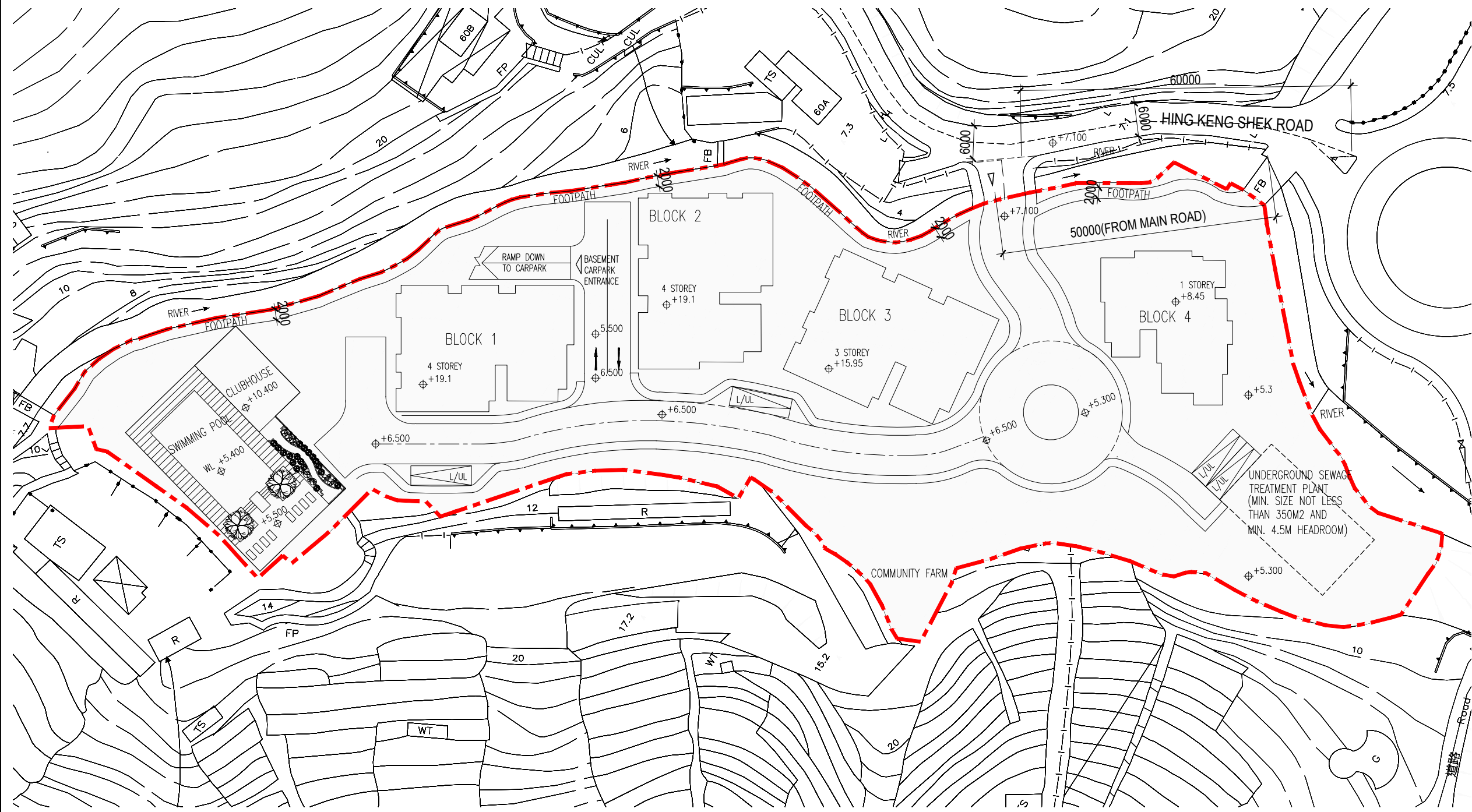
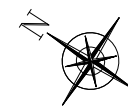
S = Sharpness of flare = $1.6(E-V)/L$	0.52	5.76	0.24	0.16
K = $1-0.00347(A-30)-0.978(1/R-0.05)$	0.98	1.00	0.94	0.97
X2 = $V + ((E-V)/(1+2S))$	5.46	7.69	6.33	7.78
M = $EXP((D-60)/10)$	6.05	6.05	6.05	6.05
F = $303 \times X2$	1654	2329	1917	2356
Td = $1+(0.5/(1+M))$	1.07	1.07	1.07	1.07
Fc = $0.21 \times Td(1+0.2 \times X2)$	0.47	0.57	0.51	0.57
Qe = $K(F \times Fc \times Qc)$	1112	2252	1088	2215

DFC = Design flow/Capacity = Q/Qe

Total In Sum = 2567 PCU

DFC of Critical Approach = 0.56

Appendix C
Proposed Layout Plan



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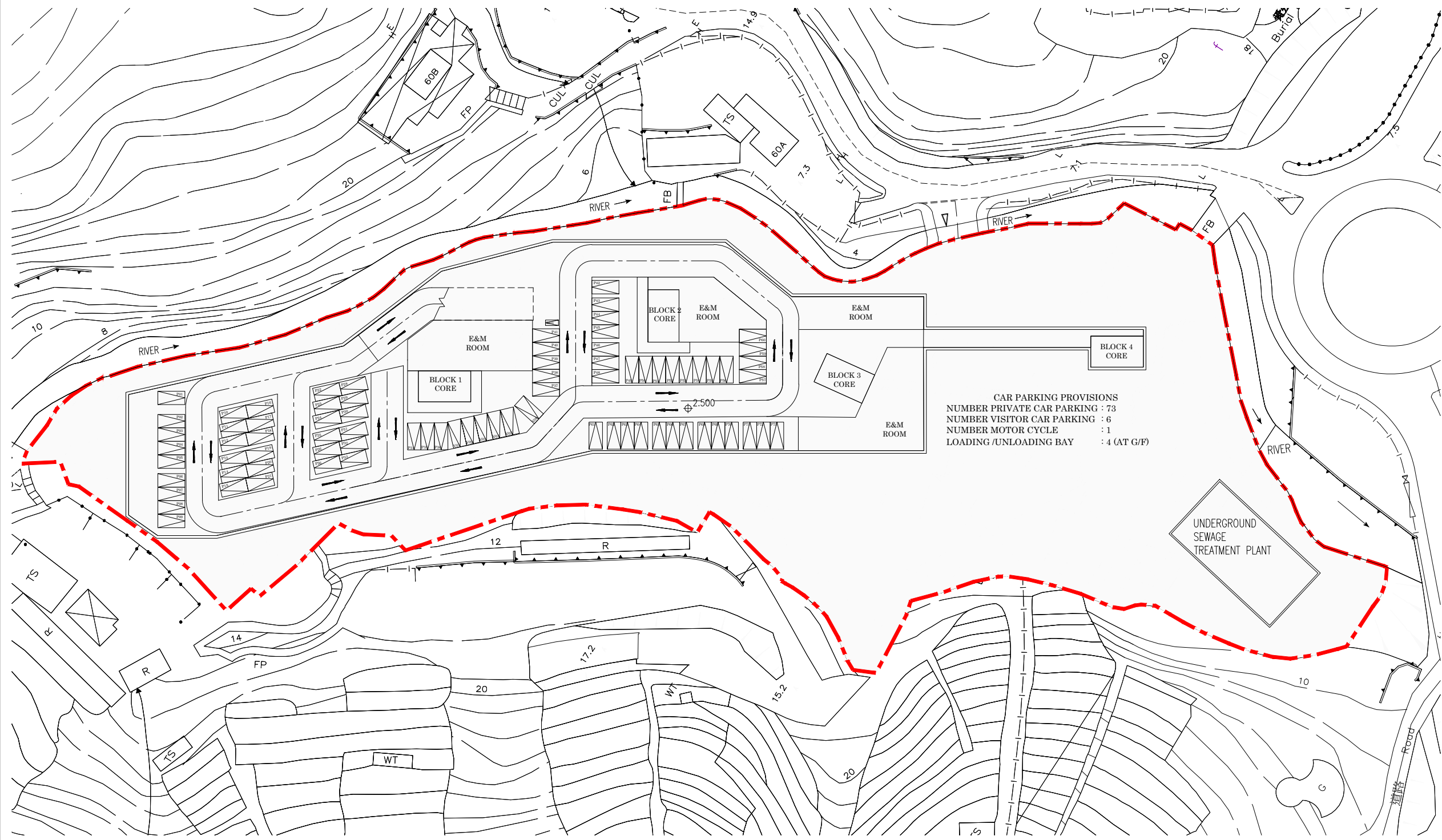
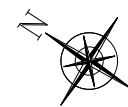
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Client

Project
 APPLICATION FOR AMENDMENT OF PLAN UNDER SECTION 12A OF THE TOWN PLANNING ORDINANCE (CAP.131) TO REZONE THEE APPLICATION SITE FROM "GREEN" BELT AND AREA SHOWN AS "ROAD" TO "RESIDENTIAL (GROUP C) 5" FOR PROPOSEED RESIDENTIAL DEVELOPMENT AT VARIOUS LOTS IN D.D.210 AND ADJOINING GOVERNMENT LAND, PAK WAI, SAI KUNG

Drawing Title
 MASTER LAYOUT PLAN

Job No.	Drawing No.	Revision No.
D1186	MLP-01	P
Scale	Date	CAD Ref.
1:800	09/06/2025	
Drawn	Checked	Approved
SF	SF	



CAR PARKING PROVISIONS
 NUMBER PRIVATE CAR PARKING : 73
 NUMBER VISITOR CAR PARKING : 6
 NUMBER MOTOR CYCLE : 1
 LOADING /UNLOADING BAY : 4 (AT G/F)

K & W Architects Ltd.
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Client

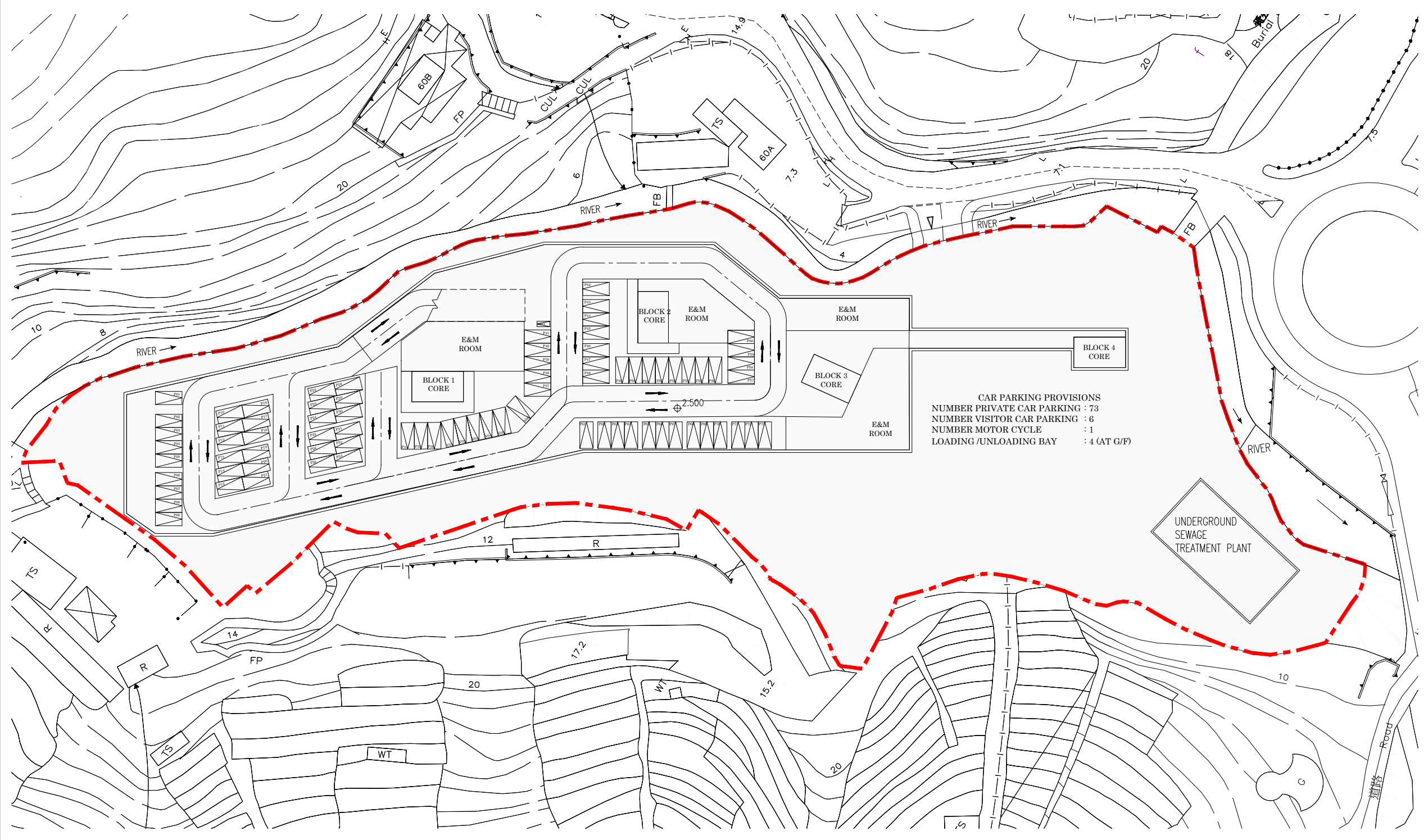
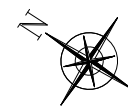
Project
 APPLICATION FOR AMENDMENT OF PLAN UNDER SECTION 12A OF THE TOWN PLANNING ORDINANCE (CAP.131) TO REZONE THEE APPLICATION SITE FROM "GREEN" BELT AND AREA SHOWN AS "ROAD" TO "RESIDENTIAL (GROUP C) 5" FOR PROPOSEED RESIDENTIAL DEVELOPMENT AT VARIOUS LOTS IN D.D.210 AND ADJOINING GOVERNMENT LAND, PAK WAI, SAI KUNG

Drawing Title
BASEMENT PLAN

Job No.	Drawing No.	Revision No.
D1186	FL-02	P
Scale	Date	CAD Ref.
1:500	23/08/2023	
Drawn	Checked	Approved
PC	PC	

Appendix D

Replacement Page of the Revised Indicative Architectural Drawings



CAR PARKING PROVISIONS
 NUMBER PRIVATE CAR PARKING : 73
 NUMBER VISITOR CAR PARKING : 6
 NUMBER MOTOR CYCLE : 1
 LOADING /UNLOADING BAY : 4 (AT G/F)

K & W Architects Ltd.
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Client

Project
 APPLICATION FOR AMENDMENT OF PLAN UNDER SECTION 12A OF THE TOWN PLANNING ORDINANCE (CAP.131) TO REZONE THEE APPLICATION SITE FROM "GREEN" BELT AND AREA SHOWN AS "ROAD" TO "RESIDENTIAL (GROUP C) 5" FOR PROPOSEED RESIDENTIAL DEVELOPMENT AT VARIOUS LOTS IN D.D.210 AND ADJOINING GOVERNMENT LAND, PAK WAI, SAI KUNG

Drawing Title
BASEMENT PLAN

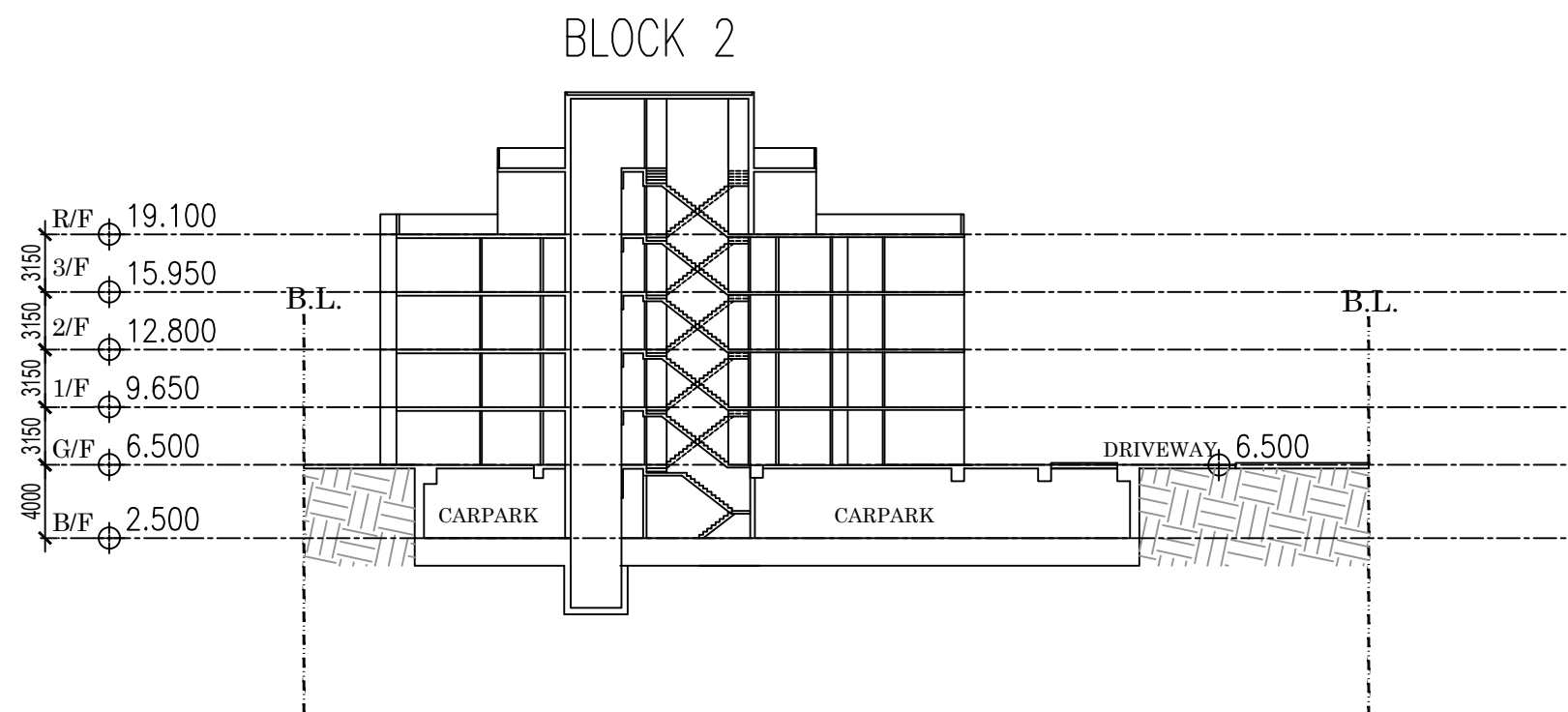
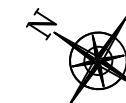
Job No.	Drawing No.	Revision No.
D1186	FL-02	P
Scale	Date	CAD Ref.
1:500	23/08/2023	
Drawn	Checked	Approved
PC	PC	

Appendix E

Supplementary Information



KEY PLAN 1 : 3000



DIAGRAMATIC SECTION

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K&W

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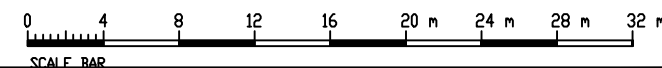
Client

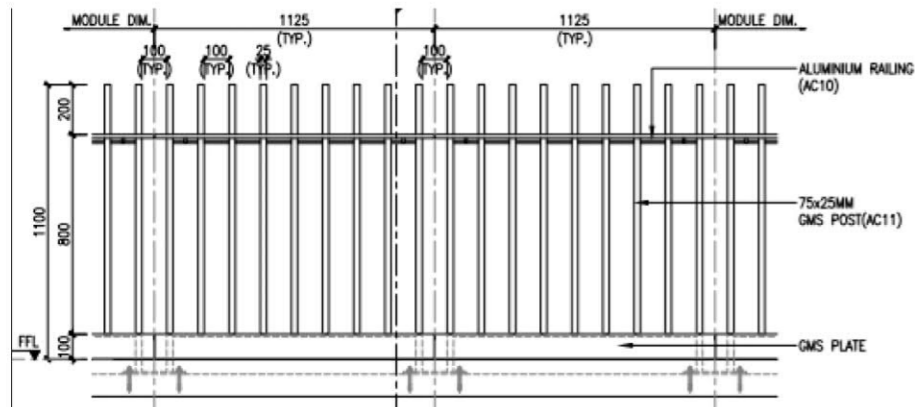
Project
APPLICATION FOR AMENDMENT OF PLAN UNDER SECTION 12A OF THE TOWN PLANNING ORDINANCE (CAP.131) TO REZONE THEE APPLICATION SITE FROM "GREEN" BELT AND AREA SHOWN AS "ROAD" TO "RESIDENTIAL (GROUP C) 5" FOR PROPOSEED RESIDENTIAL DEVELOPMENT AT VARIOUS LOTS IN D.D.210 AND ADJOINING GOVERNMENT LAND, PAK WAI, SAI KUNG

Drawing Title

DIAGRAMATIC SECTION

Job No.	Drawing No.	Revision No.
D1186	FL-05	L
Scale	Date	CAD Ref.
1:800	30/05/2024	
Drawn	Checked	Approved
PC	PC	





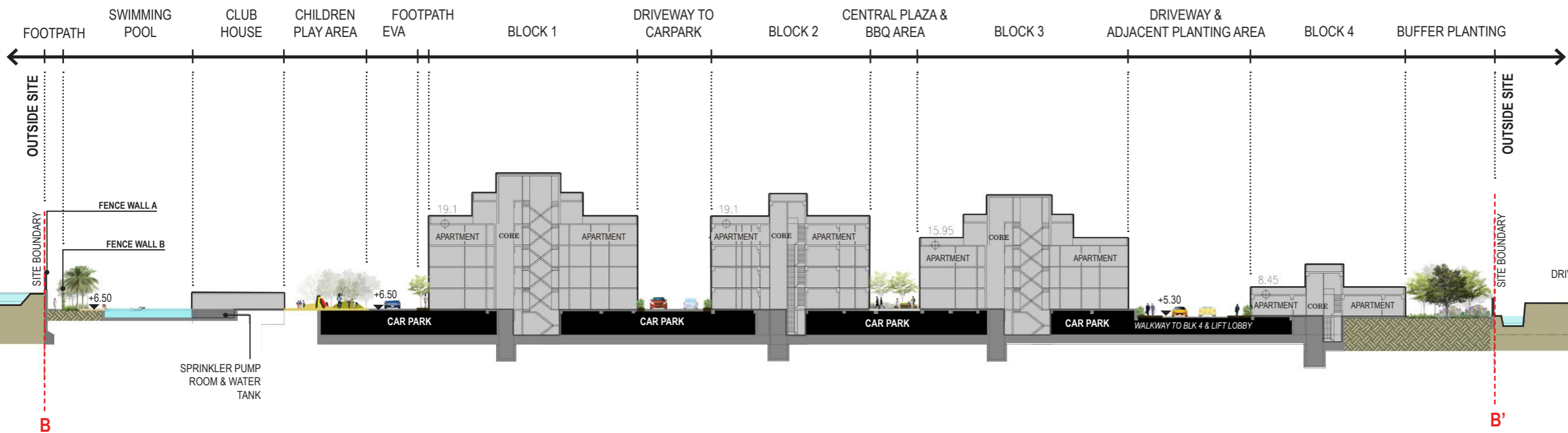
Proposed 1.2m fence wall along site boundary
"Fence Wall A"



Reference Image
Proposed 2m fence wall along eastern footpath
"Fence Wall B"



KEY PLAN



SCALE 1:800 @ A3

NO.	DESCRIPTION	DATE	BY
REVISIONS			
LANDSCAPE ARCHITECT 地利環境 TEAM 73 <small>T (852) 2965-5713 F (852) 2965-4809 UNIT A, 16/F, WINGASE CENTRAL, NO. 288 QUEEN'S ROAD CENTRAL, HONG KONG</small>			
ARCHITECT K & W Architects Ltd. 關黃建築師有限公司 K&W			
DRAWN:			
CHECKED:			
APPROVED:			
<small>All measurements must be checked on the work by contractor. This drawing not to be used for construction purposes until countersigned.</small>			
<small>This drawing and design are copyright and no portion may be reproduced without the written consent of TEAM 73 HK Ltd.</small>			
PROJECT Application for Amendment of Plan Under Section 12A of the Town Planning Ordinance (Cap. 131) to Rezone the Application Site from "Green Belt" and Area Shown as "Road" to "Residential (Group C)5" for Proposed Residential Development at Various Lots in D.D. 210 and Adjoining Government Land, Pak Wai, Sai Kung			
TITLE SECTION B - B'			
SCALE	PAPER SIZE	ISSUE DATE	
AS SHOWN	A3		
PROJECT NUMBER			
T73-23003			
COMPUTER DWG. NUMBER			
L:\2023\A\23003-00210 Sai Kung (HK So)\OFFICE\CAD_20230504			
DRAWING NUMBER			REV
SS-03			